

**City of Belmont** 

# **Attachments**

## **Ordinary Council Meeting**

Held 26 February 2019





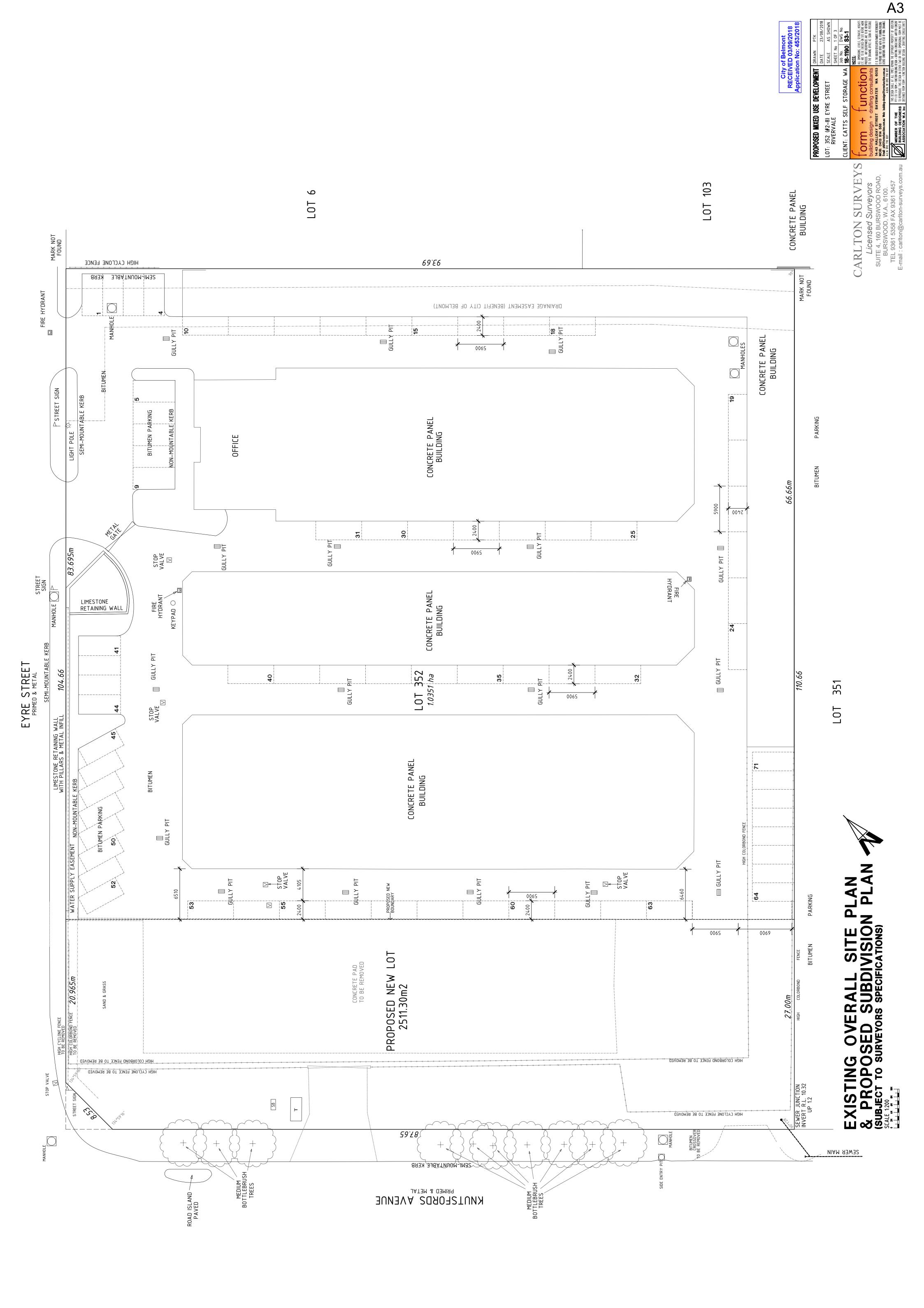
# Ordinary Council Meeting 26/02/19

Item 12.1 refers

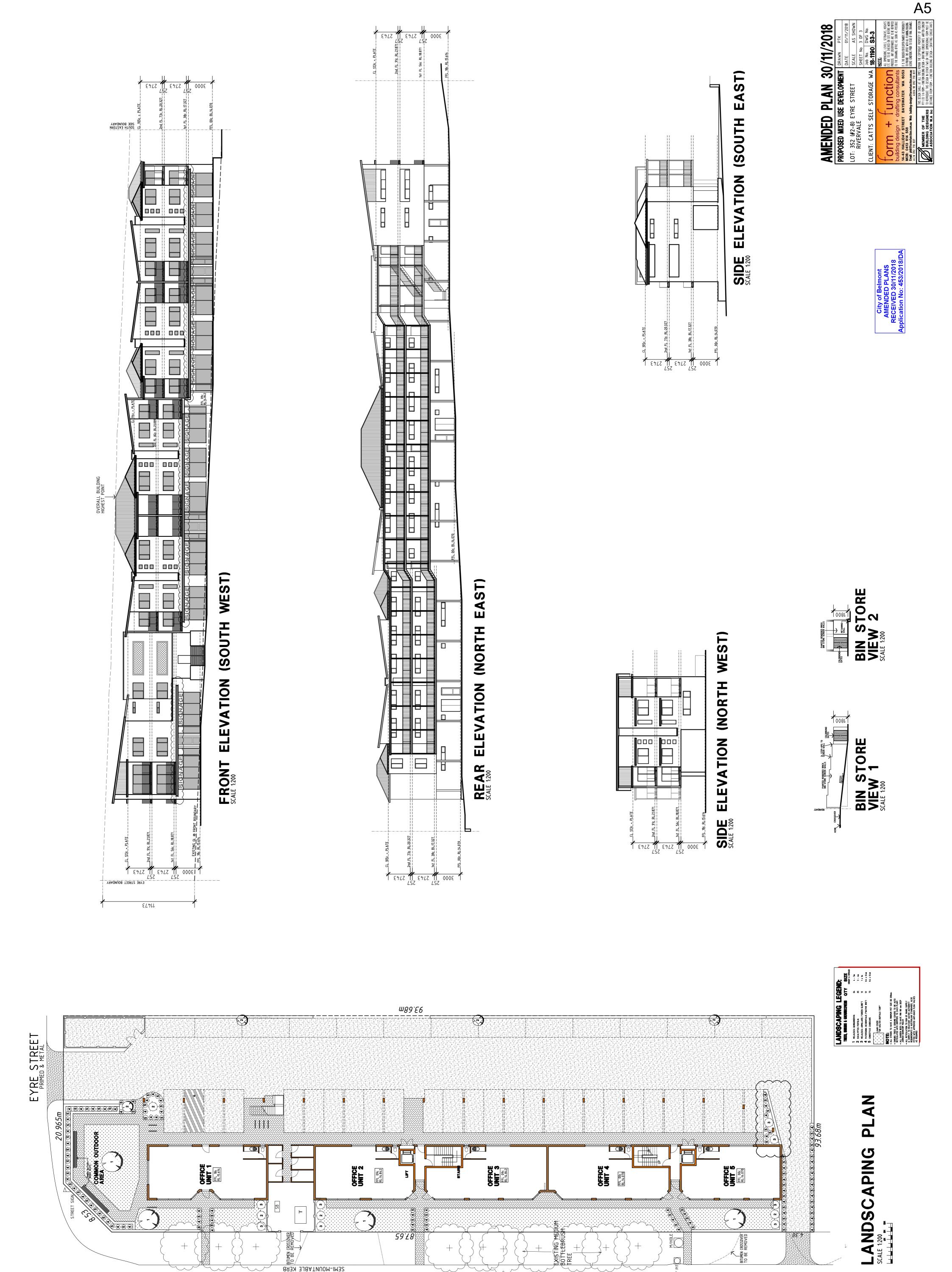
#### **Attachment 1**

Development Plans Lot 352 (2-8) Eyre Street, Rivervale









EXISTING MES BOTTLEBRUSE TREE

59.F8

KUNTSFORDS AVENUE

SEMI-MOUNTABLE KERB

BITUMEN CROSSOVER TO BE REMOVED

 $\bigcirc$ 



# Ordinary Council Meeting 26/02/19

Item 12.1 refers

**Attachment 2** 

**Schedule of Submissions** 



# CITY OF BELMONT

SCHEDULE OF SUBMISSIONS – PROPOSED MIXED USE DEVELOPMENT – 5 OFFICES AND 20 MULTIPLE DWELLINGS AT LOT 352 (2-8) EYRE STREET, RIVERVALE – (COUNCIL REF 453/2018)

7:

No.	Name & Address	Summary of Submission	Officer Comment
<del>Ĺ</del>	B. Ebsworth and C. Allard	Objection	
	Rivervale WA 6103	<ol> <li>Concerns regarding traffic flow and congestion on Eyre Street and Knutsford Avenue and the impact this will have on current residents, passers-by and businesses. Concerned traffic noise will be constant and will lead to a significant increase in the road noise for the residents.</li> </ol>	<ol> <li>Noted. Refer to Traffic and Access section of the report.</li> </ol>
		2. Concerns regarding vehicle parking for Multiple Dwellings and considers that there is currently limited street parking that is utilised by existing residents and businesses. Concerned that vehicles will be parked on the opposite side of the street, public verge areas and in front of resident's properties in their verge area, resulting in reduced road safety and congestion.	2. Noted. Refer to Car Parking section of the report.
		3. Concerns regarding the height of the proposed building. Considers that all residential dwellings along Knutsford Avenue are of a single or two-storey construction and the proposed large three-storey dwelling would not be in keeping with the aesthetic character of the area.	<ol> <li>Noted. Refer to Building Height section of the report.</li> </ol>
		4. Concerned balconies from the Multiple Dwellings facing onto Knutsford Avenue will result in overlooking into residential properties across the road and additional noise.	A. Noted.  Refer to Visual Privacy of the report. Notwithstanding, any noise generated from the site is required to comply with the Environmental Protection (Noise) Regulations 1997
		5. Concerned that if this development goes ahead this will set a precedent in the Mixed Business Zone and could lead to further highrise development submissions, impacting upon the amenity of residents.	5. Noted.  Local Planning Policy No. 10 titled Residential Land Uses within the Mixed Business Zone allows for the consideration of residential development within the Mixed Business Zone, subject to the site also incorporating some 'business' uses. Each Development Application is assessed independently on its planning merits.
2.	R. Grace	Objection	
	Rivervale WA 6103	1. Concerns regarding the height of the proposed building.	1. Noted. Defer to Officer Commont point 2 culturisaion 1
		2. Concerns regarding traffic flow and congestion.	2. Noted. Officer Comment point of submission 1.
		3. Concerns regarding car parking.	3. Noted.
		4. Concerns regarding obstruction of views to the hills.	Keler to Officer Comment point 2, submission 1.  4. Noted. Individual people and/ or properties have no ownership over specific view corridors.
3.	S. di Bona 6/16 Knutsford Avenue	Objection	
	Rivervale WA 6103	<ol> <li>Concerned regarding zoning of land and that the rezoning was not advertised.</li> </ol>	<ol> <li>Noted.         The site is not being rezoned. The zoning of the site will remain as 'Mixed Business'. The Scheme and Local Planning Policy No. 10 provide discretion for     </li> </ol>

ON	Name & Address	Summary of Submission	Officer Comment
			the proposed land uses to be considered. The suitability of the land use is considered on its planning merits.
		2. Concerns regarding traffic congestion.	2. Noted. Refer to Officer Comment point 1, submission 1.
		3. Concerns regarding impact of the proposed development on the amenity of the locality.	3. Noted. Refer to Officer Comment section of the report.
		4. Concerns regarding building height of the proposed development. Considers that residential dwellings along Knutsford Avenue are of a single or two-storey construction and the proposed large three-storey dwelling would not be in keeping with the aesthetic character of the area.	4. Noted. Refer to Officer Comment point 3, submission 1.
		5. Concerns regarding the height of the proposed building and the impact of overlooking into residential properties located across the road as a result.	5. Noted. Refer to Officer Comment point 4, submission 1.
		6. Concerns regarding loss of views to the hills.	<b>6. Noted.</b> Refer to Officer Comment point 4, submission 2.
		7. Concerns regarding car parking.	7. Noted. Refer to Officer Comment point 2, submission 1.
4	A. Gorjy 20 Knutsford Avenue	Support	
	Rivervale 6103	Considers that the development appears to be suited to the area.	<ul> <li>Noted.         There was an error in the advertising letters sent to landowners/occupiers. The first letter outlined that there was proposed to be 10 Multiple Dwellings when there are 20 Multiple Dwellings proposed. These comments received were based off the first advertising letter.     </li> </ul>

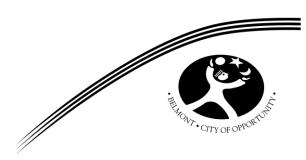


# Ordinary Council Meeting 26/02/19

Item 12.1 refers

**Attachment 3** 

# Acoustic Report by Herring Storer Acoustics





City of Belmont AMENDED PLANS RECEIVED 17/10/2018 Application No: 453/2018

#### **FORM & FUNCTION**

# 2-8 EYRE STREET, RIVERVALE MIXED USE DEVELOPMENT

**ACOUSTIC ASSESSMENT REPORT** 

OCTOBER 2018

OUR REF: 23610-1-18157



#### **DOCUMENT CONTROL PAGE**

City of Belmont AMENDED PLANS RECEIVED 17/10/2018 Application No: 453/2018

### ACOUSTIC ASSESSMENT REPORT RIVERVALE

Job No: 18157

Document Reference: 23610-1-18157

**FOR** 

#### **FORM & FUNCTION**

		DOCUMENT IN	IFODA 4 A TION			
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1	1	Attn : Phil Kerr				✓
		Email : phil@formnfun	ction.com.au			

City of Belmont AMENDED PLANS RECEIVED 17/10/2018 Application No: 453/2018

#### **CONTENTS**

1.	INTRODUCTION	1
2.	SUMMARY	1
3.	CRITERIA	1
4.	MEASUREMENTS	2
5.	ANALYSIS	2
6.	CONCLUSION	2

#### 1. INTRODUCTION

Herring Storer Acoustics was commissioned by Form & Function to undertake an acoustical assessment for the proposed development of 2-8 Eyre Street, Rivervale. The acoustical assessment is understood to have been requested to be undertake by the local council to ensure that the "requirements of the Environmental Protection (Noise) Regulations 1997" are met at the proposed apartments.

This request is in regard to noise ingress into the proposed development, hence, the query is understood to be if the existing noise environment is suitable for a residential development. It is understood that this is due to the surrounding land use consisting of both commercial and residential premises.

The details and results of the above are provided within this report.

#### 2. <u>SUMMARY</u>

Background noise levels at the proposed location of the development were measured, with the purpose of identifying any land uses in the area that may adversely affect the proposed development.

Background noise levels were dominated by the surrounding road network, with no discernible noise emissions associated with the surrounding land uses detectable.

Hence, the surrounding land uses does not impede the proposed development, in terms of noise impact, with standard constructions for the development considered appropriate to control the ambient noise environment.

#### 3. <u>CRITERIA</u>

Noise emissions from surrounding premises will need to comply with the requirements of the *Environmental Protection (Noise) Regulations 1997.* 

The Environmental Protection (Noise) Regulations 1997 stipulate the allowable noise levels at any noise sensitive premises from other premises. The allowable noise level is determined by the calculation of an influencing factor, which is added to the baseline criteria set out in Table 1 of the Regulations. At the proposed development, the influencing factor has been determined to be 6.

The assigned noise level at the various times of the day would be as listed in Table 1.

**TABLE 1 - ASSIGNED NOISE LEVELS AT NEIGHBOURING PREMISES** 

Duamiana	Time of Day	Ass	igned Noise Le	evel
Premises	Time of Day	L <sub>A10</sub>	L <sub>A1</sub>	L <sub>max</sub>
	0700 - 1900 hours - Monday to Saturday	51	61	71
	0900 - 1900 hours - Sunday & Public Holidays	46	56	71
Noise Sensitive	1900 - 2200 hours - All Days	46	56	61
	2200 - 0700 hours - Monday to Saturday	41	51	61
	2200 - 0900 hours - Sunday & Public Holidays	41	51	61

Note:

The  $L_{A10}\, noise$  level is the noise that is exceeded for 10% of the time.

The L<sub>A1</sub> noise level is the noise that is exceeded for 1% of the time.

The L<sub>Amax</sub> noise level is the maximum noise level recorded.

City of Belmont AMENDED PLANS RECEIVED 17/10/2018 Application No: 453/2018 Our ref: 23610-1-18157 2

#### 4. **MEASUREMENTS**

Noise level measurements were recorded on the proposed development site on 8th October 2018 at approximately 11:30am. The octave band data is listed in Table 4.1.

Table 4.1 – Measurement Octave Band Data

		Octave	Band Centre	Frequency (H	lz) / Noise Le	evel dB		
63	125	250	500	1K	2K	4K	8K	dB(A)
30	33	37	38	42	39	37	34	52

The noise levels recorded were associated with traffic on the road network in the area (i.e. Belmont Ave, Francisco Street and Great Eastern Highway). Noise emissions from specific commercial premises in the immediate surrounding area was not discernible – i.e. noise emissions from the surrounding premises were not discernible.

#### 5. **ANALYSIS**

Given the low background noise levels in the area, and no discernible noise emissions from the surrounding premises, standard constructions for the proposed development would be suitable, with no foreseeable issue with the surrounding land use determined to be applicable.

#### 6. CONCLUSION

Given the background noise levels in the area, the proposed development is not considered to be impeded by the surrounding land uses in terms of noise impact.

There are no additional construction measures, other than standard construction, that is recommended for the proposed development.

> **City of Belmont AMENDED PLANS RECEIVED 17/10/2018** Application No: 453/2018



# Ordinary Council Meeting 26/02/19

Item 12.1 refers

#### **Attachment 4**

# Transport Impact Statement Report by Donald Veal Consultants





August 2018 Final

2 - 8 Eyre Street, Rivervale

Prepared For: W & M Johns

Transport Impact Statement Report





#### **DOCUMENT ISSUE AUTHORISATION**

Issue	Rev	Date	Description	Prepared By	Checked By	Approved By
0	0	23/08/2018	Draft Report	SGY	DNV	DNV
1	0	24/08/2018	Final Report	SGY	DNV	DNV

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Donald Veal Consultants Pty Ltd



#### TABLE OF CONTENTS

		PAGE
1.	INTRODUCTION	1
	1.1 Background	1
	1.2 SCOPE OF THIS REPORT	
2.	EXISTING SITE CONDITIONS	2
	2.1 Location	2
	2.2 CURRENT LAND USES	3
	2.3 ACCESS ARRANGEMENTS	3
	2.4 ADJACENT ROAD NETWORK	4
	2.5 EXISTING TRAFFIC VOLUMES	4
	2.6 Crash history	5
	2.7 PLANNED CHANGES TO THE ROAD NETWORK	5
3.	PROPOSAL	6
	3.1 PROPOSED DEVELOPMENT	6
	3.2 Proposed Accesses	6
	3.3 PARKING	8
4.	TRAFFIC IMPACT	11
	4.1 TRIP GENERATION	11
	4.2 SERVICE VEHICLES	11
5.	SUSTAINABLE TRANSPORT ACCESS	12
	5.1 PEDESTRIANS AND CYCLISTS	12
	5.2 PUBLIC TRANSPORT	12
6.	SUMMARY AND CONCLUSION	13
	5.1 SUMMARY	13
	5.2 CONCLUSION	13
ΔΡ	ENDLY A DEVELOPMENT DLANG	1.4



#### 1. INTRODUCTION

#### 1.1 BACKGROUND

W & M Johns has commissioned Donald Veal Consultants to prepare this Transport Impact Statement report to support a Development Application for 20 one- and two-bedroom apartments and 5 ground floor commercial units in Rivervale.

#### 1.2 SCOPE OF THIS REPORT

The structure and scope of this Transport Statement are in accordance with Volume 4 (Individual Developments) of the Western Australian Planning Commission's Transport Impact Assessment Guidelines (2016).

#### 2. EXISTING SITE CONDITIONS

#### 2.1 LOCATION

The development site is located at the intersection of Eyre Street with Knutsford Avenue in Rivervale, and is currently part of the Catts Self Storage premises. The general locality is shown in **Figure 2.1**, with the site location shown in more detail in **Figure 2.2**.



Figure 2.1: General Locality Plan Source: Nearmap



Figure 2.2: Site Location Source: Nearmap

Application No: 453/2018

#### 2.2 CURRENT LAND USES

The subject site is presently occupied by parking areas associated with the adjacent Catts Self Storage business. See **Photo 1**.



Photo 1: The site currently forms a parking area for the self-storage units.

Other land uses in the general vicinity of the site are generally commercial to the north east of Knutsford Avenue, with residential uses to the south west.

#### 2.3 ACCESS ARRANGEMENTS



Photo 2: Current Access to the site is via the Catts Self Storage site.



The site is currently only accessible through the main entrance to the Catts Self Storage business, on Eyre Street.

#### See Photo 2.

#### 2.4 ADJACENT ROAD NETWORK

The road network adjacent the site consists primarily of Eyre Street and Knutsford Avenue. Other local streets intersecting Eyre Street and Knutsford Avenue in the general vicinity of the site include Belmont Avenue to the north east and Francisco Street to the south east.

Eyre Street is constructed as a two-way single carriageway lane, with no centreline. It links Knutsford Avenue to Belmont Avenue, and provides access to a number of commercial businesses. Knutsford Avenue has a single traffic lane in each direction, with on street parking bays on one side. It runs between Cleaver Terrace in the northwest and Francisco Street in the south west, with a short cul de sac extension south east of Francisco Street.

Both Eyre Street and Knutsford Avenue are classified as Access Roads in Main Roads WA's (MRWA) Metropolitan Road Hierarchy and are subject to the default urban speed limit of 50 km/h.

#### See Figure 2.3.



Figure 2.3: MRWA Functional Road Hierarchy

#### 2.5 **EXISTING TRAFFIC VOLUMES**

No traffic data was immediately available for either Eyre Street or Knutsford Avenue. However, all indications are that flows would be very low, with no significant capacity or congestion issues.



#### 2.6 CRASH HISTORY

The MRWA Crash Analysis Reporting System (CARS) was interrogated for crash data along both Eyre Street and Knutsford Avenue in the vicinity of the site, for the latest five year period from January 2013 to December 2017.

The crash data reveals that there have been no recorded crashes along Eyre Street during this period. One Thru/left crash was recorded at the intersection of Knutsford Avenue with Francisco Street, in 2016, which required medical treatment. A crash was also recorded on Knutsford Avenue between its intersections with Eyre Street and Francisco Street, in 2014, which involved a vehicle manoeuvring.

#### 2.7 PLANNED CHANGES TO THE ROAD NETWORK

There are no known planned changes to the road network in the immediate vicinity of the site.

#### 3. PROPOSAL

#### 3.1 PROPOSED DEVELOPMENT

The proposed development will consist of a three storey building housing 20 one and two bedroom apartments, together with 5 ground floor commercial units and associated parking. The ground floor layout is shown in **Figure 3.1**, with additional drawings attached in **Appendix A**.

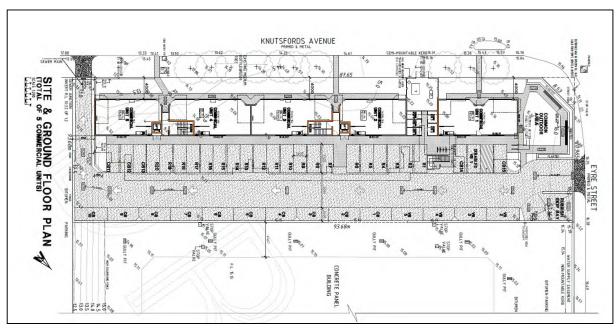


Figure 3.1: Proposed Ground Floor plan

#### 3.2 PROPOSED ACCESSES



Photo 3: Proposed vehicular access location onto Eyre Street

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Two accesses to the site are proposed, with a one way internal road connecting them. The access on Eyre Street (see **Photo 3**) is shown in the design drawings as being entry only, with the access on Knutsford Avenue (see **Photo 4**) being shown as exit only.



Photo 4: Proposed vehicular egress location onto Knutsford Avenue

The site visit carried out on Wednesday 22<sup>th</sup> August identified that there may be some visibility issues to the northeast with an egress from the site located as shown onto Knutsford Avenue, due to the proximity of parallel parking bays along this side of the road. See **Photo 5**.



Photo 5: Visibility would be limited along Knutsford Avenue to the northeast.

Client: W & M Johns

Project: 2-8 Eyre Street, TIS



No such issues were noted at the proposed access on Eyre Street, although the location is quite close to the intersection for vehicles to safely turn right into the site. See **Photo 6**.



Photo 6: Proposed access is quite close to the intersection.

If the visibility at the proposed exit cannot be improved by removing one or two on-street bays, the direction of the internal link road could instead be reversed, to allow entry only from Knutsford Avenue. This would remove the visibility issue, whilst the change to exit only onto Eyre Street would also reduce the issue of proximity to the intersection. The only obvious down side to this would be the changed direction of vehicles parking in the parallel bays within the site, with drivers' doors being adjacent the boundary wall.

#### 3.3 PARKING

The City of Belmont's Local Planning Scheme refers developers to the WAPC Residential Design Codes for parking requirements for residential developments (R-Codes), but the requirements for the commercial units would come under their scheme.

The R-codes show that, for sites within 250m of a high frequency bus route, one resident car parking space is required per dwelling, together with one visitor bay per four dwellings. The site lies within 250m of bus stops on Belmont Avenue serviced by Routes 286, 287 and 380 with a general frequency of 30 minutes. See **Figure 3.2**.

As no specific details are currently available of the likely occupants of the 5 commercial units, they have been assessed on the basis of the land use 'office'. In accordance with Table 2 of the Scheme, this land use requires 1 space for every  $30m^2$  of NLA or 1 space for each employee, whichever is greater.



This results in the residential portion of the proposed development requiring a total of 25 parking bays - 20 residential parking bays and 5 visitor bays.

The commercial element has a total NLA of approximately 448m<sup>2</sup> and will therefore require 15 parking bays.

C3.1 The following minimum number of on-site car parking spaces is to be provided for each single house, grouped dwelling and special purpose dwelling comprising the following number of bedrooms:

	Car parki	ng spaces
Type of dwelling	Location A	Location B
1 bedroom dwelling	1	1
2 + bedroom dwelling	1	2
Aged persons' dwelling	1	1
Ancillary dwelling	nil	1

#### A = within:

- 800m of a train station on a high frequency rail route, measured in a straight line from the pedestrian entry to the train station platform to any part of a lot; or
- 250m of a high frequency bus route, measured in a straight line from along any part of the route to any part of a lot.

B = not within the distances outlined in A above.

C3.2 On-site visitors car parking spaces for grouped and multiple dwelling developments provided at a rate of one space for each four dwellings, or part thereof in excess of four dwellings, served by a common access.

Figure 3.2: R-Codes parking requirements.

The ground floor layout plan for the development shows a total of 40 parking bays being provided within the site, including one ACROD bay. These include 20 designated resident bays, 15 commercial bays and 5 visitor bays, thus meeting the above requirements.

Additional parking bays are also available on-street along Knutsford Avenue. See Photo 7.

An appropriate number of bicycle bays will also need to be provided on site.

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Client: W & M Johns
Project: 2-8 Eyre Street, TIS



Photo 7: On Street parking bays are available along Knutsford Avenue adjacent the site.



#### 4. TRAFFIC IMPACT

#### 4.1 TRIP GENERATION

Based on standard published trip generation rates, the residential apartments are expected to generate a maximum of 6 daily vehicle trips, which equates to around 0.6 trips per dwelling in each weekday peak hour. This indicates that the residential element might generate around 12 trips in the PM peak hour.

The standard trip generation rate for offices is around 2 peak hour trips per 100m<sup>2</sup> GFA. This indicates that the commercial units, with a total NLA of approximately 448m<sup>2</sup>, might generate around 9 trips in the PM peak hour.

In total, therefore, the development might be expected to generate in the region of 21 trips in the peak hour.

This level of trip generation is only considered to represent a 'moderate impact', and no further technical analysis is therefore required.

#### 4.2 SERVICE VEHICLES

It is not expected that many service vehicle trips will be generated by the development, with only the weekly refuse collection truck likely to visit the site.

Bins would be kept in the enclosure near the access to the site, and may either be wheeled out onto the verge for pick up, or accessed by a private contractor in situ.



#### 5. SUSTAINABLE TRANSPORT ACCESS

#### 5.1 PEDESTRIANS AND CYCLISTS

Footpaths are provided along Francisco Street, Cleaver Street Belmont Avenue and Knutsford Avenue, but not along Eyre Street. See **Photo 8**.



Photo 8: Footpath along Knutsford Avenue opposite the site.

#### **5.2 PUBLIC TRANSPORT**

Bus routes 286, 287 and 380 run along Belmont Avenue. See Figure 5.1.

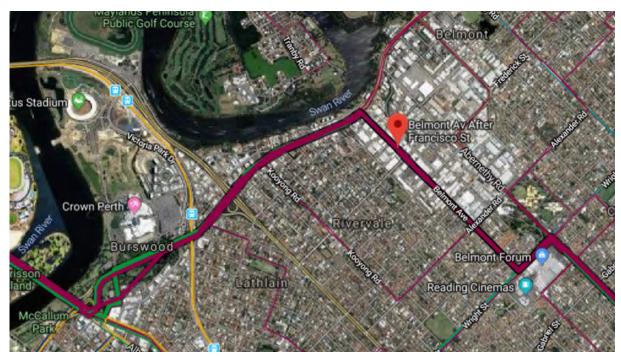


Figure 5.1: Bus routes in the vicinity of the site



#### 6. SUMMARY AND CONCLUSION

#### 6.1 SUMMARY

W & M Johns has commissioned Donald Veal Consultants to prepare this Transport Impact Statement report to support a Development Application for 20 one- and two-bedroom apartments and 5 ground floor commercial units in Rivervale. The subject site is located at the intersection of Eyre Street with Knutsford Avenue in Rivervale, and is presently occupied by parking areas associated with the adjacent Catts Self Storage business.

The City of Belmont's Local Planning Scheme refers developers to the WAPC Residential Design Codes for parking requirements for residential developments (R-Codes), but the requirements for the commercial units would come under their scheme.

This results in the residential portion of the proposed development requiring a total of 25 parking bays - 20 residential parking bays and 5 visitor bays, whilst the commercial element has a total NLA of approximately 448m² and will therefore require 15 parking bays.

The ground floor layout plan for the development shows a total of 40 parking bays being provided within the site, including one ACROD bay. These include 20 designated resident bays, 15 commercial bays and 5 visitor bays, thus meeting the above requirements. Additional parking bays are also available on-street along Knutsford Avenue.

An appropriate number of bicycle bays will also need to be provided on site.

The development is expected to generate in the region of 21 trips in the peak hour, and as such has only a moderate transport impact on the local road network.

Whilst there is currently limited visibility at the proposed egress location from the site onto Knutsford Avenue, primarily due to on street parking bays, this issue could potentially be addressed by removing one or two of these bays, or by reversing the direction of the one way internal link road.

#### 6.2 CONCLUSION

The development is not expected to generate a significant number of peak hour trips, and has sufficient parking on site to cater for the expected demand.

We therefore support the development application in terms of its traffic and road safety impact and recommend its approval.



# Ordinary Council Meeting 26/02/19

Item 12.2 refers

**Attachment 5** 

**Applicants Property Management Plan** 



#### Management Plan - Change of Use 2/21 Stanley Street Belmont

#### Introduction

In accordance with the City of Belmont Local Planning Scheme relating to change of use from residential to temporary accommodation.

- As we live in the property, it's only the spare room (Floor Plan BED 2) that is going to be utilise by the guest. There is a minimum 1 night stay and maximum stay of 1 week straight.
- Bookings will be taken and managed via the online website AirBnB. AirBnb has strict
  rules and regulations that govern both guests and owners of premises that will assist
  in the effective and efficient management of these premises. From experience guests
  are well behaved and cognoscente of their responsibilities and respect to their
  surrounding neighbours. This will be manage solely by us the owners. We will
  never let the property to guest if we are not at home.

#### **Guests**

- There is a maximum two (2) guests at any time. Infants/children are prohibited which will be indicated in the listing.
- Pets are also not allowed which will be indicated on the listing of the property in AirBnB under House Rules.
- As we are living in the same house we can ensure that there would be no un-authorised guest. Visitors are not allowed which will be indicated on the listing.
- Check- in will be from 2PM onwards and checkout will be before noon the next day.

#### Noise and Behaviour

- As we are living in the same house we can ensure that there will be minimal noise and no party/functions and/or illegal activity. This will be indicated on the listing of the property in AirBnB under House Rules. Excessive noise or disturbances to the surrounding neighbours won't be tolerated. Guests must abide by any noise abatement conditions, standards and orders issued by police or any regulatory authority to minimise impacts upon the residential amenity of neighbours and local community.
- Smoking is only allowed on the balcony and ashtray is provided in the area. This will be indicated on the listing of the property in AirBnB under House Rules.

#### **Complaints Management Procedure**

• The surrounding neighbours will each be given the contact numbers of the owner whose mobile number will be accessible 24 hours a day. Any complaint will be investigated, documented and rectified to their satisfaction.

CONTACT: Marc Sanders – 0401825524 – marcedwardsanders@gmail.com

City of Belmont AMENDED PLANS RECEIVED 17/12/2018 Application No: 572/2018/DA

#### Car Parking

- Potential guest will be informed of the car parking arrangements prior to arrival.
- The owner will outline that the potential guest is to utilise the owners own car parking bay and will not park in the visitor bays located at the front of the property at all times.
- The owner will move their vehicle from their car parking bay to park on Stanley Street when the guest requires a parking bay.
- No parking of vehicles is to occur on the verge of the property.
- Vehicles parked on Stanley Street will not obstruct bins on collection day.

#### Safety/Security

- The guests upon arrival, will be provided with emergency phone numbers and safety/evacuation procedures. Emergency information contact numbers are in form of a fridge magnet located in the kitchen. Fire alarm are in place inside the house as per existing regulations.
- Will make sure that the property is well ventilated and that temperature control is clearly
  marked and functional. As there is always one of us (owners), we will be able to show
  the guest proper use of electrical appliances e.g. use of stoves and heaters, fan.
- First aid kit are also available in their room.
- Clear marked fire escape route are printed in their room. All access are free from hazards. Stairs are safe and have railings which meets the safety guidelines when it was built. These are maintained by our strata.

#### Rubbish/Waste Disposal

- There is one bin inside the guest room and one in the toilet which will be emptied out by us (owners) after each guest checkout. There are two bins in the kitchen area – one for recycling and one for general waste which are regularly disposed by us. Guests will be encouraged to recycle where possible
- Rubbish and recycling will be placed in the relevant bins outside by the owner. On
  collection day the owner will take out the general waste and recycling bins for collection
  and then once the rubbish has been collected, bring the bins back in on the same day.

#### **Property Maintenance**

- The garden is being kept neat and orderly by the gardeners hired by our strata.
- Any damage occurred by the guest will be dealt with us.
- We'll ensure we have adequate liability coverage as well as property protection.



# Ordinary Council Meeting 26/02/19

Item 12.2 refers

**Attachment 6** 

**Schedule of Public Submissions** 



# CITY OF BELMONT SCHEDULE OF SUBMISSIONS – Proposed Bed and Breakfast – Lot 26 (2/21 Stanley Street, Belmont – (Council Ref 572/2018)

Ö	Name & Address	Summary of Submission	Officer Comment
<del>/</del> -	J. Basco	Objection	
	Belmont WA 6104	elation to the proliferation	1. Noted.
		accommodation operators within belmont. Considers that there is ample legitimate short-term accommodation in the area in the form of hotels and motels. Concerns in relation to people operating short-term accommodation without approval	Officers are investigating short-term accommodation sites that are operating without the appropriate development approval in place that have been bought to our attention.
		infoughout bemont.	A Bed and Breakfast is a 'D' Discretionary Use within the Residential Zone under the City of Belmont Local Planning Scheme No. 15 (LPS 15). A 'D' Discretionary Use means that the use can be considered subject to compliance with the provisions of LPS 15.
		2. Considers that the proposed use will adversely impact on the	2. Noted.
			The Property Management Plan outlines multiple rules which are expected to limit any unreasonable noise and disturbance from the premises including:
			- Not permitting any parties/functions.
			- Outlining that excessive noise or disturbances to the surrounding neighbours will not be tolerated.
			- Requiring guests to abide by noise conditions and standards set by Police.
			Noise from the premises will be required to comply with the Environmental Protection (Noise) Regulations 1997.
			In addition, if the application is approved, a condition is recommended not permitting guest check-in between the hours of 10:00pm and 7:00am on any day and requiring the operator to greet guests on arrival to explain the house rules and Property Management Plan.
		3. Concerned that the proposed use will impact on personal safety and security of residents with strangers accessing	3. Noted.
			The Property Management Plan outlines that the property will not be tenanted to guests if the owners are not home. In addition, the Property Management Plan outlines that disturbances to surrounding neighbours will not be tolerated. The Property

No.	Name & Address	Summary of Submission	Officer Comment
			Management Plan includes the owner's telephone number and email address, which will be accessible 24 hours a day, with every complaint investigated, documented and rectified to the complainants satisfaction.
		4. There is no proper legislation in place to govern these businesses and considers that strict standards should be	4. Noted.
			There is currently a Parliamentary Inquiry occurring in relation to the suitability of short-stay accommodation. In light of this, if approval is granted for the 'Bed and Breakfast use it should only be on a temporary basis (e.g. 12 months). Once this temporary approval period expires, if the landowner wishes to continue with the operation of the 'Bed and Breakfast' a new application for development approval will be required to be submitted. The application will be considered under the provisions for short-stay accommodation at that time.
		5. Concerns regarding how the land use will be run and the	5. Noted.
		וווף שני טו נוופ שווופווון שוום טומומטפים טו נוופ וופוטווטטעוווטטע.	The use is not proposing alterations and additions to the facade of
		Concerns regarding the responsibility and accountability of the applicant. Considers that planning approval should not be granted until state legislation is introduced that details	the dwelling, therefore not detracting from the residential character of the area.
		insurance requirements.	The Property Management Plan is required to be implemented and adhered to at all time, with the responsibility placed wholly on
		Concerns regarding the applicant not having to provide evidence of insurance covers in place.	the landowners to ensure this is occurring.
			In relation to insurance, this is not a relevant planning
		Concerns in relation to the potential damage to common property areas as a result of the proposed use and the potential financial burden of owners within the strata.	consideration and is for the owner to determine. With this being said the Property Management Plan outlines that any damage done by a guest will be dealt with by the owner and that the owner will ensure that they have adequate liability coverage as well as property protection in place.
		6. Outlines that the strata's September AGM voted to prohibit short-term accommodation through a bylaw. Five owners voted to approve the bylaw, two voted against the bylaw and one did not vote. The majority of owners were not in favour of short-term accommodation usage at the complex, however due to a technicality, the bylaw was not passed.	6. Noted.
		7. Considers short-term renters would not have the same level	7. Noted.
		have for their own homes and neighbours. Also considers that little can be done when a short-term guest cause's damage to a property.	The Property Management Plan outlines multiple rules which are expected to limit any unreasonable noise and disturbance from the premises. In addition, the Property Management Plan outlines

		Summary	
SO.	Name & Address	of Submission	Officer Comment
			that any damage done by a guest will be dealt with by the owner and that the owner will ensure that they have adequate liability coverage as well as property protection in place.
		8. Concerns regarding the management of car parking and one of the owners utilising a visitor car parking have to park a	8. Noted.
			The Property Management Plan outlines that the visitor car parking bays are not to be utilised by potential tenants of the 'Bed and Breakfast'. The owner utilising the visitor car parking bays to park their own vehicle is a Strata issue.
		Concerns regarding visitors parking on the lawn and/or verge. In addition, considers that excess vehicles should be parked on the road and should not block the collection of rubbish bins on collection day.	The Property Management Plan outlines that the owner will park their vehicle on Stanley Street. The owner will not be parking a vehicle on the verge as outlined in the Property Management Plan. In addition the Property Management Plan outlines that cars parked on Stanley Street are not permitted to obstruct bins from being collected on collection day.
		9. Concerns regarding the owner leaving his bins out on the front lawn after collection day.	9. Noted.
			The Property Management Plan outlines that the owner will place rubbish and recycling in the relevant bins and on collection day take out the bins for collection and bring the bins back in on the same day.
2.	T. H. Le 17 Stanley Street	Neither Support or Object	
	Belmont WA 6104	1. Concerns that the balcony of the subject site is overlooking into the neighbouring property.	1. Noted.
			The subject balcony is setback in excess of the 7.5m required under the Residential Design Codes and as a result the balcony is not required to contain visual privacy screening.
<sub>6</sub> .	C. Sen 7/21 Stapley Street	Object	
	Belmont WA 6104	1. Concerns in relation to the applicant not having to provide	1. Noted.
		ace. billity	Refer to Officer Comments for Submission 1. (Objections 4, 5 and 7).
		the applicant and in relation to the potential damage to common property areas as a result of the proposed use and the financial burden of owners within the strata. Considers that there is no proper legislation in place to govern these	
		businesses and considers that strict standards should be applied to operators and that these should be strictly enforced.	

	Name & Address	Summary of Submission	Officer Comment
		2. Concerns regarding safety, security, noise, anti-social hebayiour and damage to common property associated with	2. Noted.
		the proposed land use. In addition, concerned about the impact on the amenity of residents as a result of the proposed land use.	Refer to Officer Comment for Submission 1. (Objections 2 and 3).
		3. Outlines that the strata's September AGM voted to prohibit short-term accommodation through a bylaw. Five owners voted to approve the bylaw, two voted against the bylaw and one did not vote. The majority of owners were not in favour of short-term accommodation usage at the complex, however	3. Noted.
		due to a technicality, the bylaw was not passed.  4. Concerned about the suitability of the proposed land use and the proliferation of short-term accommodation operators within Belmont. Considers that there is ample legitimate short-term accommodation in the area in the form of hotels	4. Noted. Refer to Officer Comment for Submission 1. (Objection 1).
A Ø B	A. Moylan 6/21 Stanley Street Belmont WA 6104	and motels.  Object  1. Concerned about the proliferation of short-term accommodation operators within Belmont. Considers that there is ample legitimate short-term accommodation in the area in the form of hotels and motels.	1. Noted. Refer to Officer Comment for Submission 1. (Objection 1).



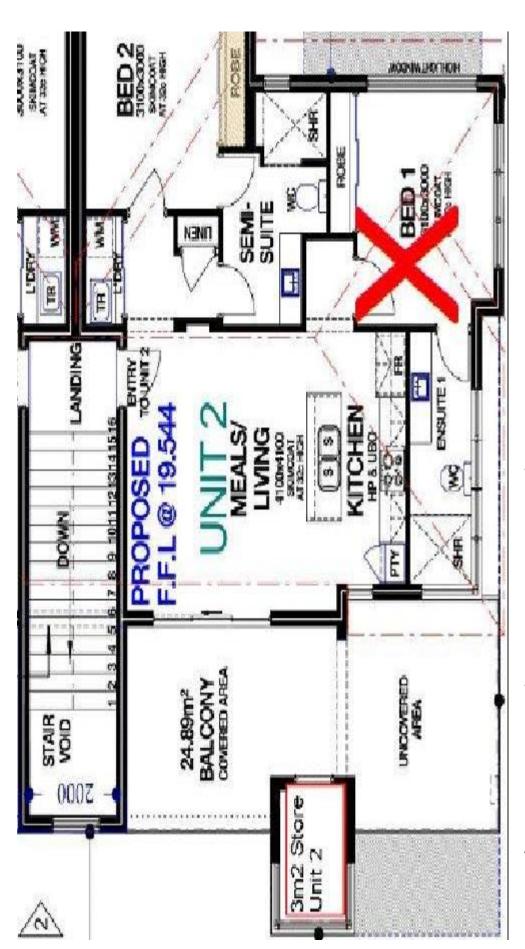
# Ordinary Council Meeting 26/02/19

Item 12.2 refers

**Attachment 7** 

Development Plans - Lot 26 (2/21) Stanley Street Belmont





Floor Plan – 2/21 Stanley Street Belmont (Marked X is not accessible to guest)

L 26 999m<sup>2</sup>

# PROPOSED MVG DEVELOPMENT LOT 26 (#21) STANLEY STREET, BELMONT





BED 1

178A ST BRIGIDS TCE, SCARBOROUGH M - 0407 287 833 E - info@purebaseliving.com.au

SCALE: 1: 200 DATE: 30.08.13

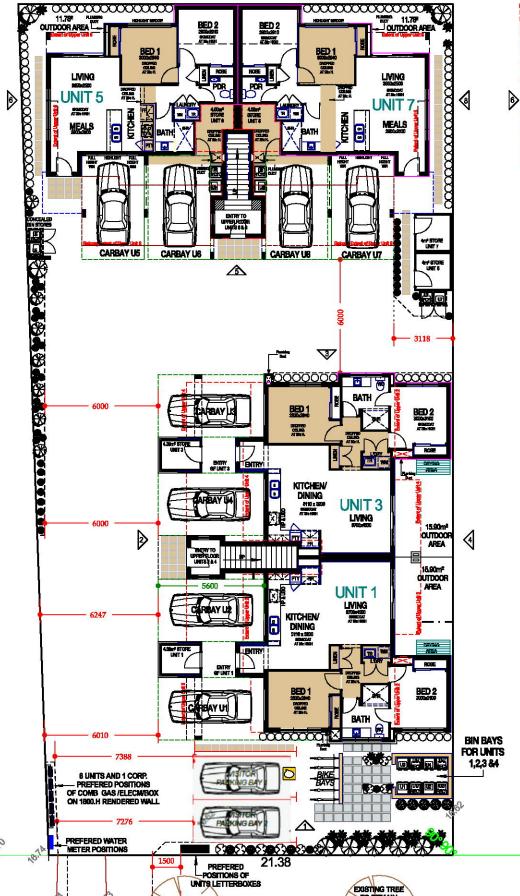


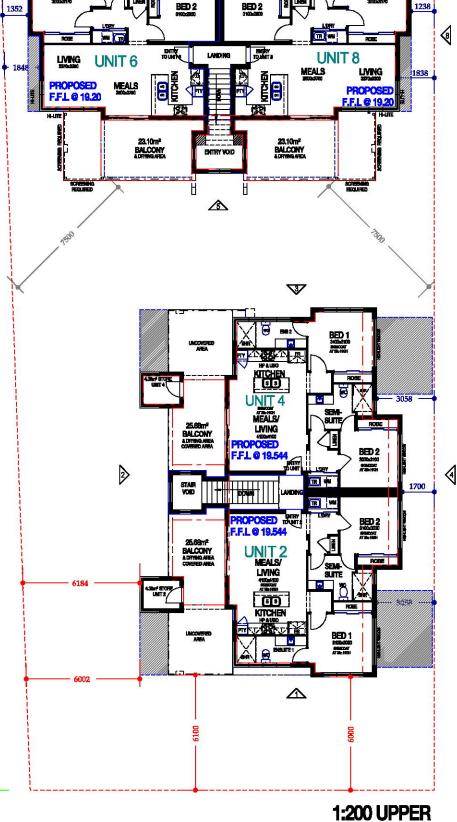




BED 1

UNIT 8 - U.F. AREAS	
UPPER FL	70.95 m²
STORE	4.00 m²
CARPORT:	18.15 m²
TOTAL AREA	93,18m²
BALCONY:	23.10 m²
UNIT PERIM:	38.00m
INTERNAL AREA:	64.00 m²





# UNIT 2 - U.F. AREAS

UPPEKFL	76.67 m <sup>2</sup>
STORE:	4.39 m²
CARPORT:	18.48 m²
TOTAL AREA	99.54 m²
BALCONY:	25.68 m²
UNIT PERIM:	38.29m
INTERNAL AREA:	68.00 m²

UNIT	4-L	J.F.	AR	EAS

UPPER FL.	76.67 m²
STORE	4.39 m²
CARPORT:	18.48 m²
TOTAL AREA	99.54 m²
BALCONY:	25.68 m²
UNIT PERIM:	38.29m
INTERNAL AREA:	68.00 m²

**FLOOR PLANS** 

1:200

**FLOORPLANS City of Belmont RECEIVED 06/11/2018** Application No: 572/2018

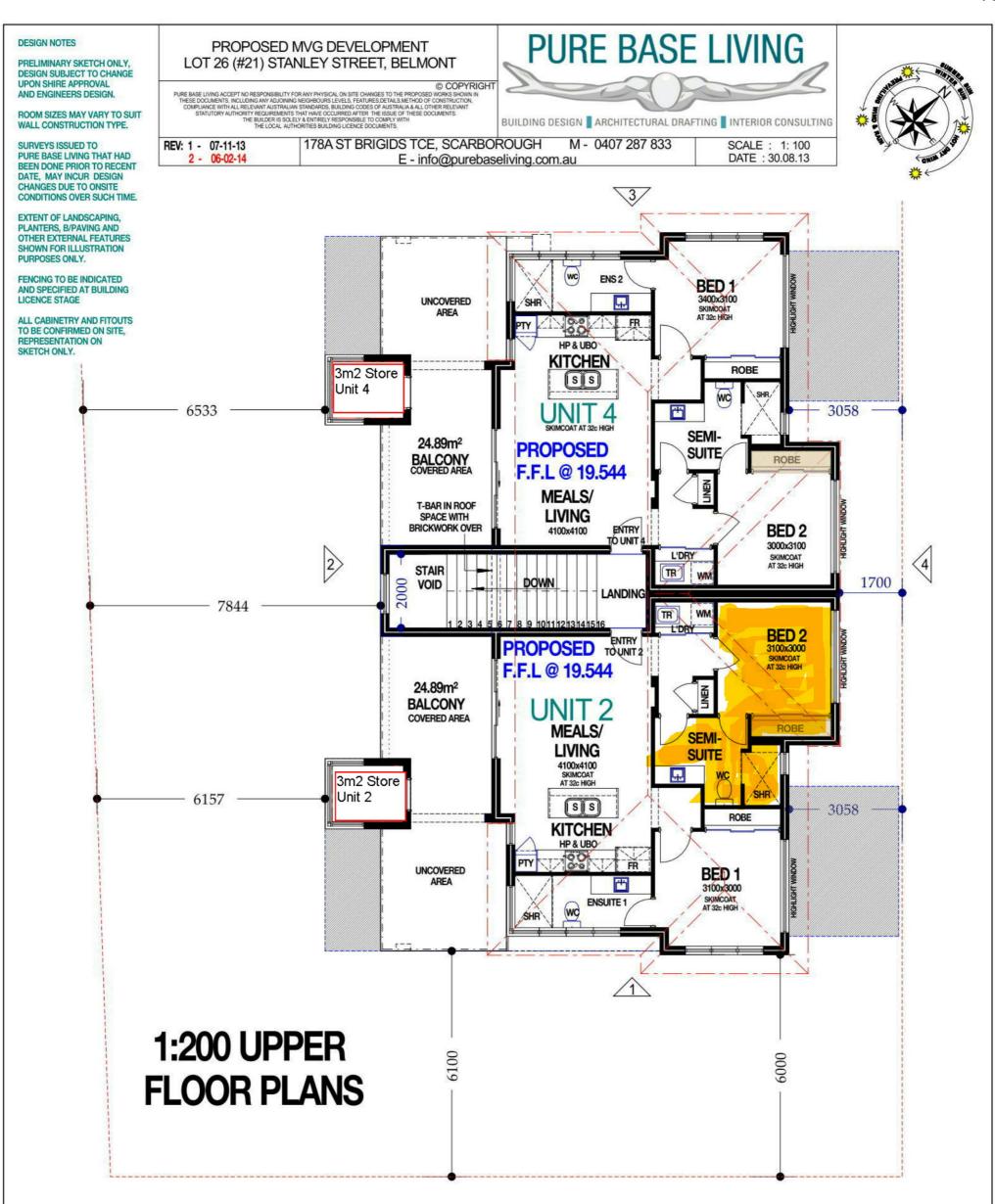
# UNIT 1 - G.F. AREAS

GROUND FL	79.72 m²
STORE	4.39 m²
CARPORT:	18.48 m²
TOTAL AREA	102.59 m²
COURTYARD:	15.90 m²
UNIT PERIM:	42.29m
INTERNAL AREA:	70.00 m²

# UNIT 3 - G.F. AREAS

**1:200 GROUND FLOOR PLANS** 

GROUND FL	79.72 m²
STORE:	4.39 m²
CARPORT:	18.48 m²
TOTAL AREA	102.59 m²
COURTYARD:	15.90 m²
UNIT PERIM:	42.29m
INTERNAL AREA:	70.00 m²



City of Belmont RECEIVED 06/11/2018 Application No: 572/2018

# UNIT 2 - U.F. AREAS

UPPER FL	76.00 m <sup>2</sup>
STORE:	4.39 m <sup>2</sup>
CARPORT:	18.48 m <sup>2</sup>
TOTAL AREA	98.87 m <sup>2</sup>
BALCONY:	24.89 m²
UNIT PERIM:	38.29m
INTERNAL AREA:	68.00 m <sup>2</sup>

# UNIT 4 - U.F. AREAS

UPPER FL	76.00 m <sup>2</sup>
STORE:	4.39 m <sup>2</sup>
CARPORT:	18.48 m <sup>2</sup>
TOTAL AREA	98.87m <sup>2</sup>
BALCONY:	24.89m²
UNIT PERIM:	38.29m
INTERNAL AREA:	68.00 m <sup>2</sup>

REAR - UNIT 2 AND UNIT 4 -(UPPER LEVEL) FLOOR PLAN



# Ordinary Council Meeting 26/02/19

Item 12.3 refers

# **Attachment 8**

# Applicant's Supporting Information Cover Letter and Developments Plans





# **Application for Development Approval**

# **Change of Use**

# 89 Abernethy Road, Belmont Western Australia

**Applicant** 

**Brilliant Detailing** 

79 Abernethy Road Belmont W.A. 6104

**Karl Pratley** 

08 6162 9774



# **Details of Operation**

Company Brilliant Detailing

The applicant (*Brilliant Detailing*) is in the process of making an application to secure the subject property under a commercial lease agreement. Subject to that proposed lease agreement, a change of use may be required to confirm the operation meets the requirements under your local planning scheme 15. The business profile aligns itself under a motor vehicle repair operation noted which is noted as a "discretionary use" according to Table 1 – Zoning Table – LPS 15. The vast majority of the works carried out involving the removal of vehicle interiors to clean and repair, carry out vinyl and seats repairs, repairs to roof linings and all plastic components inside the vehicle. The applicant also restores vehicles headlights, are an Agent for Toughseal paint and interior protection for new vehicles and offer a window tinting facility as require.

The second main area of the operation is to accept the delivery of vehicles from Eastern states vehicle dealers on behalf of their new owners, detail and deliver cars when completed. On some occasions the applicant will store vehicles for these customers for a period. On occasions they will carry out minor repairs and paint touch ups as required during the refinishing process. Finally, the refinishing process involves having to hand wash the outside of the vehicles which equates to only a minor part of the whole process.

The applicant is already under operation at 79 Abernathy Road, Belmont under the same zoning of Mixed Business according to the provisions of your Local Planning Scheme 15.

The premises at 89 Abernathy Road offers a large 1,088m2 (approx.) building located on a 1,822m2 (approx.) land holding. The property offers ample office space and parking for 12 vehicles, roller door access leading to the rear workshop and plenty of internal workshop space for the repair, refinishing and storage of the vehicles.

Opening hours - Monday to Friday 8.00am-5.00pm

Saturday 8.00am-3.30pm

Number of employees - 4 to 6 staff working during the hours of operation.

Customer attendance - Expect 2 to 3 customers ped day

Dropping off cars between 8.00am to 9.30am Collecting cars between 2.30pm to 5.00pm.

Equipment to be used - Machine polisher, water pressure washer, air compressor.

Deliver of supplies - Delivery is scheduled Tuesday between 10.00am/12.00 noon.

Trade water discharge - The subject site is currently fitted with a quad tank water

catchment and filtration system that meets the requirement under a trade water discharge permit. The applicant is in possession of a transferrable permit from the WaterCorp. for the discharging of trade water into deep sewerage.



# **Description of the land**

Street address - 89 Abernathy Road, Belmont W.A.

Lot - 83

Diagram plan - 42500

Vol/Fol - 2208 / 567

Land area - 1821m2 (as per certificate of title)

# Improvements to the land

Office area - 36m2 (approx.) as per plan.

Factory area - 642m2 (approx.) as per plan.

Storage - 78m2 (approx.) as per plan.

Total area (internal) - 756m2 (approx.) as per plan.

Total site coverage - 1088m2 (approx.)

F.F.L. - F.F. Level at RL 12.32 as per plan.

Trade water filtration - Quad tank water catchment and filtration system installed.

Staff facilities - Male & female ablutions, lunchroom and changeroom.

Fencing - Site is completely enclosed on all sides buy neighbouring

buildings and external structures.

No. of car parking bays - 16

Dimensions of parking bays - Depth 5.5m

Width is a combination of 2.5 and 2.75m bays

Pedestrian access - Via visitors carpark to office.

Consumables delivery location - Via visitor parking area.

Bin storage - Bins stored in suitable location in open area next to rear

staff carpark.



# **Vehicle Movements Within Property**

# **Delivery of vehicles**

### **Vehicles delivered by staff:**

Generally, the site receives 3-4 vehicles a day which are driven to and from the site by staff where the vehicles are picked up from the client's location or at the delivery depot for vehicles transported from the Eastern states. The staff deliver the vehicle to the delivery bay through the main rolla door access and under the undercover area in a forward's motion to await assignment to the appropriate bay dependent on what services are required and scheduled.

\*Car carrier transporter trucks are not utilised to delivery or pick up vehicles from the site.

### Vehicles delivered by client:

Vehicles are generally picked up by staff and driven to the site however, on the occasions where a vehicle is delivered by a client, the vehicle is to be parked into the visitors parking bays located at the front of the site. Keys are to be handed over to staff in the reception area as noted on the site plan. Staff will then move the vehicle to the delivery bay through the main rolla door access and under the undercover area in a forward's motion to await assignment to the appropriate bay dependent on what services are required and scheduled.

# Vehicles to be received from Eastern states:

Vehicles to be received from the Eastern states will be picked up at the delivery location of the transport company by staff and driven to the site. Staff will then move the vehicle in to the delivery bay through the main rolla door access point, through the undercover area to await assignment to the appropriate bay depending on what service is required and scheduled.

# Pickup of vehicles

# Vehicles to be picked up by client:

Generally, vehicles are delivered to clients directly however on the odd occasion a client is to pick up the vehicle onsite, the vehicle will be driven up to up to the waiting client by staff in a forward's motion exiting the workshop through the rear rolla-door, through the open area, through the undercover area and through the main rolla-door where the client will enter the vehicle and exit the property in a forward motion via the driveway and crossover.

# **Visitor Parking**

# **Visitor parking movements**

City of Belmont RECEIVED 14/11/2018 Application No: 567/2018

Generally, a low volume of visitors are received as the vast majority of the vehicles that are deliveries to and from the site are carried out by staff. A visitor utilising the visitor parking area will entre through the crossover in a forward's motion and through the manoeuvring aisle adjacent to the visitor parking area and park in any of the bays allocated. To exit, the visitor will reverse into the manoeuvring aisle, then drive through in a forward's motion and exit the property via the driveway and crossover.



# Manoeuvring of vehicles on site

# Manoeuvring of vehicles to be repaired:

Internally within the workshop.

Vehicles to be repaired or detailed will be moved by staff from the delivery bay into a particular service bay dependent on what service is required and scheduled. If multiple services are required, the vehicle will be manoeuvred from bay to bay by the staff member responsible for that service. The vehicles may be moved either by driving the vehicle or manually rolling the vehicle as some activities may require the disconnection of the vehicle's battery or other mechanical components for a short period. Generally, each of the vehicles will remain onsite for 1-2 days.

Externally to wash bays.

Vehicles that require a washdown will be moved by staff from either the delivery bay or service bay to the washdown area. The vehicle will exit the workshop through the rear rolla-door and drive through the open area into the wash bay in a forward's motion. To exit the wash bays, the staff will utilise the open area to reverse into and then entre the workshop in a forward's motion and deliver the vehicle back to either the deliver bay or service bay dependent on what service is scheduled.

## **Staff vehicle movements:**

Staff Parking Area – Front

Staff to park in the bays allocated at the front of the property will enter the property and drive through the open area fronting the visitor parking bays in a forward's motion and park the vehicles in one of two bays provided. To exit the parking area the staff will reverse into the manoeuvring aisle to be able to change direction and exit the parking area safely in a forward's motion and exit via the driveway and crossover.

Staff Parking Area - Rear

Staff to park in the bays allocated at the rear of the property will enter the property through the crossover, through the main rolla-door, drive under the undercover area in a forward's motion and park their vehicle in one of the allocated staff parking bays. To exit, the staff will utilise either the open area or manoeuvring aisle indicated on the site plan to reverse into and drive out in a forward's motion through the undercover area, through the rolla-door and exit the property via the driveway and crossover.

# **Delivery movements:**

Delivery vans/utilities - consumables

Delivery vans/utilities with consumables/parts may park in a visitor's bay located at the front of the property and will be required to attend reception. Movement of any consumables/parts through the property will generally be carried out by staff members either internally through reception or utilising the main rolla-door access and the undercover area as detailed on the site plan.



# Volume of vehicles being repaired onsite:

The volume of vehicles being repaired on site at any given time are low and range between 2 to a maximum of 8 vehicles.

On an average the operation will see around 5 vehicles per day on site at the property.

# **Short term storage of vehicles:**

Short term storage offers a maximum of 2 vehicles only.

The dimensions of the parking bays are approximately 2.75m (w) x 5.50m (d)

Given the dimensions of the area allocated on the site plan, this will offer enough space around and between the two vehicles so as to avoid any prospects of damage.

# Types of vehicles being repaired/stored onsite:

The vast majority of vehicles being repaired onsite are passenger and light commercial type vehicles. These vehicles are representative of the Australian Standards types as detailed below.

AS2890.1 (B85 type) - Width 1870mm
Passenger vehicle - Length 4910mm

Minimum turning circle (kerb to kerb) 11.5m

AS2890.1 (B99 type)

station wagon/light commercial - Width 1940mm

- Length 5200mm

- Minimum turning circle (kerb to kerb) 12.5m

On occasions the operation may carry out works on light duty trucks of between 3 to 5 tonne weight. The operation will see a very low volume of this vehicle type in the vicinity of only 1 vehicle per month. The light duty truck type is representative of a Fuso Canter or Fuso Pantech style delivery truck with representative dimensions as detailed below and diagram overleaf.

Light duty truck - Width 1750mm (as per diagram overleaf)

Length 5960mm (as per diagram overleaf)Fuso Canter turning circle (kerb to kerb) 8.8m

# **Delivery vehicles - consumables**

On occasions that the operation requires local deliveries of consumables, a light commercial sized vehicle would generally be expected from suppliers. These vehicles are representative of the Australian Standards type as detailed below.

AS2890.1 (B99 type)

station wagon/light commercial - Width 1940mm

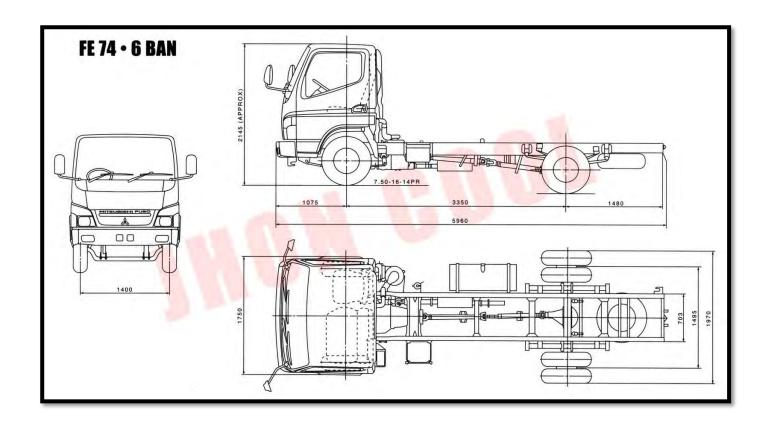
- Length 5200mm

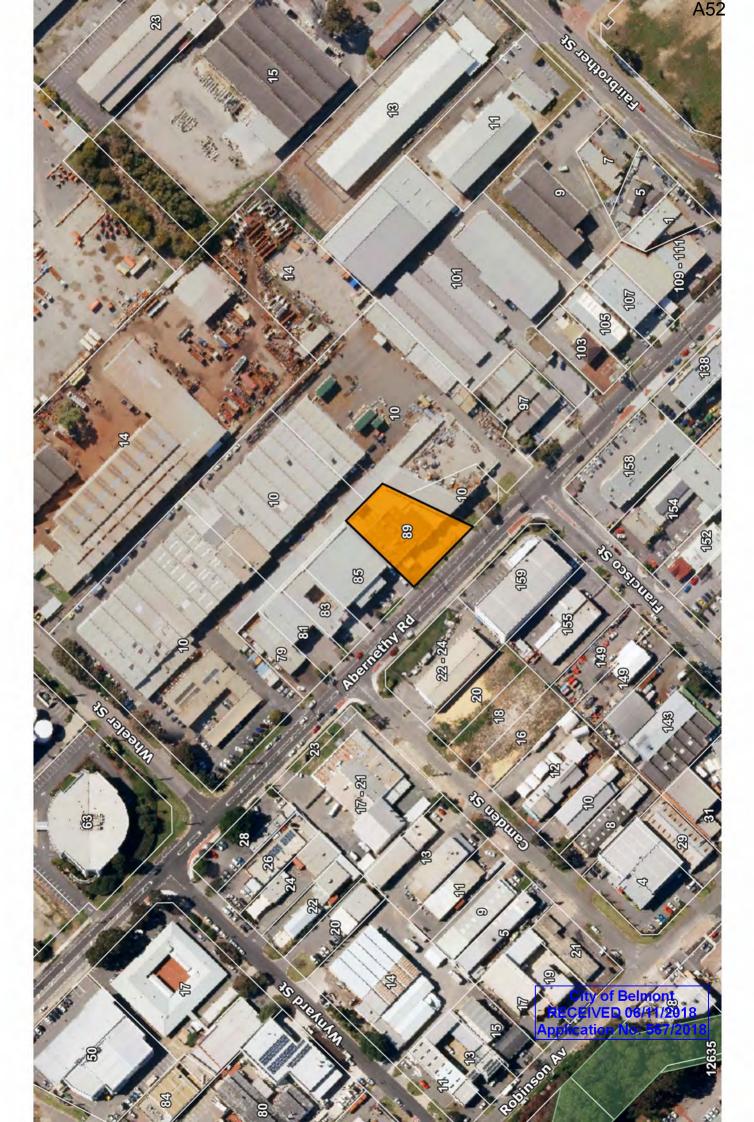
- Minimum turning circle (kerb to kerb) 12.5m

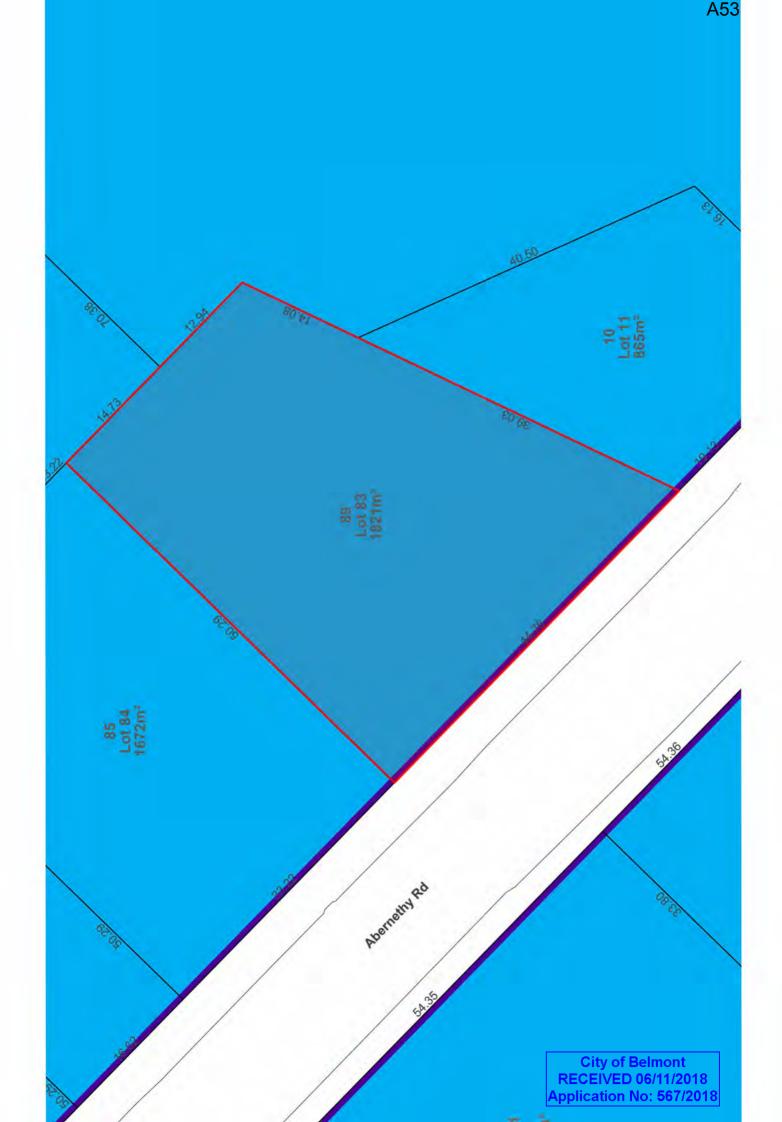


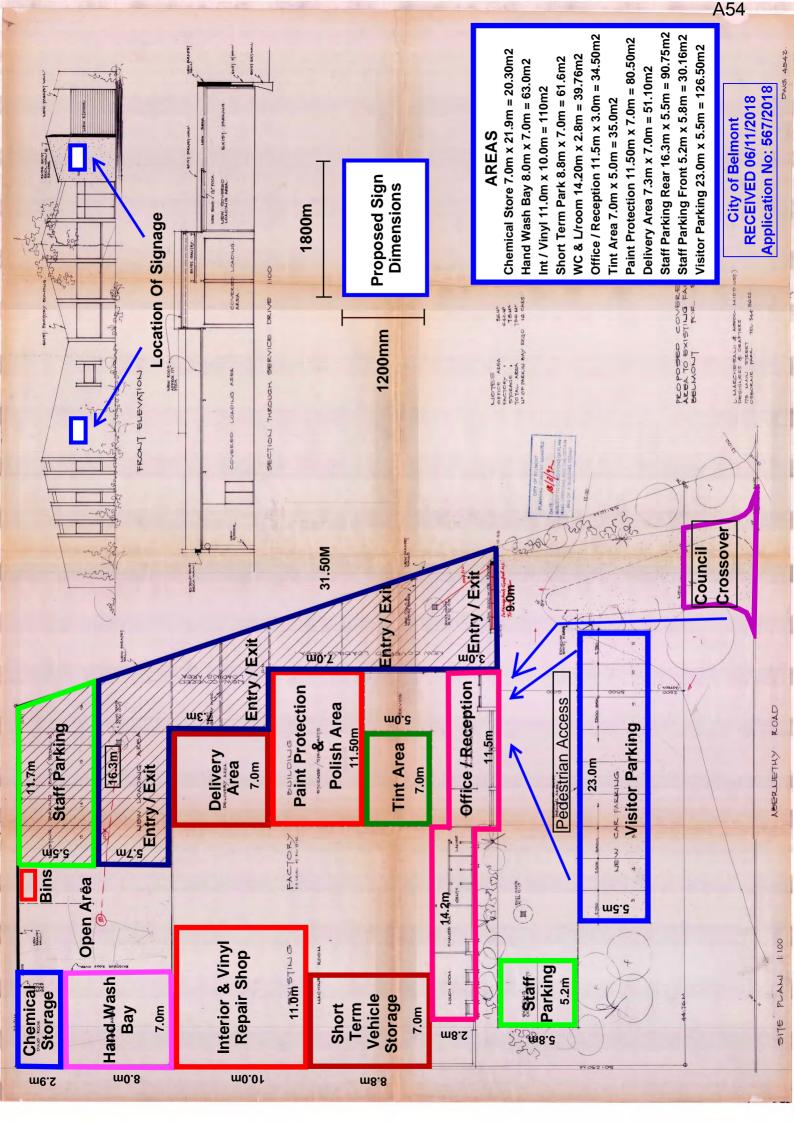
# **Representative Light Duty Truck Dimensions**

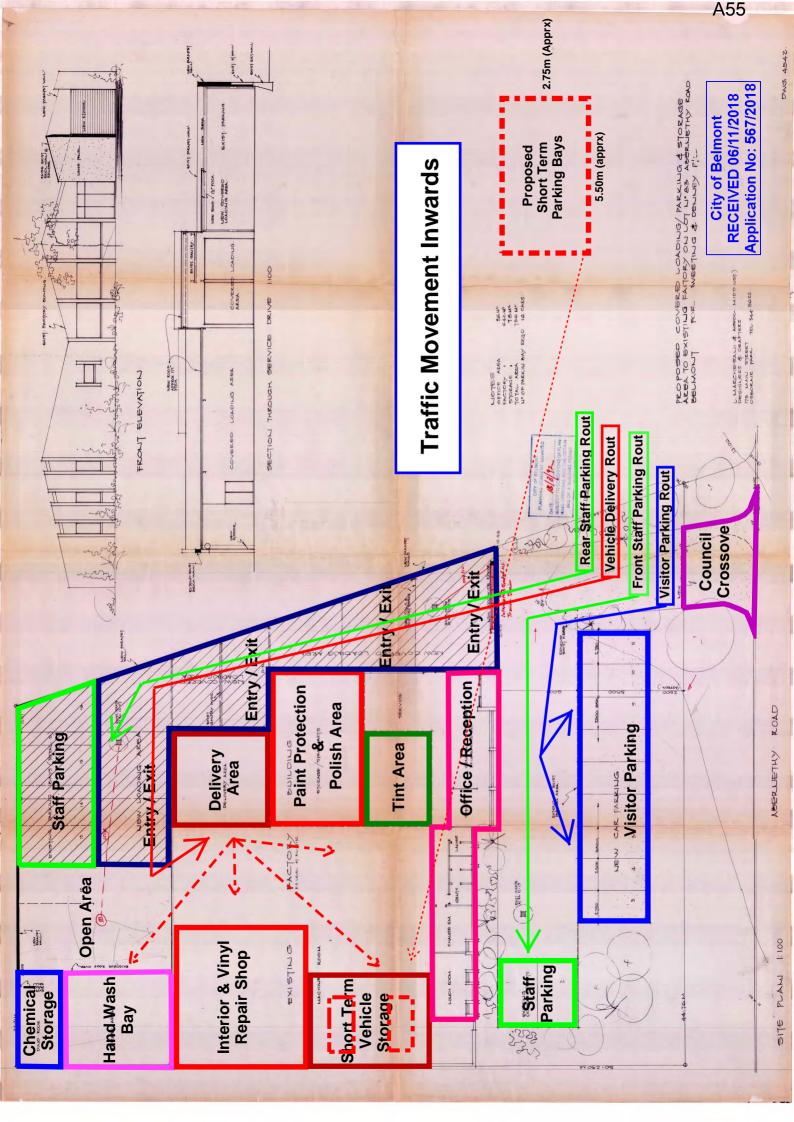
Mitsubishi Fuso Canter Light Duty Truck











# ADDITIONAL INFORMATION SHEET FOR ADVERTISEMENT APPROVAL (to be completed in addition to Application for Planning Approval)

Note: to be completed in addition to the Application for Planning Approval form

110	escription of property upon which advertisement is to be displayed including full details of proposed position within that property:  By Abernathy Road, Belmont Western Australia
	Lot 87, Deposited Plan , Volume , Folio
	tails of proposed sign:
(a)	Type of structure on which advertisement is to be erected (i.e. freestanding, wall mounted, other): _2_signs on walls of front elevation.
(b)	Height: 1200mm Width: 1800mm Depth: 0.00
(c)	Colours to be used: BLUE BLACK & SILLUR ON WHITE BACK
(d)	Height above ground level:
	(to top of advertisement): 3000mm (approx)
	• (to underside): 1200mm (approx)
(e)	Materials to be used: METAL
Illur	ninated: Yes (No)
	es, state whether steady, moving, flashing, alternating, digital, animated or scintillating state intensity of light source:
	etate interiory of right source.
Per To	od of time for which advertisement is required: be in situ until cessation of the lease and any option periods.
	ails of signs (if any) to be removed if this application is approved:
	1
Note	This application should be supported by a photograph or photographs of the
	premises showing superimposed thereon the proposed position for the advertisement and those advertisements to be removed detailed in 4 above.
	ature of advertiser(s):  fferent from land owners)
	: 1: 11: 2018 City of Belmon
	City of Polmor

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# DETAILING

Toughse

Car Pain

Protectio

Specialising in preparing cars for sale

Application No: 567/2018 RECEIVED 06/11/2018

# 1300225

www.brilliantdetailing.com.au



# Ordinary Council Meeting 26/02/19

Item 12.3 refers

**Attachment 9** 

# Schedule of Public Submissions Lot 83 (89) Abernethy Road Belmont



# SCHEDULE OF SUBMISSIONS – Proposed Use Not Listed – Motor Vehicle Detailing – Lot 83 (89 Abernethy Road, Belmont – (Council Ref 567/2018)) CITY OF BELMONT

Š.	Name & Address	Summary	Officer Comment
<del>-</del> -	E & I Fernandez	Objection	Noted.
	85 - 87 Abernetny Koad Belmont WA 6104	1. Our objection is in relation to noise. Currently Brilliant Detailing are operating from 79 Abernethy Rd. We have noted that if	The operations of the business are proposed to be enclosed within the building thereby absorbing any potential noise. The
		you are parked on the verge (quite a distance away from their roller door opening) you can distinctively hear water gushing, pressure cleaners, etc.	Road, Belmont is similar to a Motor Vehicle Repair (mechanic) business, which is a 'D' – Discretionary land use in the 'Mixed
			Business' zone. In any event, noise emissions from the premises must comply with the <i>Environmental Protection (Noise)</i> Regulations 1996.
			It is therefore considered that the objection can be adequately contained within the built structure and the imposition of an advice note that ensures the applicant complies with the relevant noise
			regulations.
5.	P. Stubber 159 Francisco Street	Neither Support or Object	Noted.
	Belmont WA 6104	"Paint & Polish of Vehicles" If they have spray painting adequate and approved booth is required to prevent fumes entering atmosphere.	The Applicant has advised that the spray painting does not form part of this application or their operations. The City has previously approved a Spray Booth addition in 2008, however the approval was never acted upon. The concern raised by the adjoining neighbour has been noted. As an additional footnote, the Applicant will be advised that spray painting is not to occur onsite without first obtaining Development Approval.

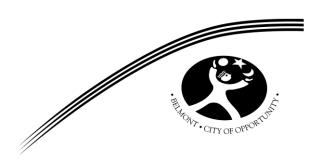


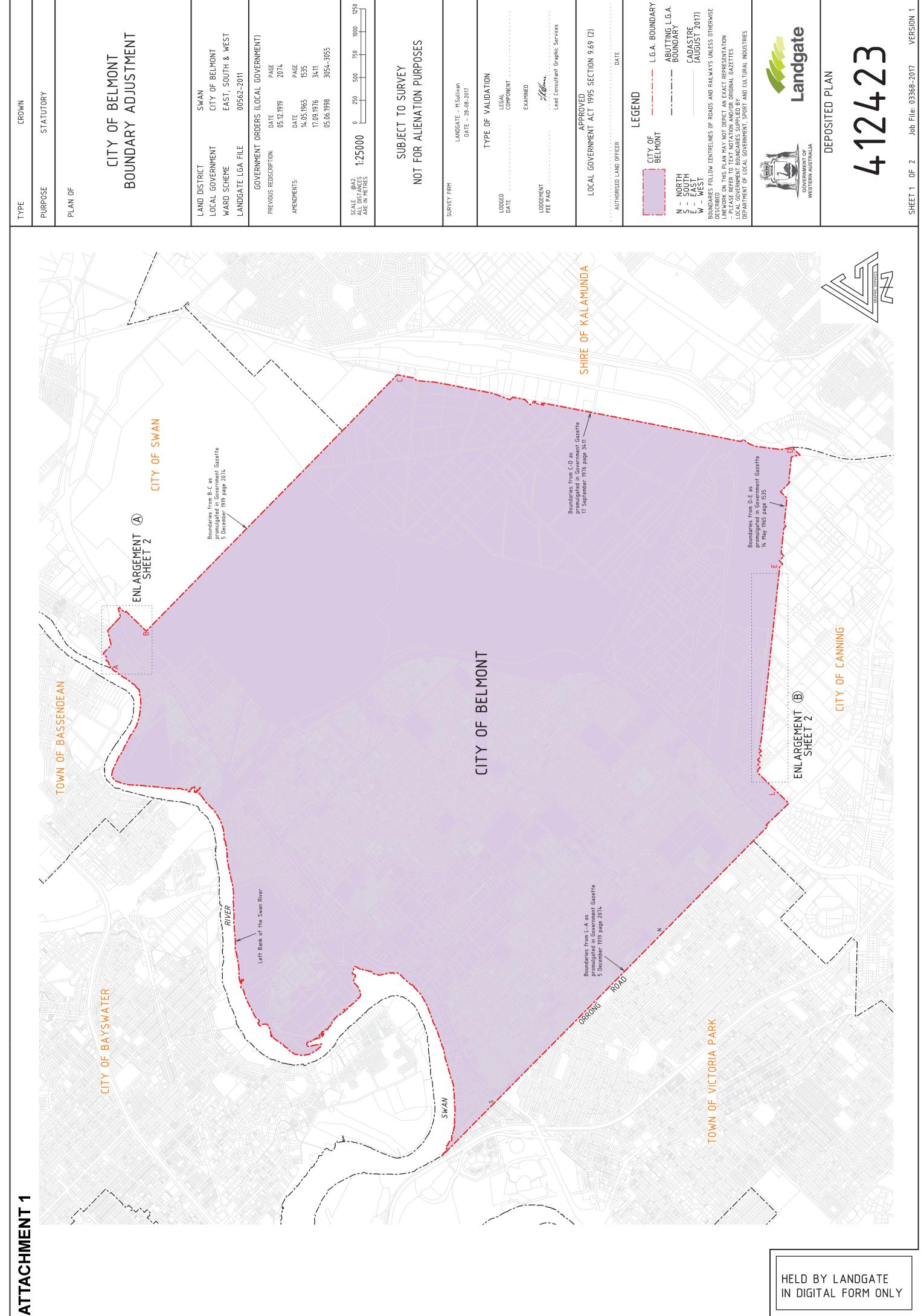
# Ordinary Council Meeting 26/02/19

Item 12.4 refers

**Attachment 10** 

Adjusted Local Government Boundaries – Deposited Plan 412423





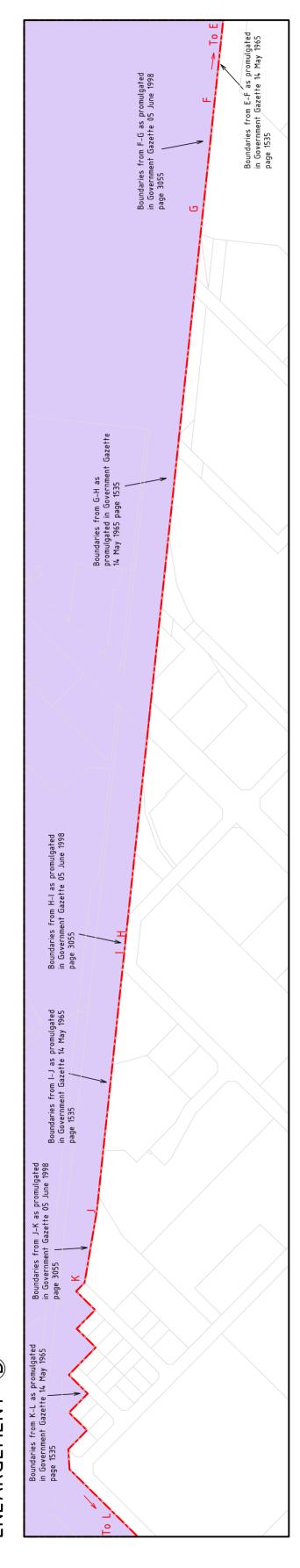
A65

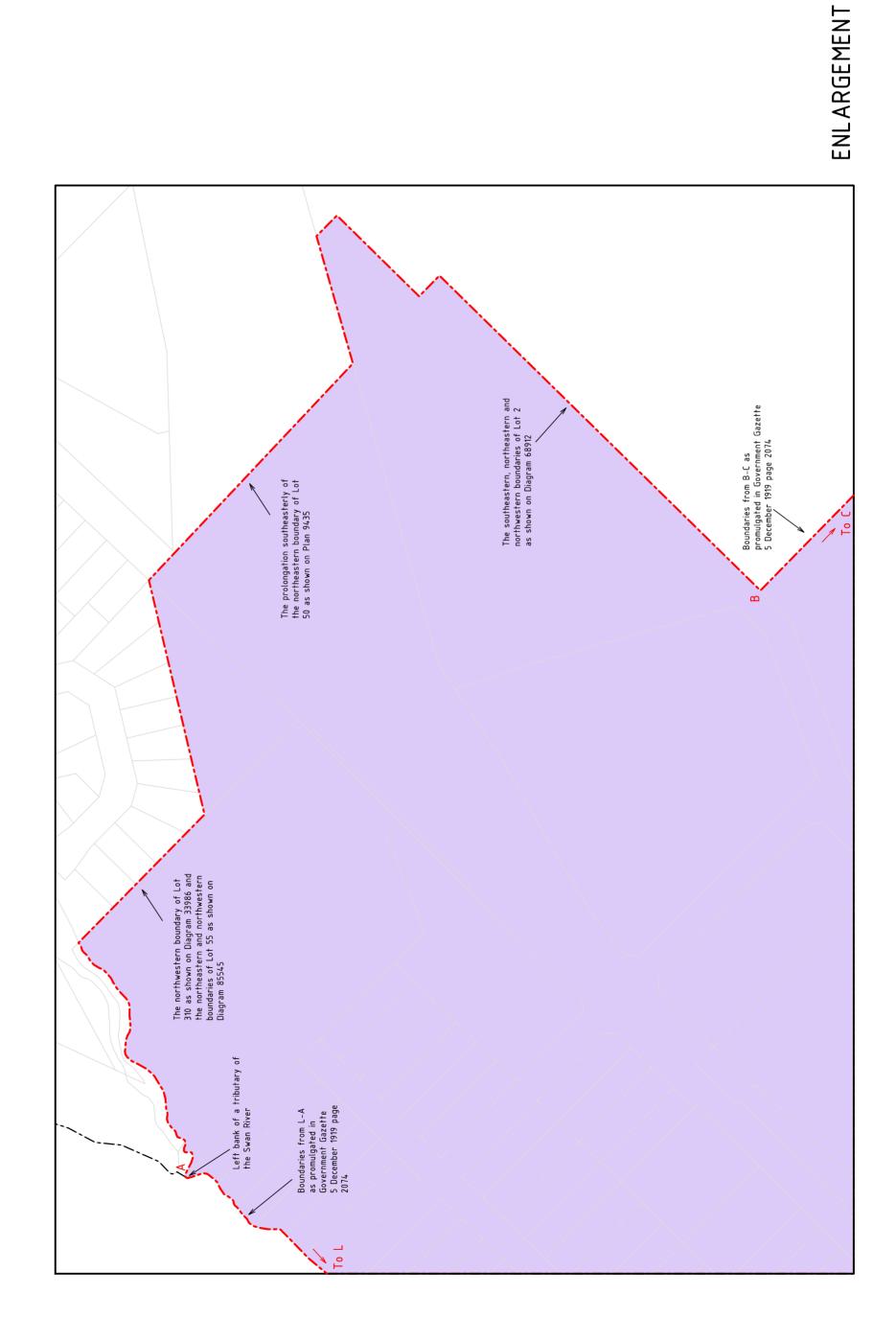
412423

DEPOSITED PLAN

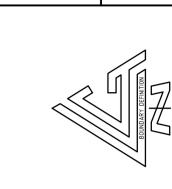
# $\bigcirc$ **ENL ARGEMENT**

FOR HEADING SEE SHEET 1





SCALE ENLARGEMENTS NOT TO



Landgate





# Ordinary Council Meeting 26/02/19

Item 12.4 refers

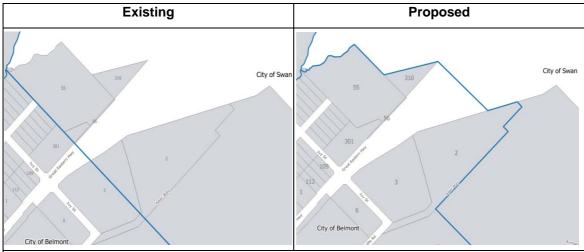
# Attachment 11

# Proposed Local Government Scheme No. 15 Scheme Map Amendments



# <u>ATTACHMENT 2 – PROPOSED LPS 15 SCHEME MAP AMENDMENTS</u>

# Map 1



# **Legend**

Scheme Area Boundary

**Property Boundaries** 

## Location:

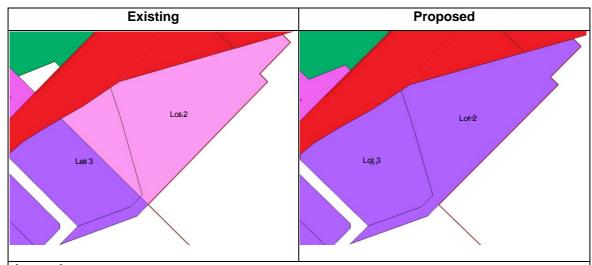
- Lot 2 (No. 5) Apac Way, South Guilford;
- Lot 3 (No. 555) Great Eastern Highway, Redcliffe;
- Lot 301 (No. 510) Great Eastern Highway, Ascot;
- Lot 55 Great Eastern Highway, South Guilford;
- Lot 310 Great Eastern Highway, South Guilford; and
- A portion of land reserved for 'Parks and Recreation' under the Metropolitan Region Scheme (MRS).

# **Mapping Amendment Description:**

Expanding of the Scheme Area to encompass the adjusted local government boundary, as depicted in the Deposited Plan No. 412423.

- The Minister for Local Government; Heritage; Culture and the Arts approved the transfer of a portion of land from the City of Swan to the City of Belmont.
- The realignment was gazetted on 8 December 2017.

Map 2



# Legend

### LPS 17:

General Industrial

### LPS 15:

Industrial

## MRS:

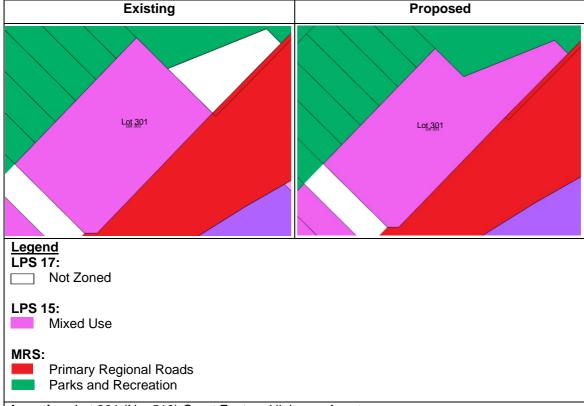
Reserve for Primary Regional Roads
Reserve for Parks and Recreation

**Location:** Lot 2 (No. 5) Apac Way, South Guilford and Lot 3 (No. 555) Great Eastern Highway, Redcliffe.

**Mapping Amendment Description:** Zoning the eastern portion of Lot 2 (No. 5) Apac Way, South Guilford and Lot 3 (No. 555) Great Eastern Highway, Redcliffe. to 'Industrial'.

- The eastern portion of these lots was zoned 'General Industrial' under the City of Swan's LPS 17.
- The entirety of the lots was transferred from the City of Swan to the City of Belmont.
- The zoning of the transferred lots needs to be consistent with the zoning adopted by the LPS 15.
- The LPS 15 does not include the 'General Industrial' zoning, however the western part of these lots is zoned 'Industrial' under the LPS15 which is similar to the current 'General Industrial' zoning.

Map 3



Location: Lot 301 (No. 510) Great Eastern Highway, Ascot

**Mapping Amendment Description:** Zoning the eastern portion of Lot 301 (No. 510) Great Eastern Highway, Ascot to 'Mixed Use'

- The eastern portion of this lot was not zoned under the City of Swan's LPS 17.
- The eastern portion of this lot was transferred from the City of Swan to the City of Belmont.
- The transferred land needs to be consistent with the zoning adopted in the LPS 15.
- The western portion of the lot is zoned 'Mixed Use' under the LPS 15.

Map 4



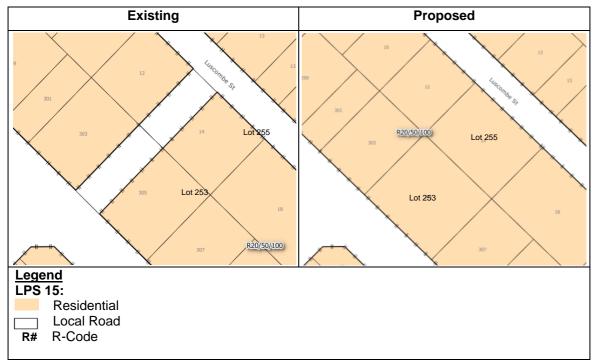
**Location:** Lot 901 (No. 336) Daly Street, Belmont; Lot 902 (No. 334) Daly Street, Belmont and Lot 105 (No. 21) Harman Street, Belmont (Harman Park).

# **Mapping Amendment Description:**

- Rezoning a portion of Lots 901 (No. 336) and 902 (No. 334) Daly Street, Belmont, from 'Local Road' to 'Residential' with an 'R20' R-Coding; and
- Rezoning the corner of Lot 105 (No. 21) Harman Street, Belmont, from 'Local Road' to 'Parks and Recreation'.

- The road reserve has been officially closed and the lots have been developed with dwellings, in accordance with the current zoning of the area.
- The 'truncation' was amalgamated with Lot 105.

Map 5

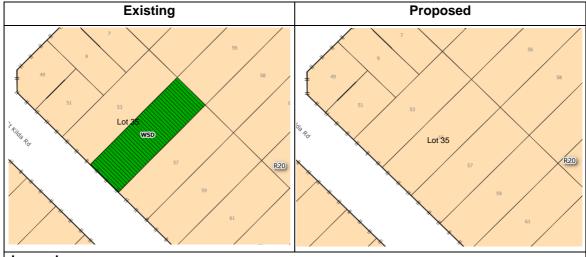


**Location:** Lot 253 (No. 305) Acton Avenue, Kewdale and Lot 255 (No. 14) Luscombe Street, Kewdale.

**Mapping Amendment Description:** Rezoning a portion of Lot 253 (No. 305) Acton Avenue and Lot 255 (No. 14) Luscombe Street, Kewdale from 'Local Road' to 'Residential' with an 'R20/50/100' R-Coding.

**Justification:** The road reserve has been officially closed and the lots have been developed with dwellings, in accordance with the current zoning of the area.

Map 6



<u>Legend</u>

LPS 15:

Residential

Parks and Recreation

Local Road

R# R-Code

Location: Lot 35 (No. 55) St Kilda Road, Rivervale

**Mapping Amendment Description:** Rezoning Lot 35 (No. 55) St Kilda Road, Rivervale, from 'Parks and Recreation', with the notation 'Water Supply Sewage and Drainage (WSD)' to 'Residential' with an 'R20' R-Coding.

**Justification:** A dwelling has been built on site. The land was vacant on 7 September 1993 and was disposed by public tender on 18 October 1993.

Map 7

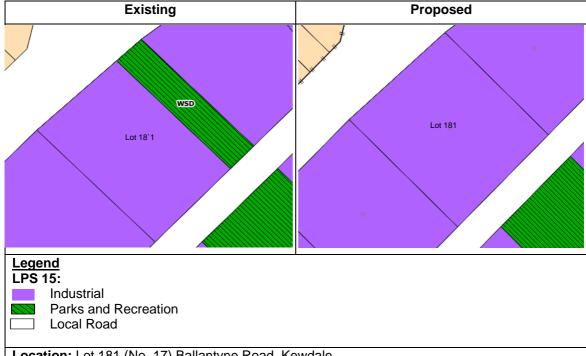


# **Mapping Amendment Description:**

Zoning a portion of Lot 35 (No. 1) Epsom Avenue, Ascot to 'Mixed Use'.

- The northern portion of the lot was previously reserved for the 'Parks and Recreation' under the MRS and was rezoned to Urban via MRS Amendment 1275/57.
- The entire lot should be zoned 'Mixed Use' to ensure consistency with the rest of the property.

Map 8



Location: Lot 181 (No. 17) Ballantyne Road, Kewdale

Mapping Amendment Description: Rezoning a portion of Lot 181 (No. 17) Ballantyne Road, Kewdale, from 'Parks and Recreation' with the notation 'Water Supply Sewage and Drainage (WSD)' to 'Industrial'.

#### Justification:

- The Certificate of Title of Lot 181 has an easement registered to the benefit of Water Corporation that restricts development and protects the water supply, sewer and drainage infrastructure located underground the lot.
- There is no need to have two different zonings in this lot as the easement is sufficient to protect the abovementioned infrastructure.

Map 9

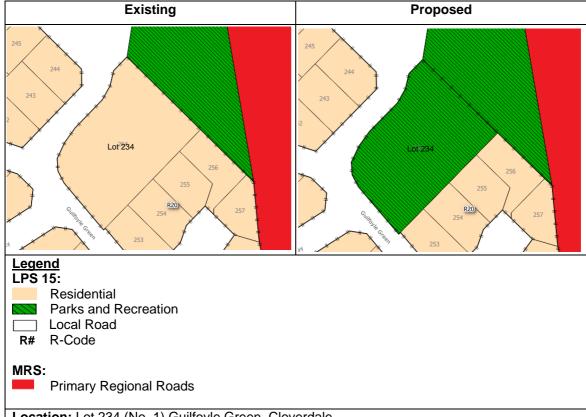


Location: The Bell Street road reserve, Kewdale, between Orrong Road and Sissons Street.

**Mapping Amendment Description:** Rezoning land comprising the Bell Street road reserve, between Orrong Road and Sissons Street, Kewdale, from 'Industrial' and 'Parks and Recreation' with the notation 'Water Supply Sewage and Drainage (WSD)' to 'Local Road'.

**Justification:** The Bell Street road reserve has been constructed on land formerly reserved for drainage.

Map 10

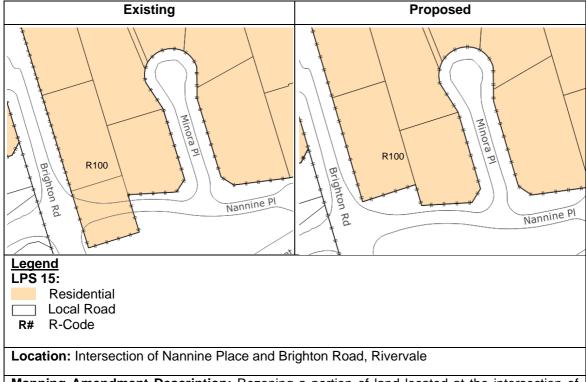


Location: Lot 234 (No. 1) Guilfoyle Green, Cloverdale

Mapping Amendment Description: Rezoning Lot 234 (No. 1) Guilfoyle Green, Cloverdale, from 'Residential' with an 'R20' R-Coding to 'Parks and Recreation'.

Justification: This lot is an existing area of public open space (Reserve No. 47605 - Whiteside Park).

**Map 11** 



**Mapping Amendment Description:** Rezoning a portion of land located at the intersection of Nannine Place and Brighton Road, Rivervale, from 'Residential' with an 'R100' R-Coding to 'Local Road'.

Justification: A road has been constructed on this portion of land (Nannine Place).

**Map 12** 



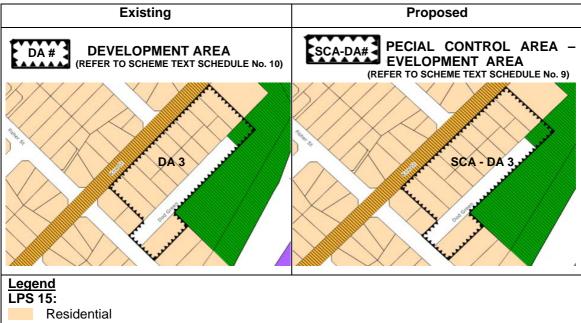
#### Location:

- Lot 302 (No. 553) Abernethy Road, Kewdale

 Lot 22 (No. 159) McDowell Street, Kewdale
 Mapping Amendment Description: Zoning Lot 302 (No. 553) Abernethy Road and a portion of Lot 22 (No. 159) McDowell Street, Kewdale to 'Industrial'.

Justification: The LPS 15 zoning should be in accordance with the MRS zoning

Map 13



Parks and Recreation
Property Boundaries

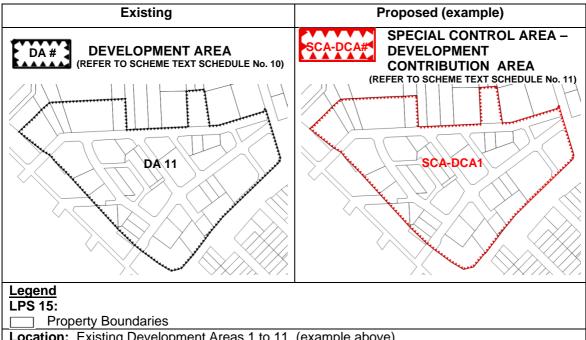
**Location:** Existing Development Areas 1 to 11 (example above)

**Mapping Amendment Description:** Modifying all notations and the legend of 'Development Area', depicted on the Scheme Map as 'DA', to 'Special Control Area – Development Area' depicted as 'SCA - DA' followed by a number from 1 to 11, corresponding with the number contained in the Schedule 14 of the Scheme Text.

#### Justification:

- The Scheme Map is to be in accordance with the amendments proposed to Clause 6.1 and Schedule 14 of the Scheme Text.
- Schedule 14 has been renumbered as Schedule 9.
- The current Scheme Map shows a wrong notation referring to Schedule No. 10

**Map 14** 



**Location:** Existing Development Areas 1 to 11 (example above)

Mapping Amendment Description: Modifying notations and the legend of 'Development Area', for the Special Control Areas subject to Development Contribution, depicted on the Scheme Map as 'DA'; to 'Special Control Area - Development Contribution Area', depicted as SCA -DCA' followed by a number, corresponding with the number contained in the Schedule 16 of the Scheme Text.

#### Justification:

- The Scheme Map is to be in accordance with the amendments proposed to Clause 6.1 and Schedule 16 of the Scheme Text.
- Schedule 16 has been renumbered as Schedule 11
- The current Scheme Map shows a wrong notation referring to Schedule No. 10
- The boundary of the 'SCA- DA11' overlaps the boundary of 'SCA-DCA1'. However, both legends will be maintained in the Scheme Map.



# Ordinary Council Meeting 26/02/19

Item 12.4 refers

**Attachment 12** 

## Amended Local Planning Scheme No.15 Scheme Text



#### **City of Belmont**

#### LOCAL PLANNING SCHEME NO. 15 SCHEME TEXT

**Gazetted 1 December 2011** 

#### **Disclaimer**

This Scheme Text is to be read in conjunction with Schedule 2 of the Planning and Development (Local Planning Schemes) Regulations 2015. In accordance with Regulation 10(4), the provisions in Schedule 2 are deemed provisions which are incorporated into City of Belmont Local Planning Scheme No. 15 Scheme Text and which prevail over the provisions of City of Belmont Local Planning Scheme No. 15 Scheme Text to the extent of any inconsistency.



#### **Scheme Amendments**

Scheme Amendment No.	Brief Description	Government Gazette Date				
3	Amending clause 5.5.1 to read: If a development is the subject of an application for development approval does not comply with a standard or requirement prescribed under the Scheme, the local government may, despite the non-compliance, approve the application unconditionally or subject to such conditions as the local government thinks fit.	28/02/2014				
5	Amending Table I use class to modify Child Family Day Care land use in the Special Development Precinct.  Amending the scheme map to remove the underlying zoning of 'Residential' from all Special Development Precincts and creating a new legend delineation for 'Special Development Precinct' on the LPS15 Scheme Map under Local Scheme Zones and rezoning Lot 401 (20) Gladstone to 'R20/60'.  Amending Clause 5.18.1 to clarify the development standards of commercial vehicles parked on Residential land.  Amending Schedule 2 to include the use of 'Horse Sales' and 'Stables' as Additional Use 18.	28/11/2014				
4	Delete Clause 5.3.2 and insert amended Clause 5.3.2(2) to clarify that development of Multiple Dwellings at the R30 density is not permitted on R20 coded corner lots.	26/03/2015				
1	Omnibus amendment with minor administrative changes. Refer to Government Gazette dated 15 December 2015 for all changes.	15/12/2015				
7	Amending Table 1 (use class table) and Schedule 1 (General and Land Use Definitions).	1/11/2016				
9	Amending Clause 5.3.2(2)(c).	11/11/2016				
2	Amending Scheduled 16 to include Development Contribution Area 1 (The Springs Special Development Precinct)	07/02/2017				
6	To amend Scheme Maps, Clause 5.8.2 and Schedule 2 (Belvidere Main Street Precinct).	21/02/2017				
10	Replacing Clause 4.12.1(a) of the Scheme Text to introduce provisions to permit outdoor storage in the Mixed Business Zone.	Pending (public advertising closes on 24/08/2018)				
11	Omnibus amendment that:  1. Amended the Scheme Text to remove provisions that are obsolete and/inconsistent with the Planning and Development (Local Planning Schemes) Regulations 2015.  2. Amended Clause 4.7.7 to clarify requirements for the subdivision of land in flexible coded areas.  3. Amended the Scheme Map to encompass and zone land transferred from the City of Swan to the City of Belmont through a local government boundary realignment.  4. Amended the Scheme Map to correct minor errors and anomalies.	Subject amendment				



#### The City of Belmont

### Local Planning Scheme No. 15 District Zoning Scheme

This Local Planning Scheme of the City of Belmont consists of this Scheme Text and the Scheme Maps. The Scheme Text should be read with the Local Planning Strategy for the City.

The Scheme divides the local government district into zones to identify areas for particular uses and identifies land reserved for public purposes. Most importantly, the Scheme controls the types of uses and development allowed in different zones. There are particular controls included for heritage and special control areas. The Scheme Text also sets out the requirements for development approval, enforcement of the Scheme provisions and non conforming uses.

#### **Scheme Details**

The City of Belmont

Local Planning Scheme No.15

District Zoning Scheme

The City of Belmont under the powers conferred by the *Planning and Development Act 2005* makes the following Local Planning Scheme.

#### - Table of Contents -

1.1 Citation	1 1 1
1.3 Scheme Area	1 1
1.4 Contents of Scheme 1.5 Purposes of Scheme 1.6 The Aims of the Scheme 1.7 Definitions 1.8 Relationship with Local Laws 1.9 Relationship with Other Schemes 1.10 Relationship with the Metropolitan Region Scheme  PART 2 - RESERVES 2.1 Reserves 2.2 Regional Reserves 2.3 Local Reserves 2.4 Use and Development of Local Reserves 2.4 Use and Development of Local Reserves 3.1 Zones 3.2 Objectives of the zones 3.3 Zoning Table 3.4 Interpretation of the Zoning Table 3.5 Additional Uses 3.6 Restricted Uses 3.7 Special Use Zones	1
1.5 Purposes of Scheme  1.6 The Aims of the Scheme  1.7 Definitions  1.8 Relationship with Local Laws  1.9 Relationship with Other Schemes  1.10 Relationship with the Metropolitan Region Scheme  PART 2 - RESERVES  2.1 Reserves  2.2 Regional Reserves  2.3 Local Reserves  2.4 Use and Development of Local Reserves  PART 3 - ZONES AND THE USE OF LAND  3.1 Zones  3.2 Objectives of the zones  3.3 Zoning Table  3.4 Interpretation of the Zoning Table  3.5 Additional Uses  3.6 Restricted Uses  3.7 Special Use Zones	
1.6 The Aims of the Scheme 1.7 Definitions	1
1.7 Definitions  1.8 Relationship with Local Laws  1.9 Relationship with Other Schemes  1.10 Relationship with the Metropolitan Region Scheme  PART 2 - RESERVES  2.1 Reserves  2.2 Regional Reserves  2.3 Local Reserves  2.4 Use and Development of Local Reserves  2.5 Visual Reserves  2.6 Objectives of the zones  3.7 Special Use Zones  3.8 Restricted Uses  3.9 Relationship with Local Laws  3.1 Local Reserves  3.1 Zones  3.2 Objectives of the zones  3.3 Zoning Table  3.4 Interpretation of the Zoning Table  3.5 Additional Uses  3.6 Restricted Uses  3.7 Special Use Zones	
1.8 Relationship with Local Laws 1.9 Relationship with Other Schemes 1.10 Relationship with the Metropolitan Region Scheme  PART 2 - RESERVES  2.1 Reserves 2.2 Regional Reserves 2.3 Local Reserves 2.4 Use and Development of Local Reserves  PART 3 - ZONES AND THE USE OF LAND  3.1 Zones 3.2 Objectives of the zones 3.3 Zoning Table 3.4 Interpretation of the Zoning Table 3.5 Additional Uses 3.6 Restricted Uses 3.7 Special Use Zones	2
1.9 Relationship with Other Schemes  1.10 Relationship with the Metropolitan Region Scheme  PART 2 - RESERVES  2.1 Reserves  2.2 Regional Reserves  2.3 Local Reserves  2.4 Use and Development of Local Reserves  PART 3 - ZONES AND THE USE OF LAND  3.1 Zones  3.2 Objectives of the zones  3.3 Zoning Table  3.4 Interpretation of the Zoning Table  3.5 Additional Uses  3.6 Restricted Uses  3.7 Special Use Zones	2
1.10 Relationship with the Metropolitan Region Scheme	3
PART 2 - RESERVES  2.1 Reserves  2.2 Regional Reserves  2.3 Local Reserves  2.4 Use and Development of Local Reserves  PART 3 - ZONES AND THE USE OF LAND  3.1 Zones  3.2 Objectives of the zones  3.3 Zoning Table  3.4 Interpretation of the Zoning Table  3.5 Additional Uses  3.6 Restricted Uses  3.7 Special Use Zones	3
2.1 Reserves  2.2 Regional Reserves  2.3 Local Reserves  2.4 Use and Development of Local Reserves  PART 3 - ZONES AND THE USE OF LAND  3.1 Zones  3.2 Objectives of the zones  3.3 Zoning Table  3.4 Interpretation of the Zoning Table  3.5 Additional Uses  3.6 Restricted Uses  3.7 Special Use Zones	3
2.2 Regional Reserves 2.3 Local Reserves 2.4 Use and Development of Local Reserves  PART 3 - ZONES AND THE USE OF LAND  3.1 Zones 3.2 Objectives of the zones 3.3 Zoning Table 3.4 Interpretation of the Zoning Table 3.5 Additional Uses 3.6 Restricted Uses 3.7 Special Use Zones	
2.3 Local Reserves. 2.4 Use and Development of Local Reserves.  PART 3 - ZONES AND THE USE OF LAND.  3.1 Zones. 3.2 Objectives of the zones. 3.3 Zoning Table. 3.4 Interpretation of the Zoning Table. 3.5 Additional Uses. 3.6 Restricted Uses. 3.7 Special Use Zones.	4
2.4 Use and Development of Local Reserves  PART 3 - ZONES AND THE USE OF LAND	4
PART 3 - ZONES AND THE USE OF LAND	4
3.1 Zones	4
3.2 Objectives of the zones 3.3 Zoning Table	5
<ul> <li>3.3 Zoning Table</li></ul>	5
<ul> <li>3.4 Interpretation of the Zoning Table</li> <li>3.5 Additional Uses</li> <li>3.6 Restricted Uses</li> <li>3.7 Special Use Zones</li> </ul>	5
<ul><li>3.5 Additional Uses</li><li>3.6 Restricted Uses</li><li>3.7 Special Use Zones</li></ul>	6
3.6 Restricted Uses	7
3.7 Special Use Zones	11
	11
	11
3.8 Non Conforming Uses	11
3.9 Extensions and Changes to a Non Conforming Use	12
3.10 Discontinuance of Non Conforming Use	12
3.11 Termination of a Non Conforming Use	12
3.12 Destruction of Non Conforming Use Buildings	12
PART 4 - GENERAL DEVELOPMENT REQUIREMENTS	13
4.1 Compliance with Development Standards and Requirements	13
4.2 Residential Design Codes	13
4.3 Special Application of Residential Design Codes	13
4.4 Restrictive Covenants	15
4.5 Variations to Site and Development Standards and Requirements	15
4.6 Environmental Conditions	15
4.7 Residential Zone	16
4.8 Special Development Precincts	18
4.9 Residential and Stables Zone	18
4.10 Town Centre and Commercial Zone	19
4.11 Mixed Use Zone	

#### - Table of Contents -

4.12	Mixed Business Zone	21
4.13	Industrial Zone	
4.14	Public Assembly Zone	
4.15	Service Station Zone	
4.16	Vehicle Parking and Loading	29
4.17	Bicycle Parking	34
4.18	Parking of Commercial Vehicles on Residential Land	37
4.19	Development on Great Eastern Highway	38
4.20	Structures Height Control Contours Map	38
4.21	Prohibited Signage	38
PART 5 -	SPECIAL CONTROL AREAS	40
5.1	Operation of Special Control Areas	40
5.2	Development Contribution Areas	
SCHEDUI	LES	48
Schedu	le No. A - Supplemental Provisions to the Deemed Provisions	48
	le No. 1 - Dictionary of Defined Words and Expressions	
Schedu	le No. 2 - Additional Uses	63
Schedu	le No. 3 - Restricted Uses	78
Schedu	le No. 4 - Special Use Zones	79
Schedu	le No. 5 - Exempted Advertisements	80
Schedu	le No. 6 - Environmental Conditions	82
Schedu	le No. 7 - Car Parking Layout	83
Schedu	le No. 8 - Structures Height Control Contours Map	84
Schedu	le No. 9 - Special Control Areas - Development Areas	85
Schedu	le No. 10 - Statutory Static Feasibility Assessment Model	87
Schedu	le No. 11 - Special Control Areas - Development Contribution Areas	88
List of Tab	<u>bles</u>	
Table 1 - Z	oning Table	8
Table 2 - C	ar Parking Requirements	31
rable 3 - B	icycle Parking Requirements	35

#### **PART 1 - PRELIMINARY**

#### 1.1 Citation

- 1.1.1 The City of Belmont Local Planning Scheme No. 15 (the Scheme) comes into operation on its Gazettal date.
- 1.1.2 The following Scheme is revoked:

City of Belmont Town Planning Scheme No. 14 - District Zoning Scheme gazetted 10 December 1999.

#### 1.2 Responsible Authority

The City of Belmont is the responsible authority for implementing the Scheme.

#### 1.3 Scheme Area

The Scheme applies to the Scheme Area which covers all of the local government district of the City as shown on the Scheme Map.

Note: The Scheme Area is also subject to the Metropolitan Region Scheme.

#### 1.4 Contents of Scheme

The Scheme comprises:

- (a) the Scheme Text;
- (b) the Scheme Map(s).

The Scheme is to be read in conjunction with the Local Planning Strategy.

#### 1.5 Purposes of Scheme

The purposes of the Scheme are to:

- (a) set out the local government's planning aims and intentions for the Scheme Area;
- (b) set aside land as reserves for public purposes;
- (c) zone land within the Scheme Area for the purposes defined in the Scheme;
- (d) control and guide land use and development;
- (e) set out procedures for the assessment and determination of development applications;
- (f) make provision for the administration and enforcement of the Scheme; and

(g) address other matters set out in the Seventh Schedule to the Planning Act.

#### 1.6 The Aims of the Scheme

The aims of the Scheme are:

- (a) to assist the effective implementation of regional plans and policies including the State Planning Strategy;
- (b) to ensure there is a sufficient supply of serviced and suitable land for housing, employment, commercial activities, community facilities, recreation and open space;
- (c) to provide for housing choice and variety in neighbourhoods with a community identity and high levels of amenity;
- (d) to assist employment and economic growth by facilitating the timely provision of suitable land for retail, commercial, industrial, entertainment and tourist developments, as well as providing opportunities for home-based employment;
- to protect and enhance the environmental values and natural resources of the local government and to promote ecologically sustainable land use and development;
- (f) to safeguard and enhance the character and amenity of the built and natural environment of the local government;
- (g) to incorporate public art to enhance the character and amenity of the built and natural environment of the local government;
- (h) to maximise the built-in safety of the local government;
- (i) to protect and maximise efficacy of existing and future community infrastructure needs.

#### 1.7 Definitions

- 1.7.1 Unless the context otherwise requires words and expressions used in the Scheme have the same meaning as they have:
  - (a) in the *Planning and Development Act 2005*; or
  - (b) if they are not defined in that Act:
    - (i) in the Dictionary of defined words and expressions in Schedule 1; or
    - (ii) in the Residential Design Codes.
- 1.7.2 If there is a conflict between the meaning of a word or expression in the Dictionary of defined words and expressions in Schedule 1 and the meaning of that word or expression in the Residential Design Codes:
  - (a) in the case of residential development, the definition in the Residential Design Codes shall prevail; and

- (b) in any other case the definition in the Dictionary prevails.
- 1.7.3 Notes, and instructions printed in italics, are not part of the Scheme.

#### 1.8 Relationship with Local Laws

Where a provision of the Scheme is inconsistent with a local law, the provision of the Scheme prevails.

#### 1.9 Relationship with Other Schemes

By way of information, the following other Schemes of the City of Belmont are, at the Gazettal date of the Scheme, complementary to the Scheme:

Scheme No. Gazettal date

There are no other Schemes of the City of Belmont which apply to the Scheme area.

#### 1.10 Relationship with the Metropolitan Region Scheme

The Scheme is complementary to the Metropolitan Region Scheme and the provisions of the Metropolitan Region Scheme continue to have effect.

Note: The authority responsible for implementing the Metropolitan Region Scheme is the Western Australian Planning Commission.

#### **PART 2 - RESERVES**

#### 2.1 Reserves

Certain lands within the Scheme Area on the Scheme Map are classified as:

- (a) Regional Reserves; or
- (b) Local Reserves.

#### 2.2 Regional Reserves

- 2.2.1 The lands shown as "Regional Reserve" on the Scheme Map are lands reserved under the Metropolitan Region Scheme and are shown on the Scheme Map for the purposes of the *Planning and Development Act 2005.* These lands are not reserved under the Scheme.
- 2.2.2 The approval of the local government under the Scheme is not required for the commencement or carrying out of any use or development on a Regional Reserve.
- Note: The provisions of the Metropolitan Region Scheme continue to apply to such Reserves and approval is required under the Metropolitan Region Scheme from the Commission for the commencement or carrying out of any use or development on a Regional Reserve unless specifically excluded by the Region Scheme.

#### 2.3 Local Reserves

"Local Reserves" are delineated and depicted on the Scheme Map according to the legend on the Scheme Map.

#### 2.4 Use and Development of Local Reserves

- 2.4.1 A person must not:
  - (a) use a Local Reserve; or
  - (b) commence or carry out development on a Local Reserve;

without first having obtained development approval under Part 8 of Schedule 2 of the *Planning and Development (Local Planning Schemes) Regulations 2015.* 

- 2.4.2 In determining an application for development approval, the local government is to have due regard to -
  - (a) matters set out in Clause 67 of the *Planning and Development (Local Planning Schemes) Regulations 2015* Schedule 2; and
  - (b) the ultimate purpose intended for the Reserve.
- 2.4.3 In the case of land reserved for the purposes of a public authority, the local government is to consult with that authority before determining an application for development approval.

#### PART 3 - ZONES AND THE USE OF LAND

#### 3.1 **Zones**

- 3.1.1 The Scheme area is classified into the zones shown on the Scheme Map.
- 3.1.2 The zones are delineated and depicted on the Scheme Map according to the legend on the Scheme Map.

#### 3.2 Objectives of the zones

The objectives of the zones are:

#### **Residential Zone**

The purpose and intent of the Residential Zone is to increase the population base of the City of Belmont by permitting a mix of single housing and other housing types to reflect household composition and thereby increase the resident population.

#### **Special Development Precinct Zone**

The 'Special Development Precinct' Zone is intended to allow for the development of predominantly residential precincts which also allows for a mix of varied but compatible supporting land uses such as offices, showrooms and eating establishments which do not generate nuisances detrimental to the amenity of the precincts' residents. Buildings should be of a very high standard of architectural design.

#### **Residential and Stables Zone**

The Residential and Stables Zone is intended to provide for compatible and environmentally responsible use of land in proximity to the Ascot Racecourse and the Swan River by residential accommodation and stables and ancillary functions of the horse racing industry.

#### **Town Centre and Commercial Zones**

The Town Centre and Commercial Zones are intended to provide for the retail commercial function and entertainment.

#### **Mixed Use Zone**

The Mixed Use Zone is intended to allow for the development of a mix of varied but compatible land uses such as housing, offices, showrooms, amusement centres, eating establishments and appropriate industrial activities which do not generate nuisances detrimental to the amenity of the district or to the health, welfare and safety of its residents. Buildings should be of a high standard of architectural design set in pleasant garden surrounds with limited vehicular access from properties to primary roads.

#### **Mixed Business Zone**

The 'Mixed Business' zone is intended to allow for the development of a mix of varied but compatible business uses such as offices, showrooms, amusement centres, eating establishments and appropriate industrial activities which do not generate nuisances detrimental to the amenity of the district or to the health, welfare and safety of residents

and workforce. Uses can mix on adjacent lots of land or on the same lot and uses may mix horizontally on the same or separate lots and/or vertically in buildings. Buildings should be of a high standard of architectural design set in pleasant garden surrounds with limited vehicular access from properties to primary roads.

#### **Industrial Zone**

The Industrial Zone is intended to provide for the industrial development of the Kewdale Industrial Estate and the Redcliffe Industrial Estate. The significance of the Kewdale Industrial Estate as a transport and logistics hub as part of the Kewdale-Hazelmere Integrated Masterplan is acknowledged. The local government may approve a wide range of industrial activities within this zone subject to conditions designed to achieve a high standard of industrial environment.

#### **Service Station Zone**

The 'Service Station' zone is intended to allow for the development of service stations and appropriate support activities which do not generate nuisances detrimental to the amenity of the district and having particular regard for the health, welfare and safety of any residents and workforce associated with any immediately abutting zoned land.

#### **Places of Public Assembly Zone**

The 'Places of Public Assembly' zone is intended to allow for special places of assembly, such as halls, private schools, grounds for athletics, sports grounds with provision for spectators, racecourses, trotting track, stadia and/or showgrounds.

#### 3.3 Zoning Table

3.3.1 The Zoning Table indicates, subject to the provisions of the Scheme, the uses permitted in the Scheme area in the various zones. The permissibility of any uses is determined by cross reference between the list of use classes on the left hand side of the Zoning Table and the list of zones at the top of the Zoning Table.

#### 3.3.2 Symbols

The symbols used in the cross-reference in the Zoning Table, and where used elsewhere in the Scheme have the following meanings:

- 'P' means that the use is permitted by the Scheme providing the use complies with the relevant development standards and the requirements of the Scheme;
- 'D' means that the use is not permitted unless the local government has exercised its discretion by granting development approval;
- 'A' means that the use is not permitted unless the local government has exercised its discretion by granting development approval after giving special notice in accordance with the provisions contained in Clause 64 of the *Planning and Development (Local Planning Schemes) Regulations 2015* Schedule 2;
- 'X' means a use that is not permitted by the Scheme.
- 3.3.3 A change in the use of land from one use to another is permitted if:

- (a) the local government has exercised its discretion by granting development approval;
- (b) the change is to a use which is designated with the symbol 'P' in the cross reference to that zone in the Zoning Table and the proposed use complies with all the relevant development standards and any requirements of the Scheme;
- (c) the change is an extension of a use within the boundary of the lot which does not change the predominant use of the lot; or
- (d) the change is to an incidental use that does not change the predominant use of the land.

Note:

- 1. The development approval of the local government is required for the development of land in addition to any approval granted for the use of land. In normal circumstances one application is made for both the use and development of land.
- 2. The local government will not refuse a 'P' use because of the unsuitability of the use for the zone but may impose conditions on the use of the land to comply with any relevant development standards or requirements of the Scheme, and may refuse or impose conditions on any development of the land.
- 3. In considering a 'D" or 'A" use, the local government will have regard to the matters set out in Clause 67 of the *Planning and Development (Local Planning Schemes) Regulations* 2015 Schedule 2.
- 4. The local government must refuse to approve any 'X' use of land. Approval to an 'X' use of land may only proceed by way of an amendment to the Scheme.

#### 3.4 Interpretation of the Zoning Table

- 3.4.1 Where a specific use is mentioned in the Zoning Table, it is deemed to be excluded from the general terms used to describe any other use.
- 3.4.2 If a person proposes to carry out on land any use that is not specifically mentioned in the Zoning Table and cannot reasonably be determined as falling within the type, class or genus of activity of any other use category the local government may:
  - (a) determine that the use is consistent with the objectives of the particular zone and is therefore permitted;
  - (b) determine that the use may be consistent with the objectives of the particular zone and thereafter follow the advertising procedures contained in Clause 64 of the *Planning and Development (Local Planning Schemes) Regulations 2015* Schedule 2; or
  - (c) determine that the use is not consistent with the objectives of the particular zone and is therefore not permitted.

Table 1 - Zoning Table

	ZONES									
USE CLASSES	Residential	Town Centre	Commercial	Mixed Use	Mixed Business	Industrial	Service Station	Places of Public Assembly	Residential and Stables	Special Development Precinct
Aged or Dependent Persons Dwelling	D	Х	D	D	А	X	X	X	D	D
Amusement Facility	Х	Р	D	D	D	Х	Х	Х	Х	Х
Amusement parlour	Х	Р	Х	D	D	X	Х	X	Х	Х
Ancillary Dwelling	P	X	X	D D	D D	X	X	X	D	D D
Art Gallery	X					X	X	X	X	
Auction Mart Bed and Breakfast	X D	X	X	A	D X	D X	X	X	X A	X A
Betting Agency	Х	Р	D	D	D	Х	Х	Х	Х	D
Car Park	D	D	D	D	D	D	D	D	D	D
Caravan Park	Х	Х	Х	X	X	Х	Х	Х	Х	Х
Caretaker's Dwelling	Х	Х	D	D	D	D	X	Р	D	D
Child Care Premises	Α	D	D	D	D	D	X	D	Α	Α
Child Family Day Care	D	X	X	D	D	X	X	D	D	D
Cinema/Theatre	X D	A D	X D	X D	X D	X D	X	X	X D	X D
Civic Use Club Premises		1000			The second second					
Community Home	X D	D X	X	D D	D D	D X	X	D A	X	X D
Consulting Rooms	Х	D	D	D	D	D	Х	Α	Х	А
Convenience Store	X	A	X	A	X	X	A	X	X	A
Corrective Institution	X	X	X	X	X	X	X	X	X	X
Dog Kennels Dry-cleaning Premises	X	X D	D	X D	X D	D	X	X	X	X
Educational Establishment	Α	Х	D	D	D	Х	Х	D	D	X
Exhibition Centre	Х	D	D	Α	Α	Х	Х	А	Х	А
Fast Food Outlet/Lunch Bar	Х	D	D	A	Α	Х	A	Х	Х	А
Fuel Depot	Х	Х	Х	Х	Х	D	Х	Х	Х	Х
Funeral Parlour	Х	Х	Х	D	D	D	Х	Х	Х	X
Garden Centre Grouped Dwelling	X D	D D	D D	D D	D A	D X	X	X	X D	X D
Health Centre Health Studio	X	X	X D	D D	D D	D X	X	X	X	X D

ZONES										
USE CLASSES	Residential	Town Centre	Commercial	Mixed Use	Mixed Business	Industrial	Service Station	Places of Public Assembly	Residential and Stables	Special Development Precinct
Holiday	Х	Х	Х	Х	Х	X	X	Х	Х	Х
Accommodation Home Business	D	D	Р	Р	D	X	X	X	D	D
Home Occupation	Р	D	Р	Р	D	Х	Х	X	Р	Р
Home Store	Α	X	Х	D	D	X	X	X	A	D
Hospital Hotel	A X	X D	X	A D	A A	X	X	X	X	X A
Industry - General	X	X	X	X	X	D	X	X	X	X
Industry - Hazardous	Х	Х	Х	Х	Х	X	Х	X	Х	Х
Industry - Light	Х	Х	Х	D	D	D	Х	Х	Х	Х
Industry - Noxious	Х	Х	Х	Х	Х	Α	X	Х	Х	Х
Industry - Service	Х	Х	Х	D	D	D	Х	Х	Х	Х
Laundromat	Х	Р	D	D	D	D	Х	Х	Х	Х
Liquor Store – Small	Х	Α	D	A	X	Х	X	Α	Х	А
Liquor Store – Large	Х	А	A	X	А	X	X	Х	Х	Х
Logistics Centre	Х	X	X	X	A	D	Х	X	Х	X
Lunch Bar Massage	X	P	D X	D A	D D	D D	X	X	X	D X
Parlour Medical Centre	X	X	D	D	D	D	X	Х	X	A
			X		X	X	X	X	X	
Mining Operations	X	X		X						X
Mobile Phone Tower & Associated Facilities	Х	D	D	D	D	D	X	D	Х	A
Motel	Х	Х	X	D	А	Х	Х	Х	Х	Α
Motor Vehicle, Boat or Caravan Sales	X	Х	X	Х	Х	D	Х	Х	X	X
Motor Vehicle Hire	Х	X	Х	Х	Х	D	Х	Х	Х	Х
Motor Vehicle Repair	Х	Х	Х	Α	D	D	D	Х	Х	Х
Motor Vehicle Wash	Х	Х	Х	Х	Х	D	D	Х	Х	Х
Motor Vehicle Wrecking	Х	Х	Х	Х	Х	Х	Х	Х	Х	Х
Multiple Dwelling	D	D	D	D	Α	Х	Х	Х	Х	D
Night Club	Х	Α	Х	Α	А	D	Х	Χ	Х	Х
Nursing Home	D	Х	Х	D	Α	Х	Х	Х	Х	А
Office	Х	D	D	D	D	D	Х	Х	Х	D
Open Air Display	Х	Х	X	Х	Х	D	Х	Х	Х	X

					ZONES					
USE CLASSES	Residential	Town Centre	Commercial	Mixed Use	Mixed Business	Industrial	Service Station	Places of Public Assembly	Residential and Stables	Special Development Precinct
Pet Day Care	Х	Α	Х	Α	А	D	Х	X	Α	Х
Private Recreation	Х	Х	Х	D	D	D	Х	D	Х	А
Place of Worship	Х	А	X	А	D	Х	X	D	Х	X
Public Amusement	Х	D	А	D	D	Х	Х	D	Х	X
Radio or TV Installation	D	D	D	D	D	D	Х	D	D	Х
Reception Centre	Х	Х	Х	Α	D	X	Х	D	Х	Х
Residential Building	D	Х	Х	D	Α	Х	Х	Х	D	D
Restaurant/Cafe	Х	D	D	D	D	Х	Х	Х	Х	D
Restricted Premises	Х	D	D	D	D	Х	X	Х	Х	Х
Salvage Yard	Х	Х	Х	X	Х	Х	Х	Х	Х	Х
Service Station	Х	Α	Х	Α	Х	X	D	Х	Х	Х
Serviced Apartments	D	Х	Х	D	А	Х	X	Х	Х	D
Shop	Х	D	D	Х	X	X	X	Х	Х	D
Showroom	Х	D	D	D	D	D	X	Х	Х	D
Single House	Р	X	D	D	Α	Х	Х	Х	D	D
Small Bar	Х	Α	Α	Α	X	Х	Х	Х	Х	Α
Stables	Х	Х	Х	X	X	Х	Х	Х	D	Х
Studio	Х	Х	X	D	D	D	Х	Х	D	D
Trade Display	X	Х	X	X	X	Х	Х	Х	X	Х
Tavern	X	D	Α	Α	D	Α	Х	Х	X	Α
Telecommunicat ions Infrastructure	Р	Р	Р	Р	Р	Р	Р	Р	Р	Р
Trade Supplies	X	Х	X	X	Х	D	Х	Х	Х	Х
Transport Depot	X	Х	X	Х	D	D	Χ	Х	Х	Х
Truck Stop	X	Х	X	Х	D	D	Х	Х	Х	Х
Veterinary Centre	Х	Х	Α	А	D	D	Х	Х	Α	Х
Vet Consulting Rooms	Х	D	D	D	D	D	Х	Х	D	Х
Vet Hospital	Х	Х	Х	Α	D	D	Х	Х	Α	Х
Video Store	Х	Р	Р	D	Р	Х	D	Х	Х	D
Warehouse	Х	Х	Х	D	D	D	Х	Х	Х	Х
Waste Storage Facility	Х	Х	Х	Х	Х	А	Х	Х	Х	Х

AMD 7 GG 1/11/2016

#### 3.5 Additional Uses

Despite anything contained in the Zoning Table, the land specified in Schedule 2 may be used for the specific use or uses that are listed in addition to any uses permissible in the zone in which the land is situated subject to the conditions set out in Schedule 2 with respect to that land.

Note: An additional use is a land use that is permitted on a specific portion of land in addition to the uses already permissible in that zone that applies to the land.

#### 3.6 Restricted Uses

Despite anything contained in the Zoning Table, the land specified in Schedule 3 may only be used for the specific use or uses that are listed and subject to the conditions set out in Schedule 3 with respect to that land.

There are no restricted uses which apply to the Scheme.

Note: A restricted use is the only use or uses that is permitted on a specific portion of land and other uses that would otherwise be permissible in the zone are not permitted.

#### 3.7 Special Use Zones

- 3.7.1 Special Use zones are set out in Schedule 4 and are in addition to the zones in the Zoning Table.
- 3.7.2 A person must not use any land, or any structure or buildings on land, in a special use zone except for the purpose set out against that land in Schedule 4 and subject to compliance with any conditions set out in Schedule 4 with respect to that land.

Note: Special use zones apply to special categories of land use which do not comfortably sit within any other zone in the Scheme.

#### 3.8 Non Conforming Uses

Except as otherwise provided in the Scheme, no provision of the Scheme is to be taken to prevent —

- (a) the continued use of any land for the purpose for which it was being lawfully used immediately prior to the Gazettal date;
- (b) the carrying out of any development on that land for which, immediately prior to the Gazettal date, an approval or approvals, lawfully required to authorise the development to be carried out, were duly obtained and are current; or
- (c) subject to the provisions contained in Clause 80 of the *Planning and Development* (Local Planning Schemes) Regulations 2015 Schedule 2, the continued display of advertisements which were lawfully erected, placed or displayed prior to the Gazettal date.

Note: "Land" has the same meaning as in the *Planning and Development Act 2005* and includes houses, buildings and other works and structures.

#### 3.9 Extensions and Changes to a Non Conforming Use

#### 3.9.1 A person must not:

- (a) alter or extend a non conforming use;
- (b) erect, alter or extend a building used in conjunction with or in furtherance of a non conforming use; or
- (c) change the use of land from a non conforming use to another non conforming use; without first having applied for and obtained development approval under the Scheme.
- 3.9.2 An application for development approval under this clause is to be advertised in accordance with the provisions contained in Clause 64 of the *Planning and Development* (Local Planning Schemes) Regulations 2015 Schedule 2.
- 3.9.3 Where an application is for a change of use from an existing non conforming use to another non conforming use, the local government is not to grant its development approval unless the proposed use is less detrimental to the amenity of the locality than the existing non conforming use and is, in the opinion of the local government, closer to the intended purpose of the zone.

#### 3.10 Discontinuance of Non Conforming Use

Where a non conforming use of any land has been discontinued for a period of 6 months the land must not be used after that period otherwise than in conformity with the provisions of the Scheme.

#### 3.11 Termination of a Non Conforming Use

The local government may effect the discontinuance of a non conforming use by the purchase of the land, or by the payment of compensation to the owner or occupier or to both the owner and occupier of that land, and may enter into an agreement with the owner for that purpose.

Note: Clause 190 and 191 of the *Planning and Development Act 2005* enables the local government to purchase, or, with the consent of the Governor, compulsorily acquire land for the purpose of a town planning scheme, subject to Part 9 of the *Land Administration Act 1997*, that section and the Scheme.

#### 3.12 Destruction of Non Conforming Use Buildings

If a building used for a non conforming use is destroyed to 75% or more of its value, the building is not to be repaired, rebuilt, altered or added to for the purpose of being used for a non conforming use or in a manner not permitted by the Scheme, except with the development approval of the local government.

#### **PART 4 - GENERAL DEVELOPMENT REQUIREMENTS**

#### 4.1 Compliance with Development Standards and Requirements

Any development of land is to comply with the provisions of the Scheme.

#### 4.2 Residential Design Codes

- 4.2.1 A copy of the Residential Design Codes is to be kept and made available for public inspection at the offices of the local government.
- 4.2.2 Unless otherwise provided for in the Scheme, the development of land for any of the residential purposes dealt with by the Residential Design Codes is to conform with the provisions of those Codes.
- 4.2.3 The Residential Design Codes density applicable to land within the Scheme area is to be determined by reference to the Residential Design Codes density number superimposed on the particular areas contained within the borders shown on the Scheme Map or where such an area abuts another area having a Residential Design Code density, as being contained within the area defined by the centre line of those borders.

#### 4.3 Special Application of Residential Design Codes

4.3.1 Residential Design Codes - Variations and Inclusions

The Residential Design Codes (R Codes) are hereby varied as set out hereunder.

#### 4.3.2 Residential Zone

- (1) Where an existing R20 coded lot is 450m<sup>2</sup> or less, the local government will:
  - (a) allow a minimum front setback of 2 metres and an average of 4 metres; and
  - (b) allow a minimum open space requirement of 45%.
- (2) The local government may permit the development, or support the subdivision of, an existing R20-coded corner lot to a maximum density of R30 provided -
  - (a) The existing lot has frontage to two constructed roads; and
  - (b) Any existing improvement/s which in the opinion of the local government is -
    - (i) of low quality or incapable of being upgraded to a standard commensurate with new development; or
    - (ii) poorly sited and fails to maximise opportunities in relation to proposed lot boundaries;

is demolished; and

- (c) The created lots are of a regular shape, or it can be demonstrated that a dwelling can be accommodated on any new lot.
  - Development of Multiple Dwellings at the R30 density is not permitted on R20 coded corner lots. *AMD 9 GG 11/11/2016*
- (3) With the exception of minor boundary realignments, the amalgamation of abutting lots with an existing R20-coded corner lot in order to create a larger lot for the purpose of development and/or subdivision at a higher density is not consistent with the intent of the provisions of Clause 4.3.2.2 and the R20 code shall apply to the amalgamated lot. *AMD 1 GG 15/12/2015*

#### 4.3.3 Residential and Stables Zone

The provisions of the R10 Code with regard to "Open Space" and "Minimum Setbacks from Boundaries" only, shall apply to any residential development to the Residential and Stables Zone. These requirements may be varied by the local government in order to allow the satisfactory development of a residence and stables on a lot.

The local government in exercising such discretion must be satisfied in regard to the following matters:

- (a) That the landowner within the zone will develop the lot for both a residence and stables and that this intention be supported by an application to commence development.
- (b) The standard of the proposed development is of a sufficiently high quality appropriate for the area.
- (c) That acceptable standards of residential amenity and health will be maintained or improved.
- (d) The appearance of the streetscape will in the opinion of the local government be either maintained or enhanced.

#### 4.3.4 Special Development Precincts

- (1) The provisions of the R Codes relating to front and rear setbacks, car parking and open space may be varied at the discretion of the local government in the four Special Development Precincts, provided Local Planning Policies are adopted for each of the Precincts pursuant to provisions contained in Part 2, Division 2 of the Planning and Development (Local Planning Schemes) Regulations 2015 Schedule 2.
- (2) The provisions of the R Codes relating to plot ratio may be varied at the discretion of the local government on land within the Ascot Waters Special Development Precinct and The Springs Special Development Precinct where the local government considers the development to be in accordance with the character and intent of the Ascot Waters or The Springs locality respectively. *AMD 1 GG* 15/12/2015
- (3) The local government in exercising any such discretion, and in consideration of any application for development approval, shall make its decision having regard to relevant Structure Plans and Local Planning Policies adopted under this Scheme. AMD 1 GG 15/12/2015

(4) Development, strata survey or subdivision within the Belgravia Parklands Precinct will achieve a high degree of compliance with the Subdivision and Development Plan contained in the relevant Local Planning Policy.

#### 4.4 Restrictive Covenants

- 4.4.1 Subject to clause 4.4.2, a restrictive covenant affecting any land in the Scheme area by which, or the effect of which is that, the number of residential dwellings which may be constructed on the land is limited or restricted to less than that permitted by the Scheme, is hereby extinguished or varied to the extent that it is inconsistent with the provisions of the Residential Design Codes which apply under the Scheme.
- 4.4.2 Clause 4.4.1 operates to extinguish or vary a restrictive covenant the local government is not to grant development approval to the development of the land which would, but for the operation of Clause 4.4.1, have been prohibited, unless the application has been dealt with as an 'A' use and has complied with all of the advertising requirements contained in Part 8 of the *Planning and Development (Local Planning Schemes) Regulations 2015* Schedule 2.

#### 4.5 Variations to Site and Development Standards and Requirements

- 4.5.1 If a development is the subject of an application for development approval and does not comply with a standard or requirement prescribed under the Scheme, the local government may, despite the non compliance, approve the application unconditionally or subject to such conditions as the local government thinks fit.
- 4.5.2 In considering an application for development approval under this clause, where, in the opinion of the local government, the variation is likely to affect any owners or occupiers in the general locality or adjoining the site which is the subject of consideration for the variation, the local government is to:
  - (a) consult the affected parties by following one or more of the provisions for advertising uses contained in Clause 64 of the *Planning and Development (Local Planning Schemes) Regulations 2015* Schedule 2.
  - (b) have regard to any expressed views prior to making its determination to grant the variation.
- 4.5.3 The power conferred by this clause may only be exercised if the local government is satisfied that:
  - (a) approval of the proposed development would be appropriate having regard to the criteria set out in Clause 67 of the *Planning and Development (Local Planning Schemes) Regulations 2015* Schedule 2; and
  - (b) the non compliance will not have an adverse effect upon the occupiers or users of the development, the inhabitants of the locality or the likely future development of the locality.

#### 4.6 Environmental Conditions

4.6.1 Environmental conditions to which the Scheme is, or amendments to the Scheme are, subject are incorporated into the Scheme by Schedule 6 of the Scheme.

- 4.6.2 Where appropriate, the environmental conditions are indicated on the Scheme Map by the symbol EC to indicate that environmental conditions apply to the land.
- 4.6.3 The local government is to:
  - (a) maintain a register of all relevant statements published under sections 48F and 48G of the *Environmental Protection Act 1986*; and
  - (b) make the statements available for public inspection at the offices of the local government.

There are no environmental conditions imposed by the Minister for Environment which apply to the Scheme.

Note: Environmental conditions are those required to be incorporated into a Scheme or an amendment to a Scheme following assessment under the *Environmental Protection Act 1986*.

#### 4.7 Residential Zone

- 4.7.1 Where residential land abuts a regional road reserve, vehicular access to that road shall not be permitted for residential development other than for a single house, unless a vehicular access plan has been approved by the responsible authority.
- 4.7.2 All residential development apart from a single house or an existing development (in an unsewered area) is required to be connected to a reticulated sewerage system. If no sewerage is available, development in excess of the single house is not permitted unless such development complies with the requirements of the Government Sewerage Policy.
- 4.7.3 In dealing with development applications involving or contemplating development of land within any of the flexible coded area up to a maximum density of R50 depicted on the Scheme Map, the base R20 code shall apply to any dwelling but may, at the discretion of local government, be increased to a higher code up to the maximum specified provided -
  - (a) The frontage of the parent lot is not less than 16 metres.
  - (b) Any existing building or development which, in the opinion of the local government, is of low quality and incapable of being upgraded to a standard commensurate with new development is demolished; and
  - (c) Development comprising of two or more dwellings in a front to rear arrangement achieves a minimum side setback of 6 metres between the side wall of the first dwelling fronting the public street and the side boundary of the parent lot.
  - (d) Rear dwellings are designed so that significant sections of the front elevations have an outlook to, and are visible from, the public street.
  - (e) A minimum of 50% of the total number of dwellings in the development are two storey where the density exceeds R30.
  - (f) Solid external or internal fencing is not permitted where, in the opinion of the Local government, views from dwellings to the public street will be limited.
  - (g) Dwellings located on the front portion of a lot, or where there is more than one street frontage, are oriented and designed to address all public street(s).

- (h) Dwellings located adjacent to public open space, right of ways, pedestrian access ways and other public spaces are oriented and designed to provide views and surveillance of those public areas; and
- (i) Solar design principles are incorporated in the design and orientation of each dwelling.
- (j) Carports and garages visible from the street are incorporated into the dwelling design so that they are not the dominant feature of the appearance of the dwelling and the streetscape.
- (k) Development on corner lots, or lots with more than one street frontage, have vehicle access provided from the street with lesser traffic.
- (I) The number of crossovers for any development is minimised, having regard to the relevant local planning policy.
- (m) Dwellings that are orientated in a side by side configuration comply with the vehicle access requirements contained within the relevant local planning policy.

#### AMD 1 GG 15/12/2015

- 4.7.4 The local government may vary the requirement that single storey dwellings are permitted only up to an R30 density where not less than one-third of the dwellings are to be used by aged or dependent persons.
- 4.7.5 No density bonus for Aged or Dependent Persons' Dwellings or Single Bedroom Dwellings, which is in addition to the increased density from the minimum site area being reduced by up to one-third, shall be granted on any land within any of the flexible coded areas depicted on the Scheme Map with the exception of the Town Centre Precinct. Any such additional bonus within the Town Centre Precinct shall be subject to compliance with the performance-based criteria contained in the relevant local planning policy.
- 4.7.6 In dealing with development applications involving or contemplating development of land within any of the flexible coded areas other than those with a maximum density of R50 depicted on the Scheme Map, the base R20 code shall apply to any dwelling but may, at the discretion of the local government, be increased to a higher code up to the maximum specified provided -
  - (a) compliance with the requirements of clause 4.7.3 above.
  - (b) in the opinion of the local government there is a high degree of compliance with the performance-based criteria contained in the relevant local planning policy.

AMD 1 GG 15/12/15

- 4.7.7 Subdivision of land within any of the flexible coded areas depicted on the Scheme Map may only be approved where:
  - (a) the subdivision complies with the site area requirements for a residential density of R30; and
  - (b) the application for subdivision approval demonstrates that development on the subdivided lots is capable of compliance with the requirements of clause 4.7.3; or

(c) development on the land has been constructed to plate height in accordance with a development approval granted by the local government, and the subdivision is consistent with that development approval.

#### 4.7.8 Floodway Limit Boundary

No building shall be constructed upon any land within the floodway limit boundary prescribed on the Western Australia Water-Authority Swan River Flood Study Review 1985 or those maps as modified by the Department of Water.

#### 4.8 Special Development Precincts

- 4.8.1 There are five Special Development Precincts which are delineated on the Scheme Map and named as follows:
  - 'Ascot Waters' (generally bound by Grandstand Road, Stoneham Street, Great Eastern Highway and the Swan River, Ascot);
  - 'Invercloy Estate' (generally bounded by Tibradden Circle, Hay Road, Fauntleroy Avenue and Great Eastern Highway, Ascot);
  - 'The Springs' (generally bound by Graham Farmer Freeway, Great Eastern Highway, Brighton Road and the Swan River, Rivervale); and
  - 'Belgravia Residential Estate' (generally bounded by Belgravia Street, Barker Street, Daly Street and Mixed Use zoned lots fronting Daly Street and Belgravia Street.
  - 'Belvidere Main Street Precinct' (generally adjacent to Belvidere Street between Leake Street and Keymer Street).
- 4.8.2 All development, including single houses, within the Special Development Precincts shall require the development approval of the local government.
- 4.8.3 All development shall be subject to compliance with the performance-based criteria contained in the relevant local planning policy.

#### 4.9 Residential and Stables Zone

- 4.9.1 In the absence of a specific residential density coding for the zone, the minimum lot area shall be 1000 square metres with no more than one dwelling unit per 1000m<sup>2</sup>.
- 4.9.2 For requirements relating to "Open Space" and "minimum setbacks from boundaries" under the Residential Design Codes refer to clause 4.3.3.
- 4.9.3 Use Controls: Notwithstanding provision of the Zoning Table to the contrary, uses permitted within the zone are:

The stabling and training of horses, and with the approval of the local government:

- (a) the storage of commercial vehicles used in the transport of stock; and
- (b) any other use incidental to the horse racing industry.
- 4.9.4 Setback of Stables and Yards from Site Boundaries
  - (1) No part of any stable shall be sited less than:

- (a) 24m from the frontage of the lot;
- (b) 6m from any other street boundary of the lot. The local government may allow zero setbacks on the other boundaries; and
- (2) Yards may at the discretion of the local government be located up to a secondary street boundary provided a 1.8 metre high brick wall is erected on the property boundary.

#### 4.9.5 Proposals for Stables or Residence Only

An application for development approval for a stables use only or a residence only shall comply with subclause 4.9.4 and make provision for the possibility of locating both a residence and a minimum of two stables on the lot.

#### 4.10 Town Centre and Commercial Zone

- 4.10.1 Proposals for land use and development are to demonstrate good urban design by:
  - (a) the presentation of buildings and facades that are attractive and inviting, and which harmoniously relate with each other, and have regard to climate; and
  - (b) the creation of spaces which encourage pedestrian movement and provide places for pedestrians to congregate.
- 4.10.2 Site and Development Requirements
  - (1) The extent of development shall be governed by the local government's requirements for car parking and landscaping determined in the light of the circumstances of any particular application.
  - (2) The local government, may as a condition(s) of development approval, require amongst other things the integration of building layout and design with adjoining development and determine car parking layout, vehicular access and pedestrian circulation.
  - (3) No part of any building shall be built upon that area of land between the street alignment and the building setback line drawn parallel thereto a distance of two metres within the site.

#### 4.11 Mixed Use Zone

- 4.11.1 Site and Development Requirements: The following site and development requirements shall apply within the Mixed Use Zone with the exception of residential development which, subject to subclause 4.11.6, shall conform with the provisions of the Residential Design Codes.
  - (a) External Space: The use of external space for storage or the conduct of any business activity associated with a premises is prohibited.
  - (b) Lot Area and Dimensions: The minimum lot area shall be 2000 square metres with a minimum effective frontage of 30 metres. However, the local government may support subdivision or approve development of a lot with a lesser area or a lesser effective frontage in circumstances where:

- (i) The lot existed prior to the approval of this Scheme, or
- (ii) The local government is satisfied that the proposal appropriately addresses issues relating to:
  - (1) Motor vehicle access, parking and circulation;
  - (2) Easements in gross for the creation of shared accessways;
  - (3) Rubbish disposal; and
  - (4) Building footprints and design, or
- (iii) The Commission has approved a subdivision of the subject land which creates a lot or lots with an area or effective frontage less than those nominated in this clause.
- (c) Lot Coverage: Building including accessory buildings shall not cover a total of more than 60 percent of the area of any lot. The local government may consider an increase to 70 percent of the area of any lot where the coverage includes a multi storey car park associated with the development which consists of a ground floor and uncovered upper deck.
- (d) Setbacks of Buildings from Site Boundaries: No part of any building shall be built upon that area of land between the street alignment and the building setback line drawn parallel thereto a distance of 15 metres within the site. In regard to any site having more than one street frontage, the minimum setback from the street alignment shall apply to the frontage of the site to the road or roads of higher category as determined by the local government and the setback from the lesser roads shall not be less than a distance of 7.5 metres.

The local government will only permit averaging of building setbacks in those cases where it is satisfied that the functioning of any proposed use will be improved without detriment to the appearance of the development or the street scene or to the safe movement of traffic on or about the site and in case will the local government permit a part of any building to be built closer to the street than one half of an average building setback.

- (e) Setback Areas: No use of the area between the street alignment and building setback lines shall be permitted other than for planting or for pedestrian and vehicular circulation and vehicle parking in accordance with the provisions of this Scheme, except that an area up to 25 percent of the building setback area may be used for trade display purposes with the approval of the local government.
- (f) Pedestrian and Garden Areas: No less than 3 metres of the building setback area to the primary street frontage and 1 metre to the secondary street frontage must be set aside, developed and maintained as garden space for pedestrian use only. A landscaping and reticulation plan must be submitted to the local government for approval. The landscaping subsequently carried out shall be in accordance with the approved plan. The local government will require as a condition of development approval the reticulation and landscaping of the street verge.

#### 4.11.2 Parking and Loading Requirements

Adequate off-street parking shall be provided on site in accordance with the requirements of Table 2, together with adequate parking space for customers and visitors as required by the local government. The local government shall also require adequate space for parking, loading and unloading of trade vehicles to be provided on site.

#### 4.11.3 Vehicular Cross Access

Where necessary vehicular cross access shall be provided over all parcels of land to give customer and service traffic access to streets wherever access to Great Eastern Highway or other important roads is inappropriate or should be minimised. Cross access provision shall take the form of easements in gross or encumbrances on titles granting right of carriageway with the local government a party to the agreements. All documentation costs shall be met by respective owners.

#### 4.11.4 Building Facades

The primary street frontage of all buildings must have a facade predominantly constructed of brick, concrete, glass or steel or a combination of these materials acceptable to the local government. Where a proposed structure is to extend from one street frontage to another, as distinct from the double street frontage of a corner lot, the there must be a full height facade to both street frontages of the above materials or similar materials to acceptable to the local government.

In the cases of walls fronting secondary streets, the construction must be as above to a minimum height of two metres except that in the case of a building having walls in excess of five metres to plate height the construction must be increased to half the vertical height of the wall. The local government shall retain the discretion to determine which is a primary and which is a secondary street frontage.

#### 4.11.5 Fencing and Walls

- (1) It is the local government's intention that in the Mixed Use zone fencing facing any street frontage should be kept to a minimum to allow for an attractive and open streetscape. In special circumstances, the local government may allow fencing facing a street frontage or along a secondary street frontage, provided that in the opinion of the local government it is of exceptionally high quality, is visually permeable, and visually appealing.
- (2) Notwithstanding clause 4.5, in the Mixed Use zone:
  - (a) the use of cyclone link mesh fencing is prohibited for any fence other than a boundary fence; and
  - (b) the use of barbed wire in any fence is prohibited.

#### 4.11.6 Discretion to Vary Standards:

Without affecting the generality of clause 4.2.3, for residential development in the Mixed Use Zone the local government may allow a relaxation or variation of the standards and requirements applicable to the Mixed Use Zone if the local government is satisfied that the matters referred to in items (a) and (b) conditioning the discretion in clause 4.5.3 are complied with.

#### 4.12 Mixed Business Zone

#### 4.12.1 Site and Development Requirements

The following site and development requirements shall apply within the 'Mixed Business' zone with the exception of residential development which, subject to subclause 4.12.6 shall conform with the provisions of the Residential Design Codes.

#### (a) External Space:

- (i) The use of any space external to a building for the conduct of any business activity such as fabrication, or manufacturing, or servicing of vehicles or equipment is prohibited.
- (ii) The outdoor storage of goods, materials, liquid, machinery, equipment or any other thing is prohibited, except where approved by the City pursuant to an application for development approval.

An application for outdoor storage may only be approved if the proposed development complies with all the following provisions:

- (1) Visual Amenity: Outdoor storage areas must be screened from public view, and must not be located between the street alignment(s) and the building setback line(s). Where trees are used for screening they must be mature trees or non-visually permeable screens required; and
- (2) Site Function: Outdoor storage shall be in a designated area and not conflict with areas used or needed for vehicle parking, loading, manoeuvring or access, landscaping, drainage or effluent disposal, and any other thing or area considered by the City to be necessary for the proper functioning of the development approved on the land; and
- (3) Natural Environment: Outdoor storage areas must be located on an adequately paved and drained surface. If in the view of the City the things to be stored, or the proposed method of storage, pose an unacceptable risk to the natural environment, the outdoor storage shall not be permitted; and
- (4) Health and Safety: Where appropriate an application for outdoor storage shall be accompanied by a report which addresses matters such as fire risk, impact on human health and safety, drainage, and compliance with relevant legislation. The City may require an applicant to prepare such a report, and/or a management plan to address any matter associated with the outdoor storage which in the view of the City requires ongoing management; and
- (5) If outdoor storage involves the use of sea containers: the sea containers may only be stored at ground level; may not be stacked one on top of the other; must be screened off and behind the primary street building line.
- (iii) Where outdoor storage is approved, the outdoor storage area shall be maintained in a tidy, safe and orderly condition, and in accordance with any approved management plan.
   AMD 10 (Pending - public advertising closes 24/08/2018)
- (b) Lot Area and Dimensions: The minimum lot area shall be 2,000 square metres with a minimum effective frontage of 30 metres. However, the local government may

support subdivision or approve development of a lot with a lesser area or a lesser effective frontage in circumstances where:

- (i) The lot existed prior to the approval of this Scheme, or
- (ii) The local government is satisfied that the proposal appropriately addresses issues relating to:
  - (1) Motor vehicle access, parking and circulation;
  - (2) Easements in gross for the creation of shared accessways;
  - (3) Rubbish disposal; and
  - (4) Building footprints and design, or
- (iii) The Commission has approved a subdivision of the subject land which creates a lot or lots with an area or effective frontage less than those nominated in this clause.
- (c) Lot Coverage: Building including accessory buildings shall not cover a total of more than 60 percent of the area of any lot. The local government may consider an increase to 70 percent of the area of any lot where the coverage includes a multi storey car park associated with the development which consists of a ground floor and uncovered upper deck.
- (d) Setbacks of Buildings from Site Boundaries: No part of any building shall be built upon that area of land between the street alignment and the building setback line drawn parallel thereto a distance of 15 metres within the site. In regard to any site having more than one street frontage, the minimum setback from the street alignment shall apply to the frontage of the site to the road or roads of higher category as determined by the local government and the setback from the lesser roads shall not be less than a distance of 7.5 metres.

The local government will only permit averaging of building setbacks in those cases where it is satisfied that the functioning of any proposed use will be improved without detriment to the appearance of the development or the street scene or to the safe movement of traffic on or about the site and in no case will the local government permit a part of any building to be built closer to the street than one half of an average building setback.

- (e) Setback Areas: No use of the area between the street alignment and building setback lines shall be permitted other than for planting or for pedestrian and vehicular circulation and vehicle parking in accordance with the provisions of this Scheme, except that an area up to 25 percent of the building setback area may be used for Trade Display purposes with the approval of the local government.
- (f) Pedestrian and Garden Areas: No less than 3 metres of the building setback area to the primary street frontage and 1 metre to the secondary street frontage must be set aside, developed and maintained as garden space for pedestrian use only. A landscaping and reticulation plan must be submitted to the local government for approval. The landscaping subsequently carried out shall be in accordance with the approved plan. The local government will require as a condition of development approval the reticulation and landscaping of the street verge.

# 4.12.2 Parking and Loading Requirements

Adequate off-street parking shall be provided on site in accordance with the requirements of Table 2 of the Scheme, together with adequate parking space for customers and visitors as required by the local government. The local government shall also require adequate space for parking, loading and unloading of trade vehicles to be provided on site.

#### 4.12.3 Vehicular Cross Access

Where necessary vehicular cross access shall be provided over all parcels of land to give customer and service traffic access to streets wherever access to Great Eastern Highway or other important roads is inappropriate or should be minimised. Cross access provision shall take the form of easements in gross or encumbrances on titles granting right of carriageway with the local government a party to the agreements. All documentation costs shall be met by respective owners.

## 4.12.4 Building Facades

- (1) The primary street frontage of all buildings must have a facade predominantly constructed of brick, concrete, glass or steel or a combination of these materials acceptable to the local government. Where a proposed structure is to extend from one street frontage to another, as distinct from the double street frontage of a corner lot, there must be a full height facade to both street frontages of the above materials or similar materials acceptable to the local government.
- (2) In the cases of walls fronting secondary streets, the construction must be as above to a minimum height of two metres except that in the case of a building having walls in excess of five metres to plate height the construction must be increased to half the vertical height of the wall. The local government shall retain the discretion to determine which is a primary and which is a secondary street frontage.

## 4.12.5 Fencing and Walls

- (1) It is the local government's intention that in the Mixed Business zone fencing facing any street frontage should be kept to a minimum to allow for an attractive and open streetscape. In special circumstances, the local government may allow fencing facing a street frontage or along a secondary street frontage, provided that in the opinion of the local government it is of exceptionally high quality, is visually permeable, and visually appealing.
- (2) Notwithstanding clause 4.5, in the Mixed Business zone:
  - (a) the use of cyclone link mesh fencing is prohibited for any fence other than a boundary fence; and
  - (b) the use of barbed wire in any fence is prohibited.

## 4.12.6 Discretion to Vary Standards:

Without affecting the generality of clause 4.2.3, for residential development in the 'Mixed Business' zone the local government may allow a relaxation or variation of the standards and requirements applicable to the 'Mixed Business' zone if the local government is satisfied that the matters referred to in items (a) and (b) conditioning the discretion in clause 4.5.3 are complied with.

#### 4.13 Industrial Zone

- 4.13.1 Site and Development Requirements: The following site and development requirements shall apply to all applications for development approval within the Industrial Zone.
  - (a) Amenity: No use or other development of land within the Industrial Zone shall cause in the opinion of the local government nuisance or annoyance by way of noise or other emissions beyond the site on which it is are conducted.

When considering the design of a building proposed to be erected in the Industrial Zone the local government shall have regard to and comply with the following provisions:

- the local government must be satisfied that the building will comply with any objectives adopted by the local government from time to time for industrial development, and if appropriate, be aesthetically compatible and integrated with the exterior design of neighbouring buildings;
- (ii) in considering the design of any building the local government must give consideration to the relationship which the shape and siting of the building bears to the shape of the lot (or lots) on which erection is to take place.
- (iii) The storage of goods and the carrying out of industrial activities in open yard areas visible from a street is not permitted. Such areas must be screened from public view by a closed wall or fence constructed of materials approved by the local government.
- (b) Maximum Building Height: No part of any building shall exceed a height fixed in accordance with clause 4.20 'Structures Height Control Contours Map' as a maximum height in relation to the safe operation of Perth International Airport.
- (c) Maximum Site Cover: No site shall be built upon so as to exceed a site coverage of 70 percent.
- (d) Setback of Buildings from Site Boundaries: No part of any building shall be built upon that area of land between the street alignment and the building setback line drawn parallel thereto an average distance of 15 metres within the site.

In regard to any lots having multiple street frontages, the minimum setback from the street alignment shall apply to the frontage of the site to the road or roads of higher category as determined by the local government and the setback from the lesser roads shall not be less than an average of 9 metres.

Parapet walls may be erected on side and/or rear boundaries if constructed of brick or concrete panels coloured to the satisfaction of the local government.

In the case of walls set back from side and/ or rear boundaries, the local government shall require the use of materials of a colour and texture consistent with the intention of the zone and complementary to existing or surrounding developments.

(e) Building Setback Areas: No use of the area between the street alignment and building setback lines shall be permitted other than for planting or for pedestrian and vehicular circulation and parking in accordance with the requirements of Table 2 except that not more than 25 percent of the setback area may be used for trade display purposes approved by the local government.

#### 4.13.2 Parking and Loading Requirements

- (1) Any person undertaking any development within the Industrial Zone shall submit a plan for the approval of the local government showing the number, layout and dimensions of parking and loading spaces and the proposed access thereto. In those cases where the local government is satisfied that a number of spaces less than those stipulated in Table 2 is appropriate, it may grant approval, subject to the number of spaces required being not less than 50 percent of the requirements of Table 2 and then only on the condition that adequate space is reserved to meet the full parking requirement should it be needed at any future time.
- (2) The local government shall consider such plans having regard to the adequacy, efficiency and safety of proposals for employee, visitor and customer parking, for the loading and unloading of vehicles and for access to and from the site.

#### 4.13.3 Pedestrian and Garden Areas

No less than 3 metres of the building setback area to the primary street frontage and 2 metres to the secondary street frontage must be set aside, developed and maintained as garden space for pedestrian use only. A landscaping and reticulation plan must be submitted to the local government for approval. The landscaping subsequently carried out shall be in accordance with the approved plan. The local government may require as a condition of development approval the reticulation and landscaping of the street verge.

#### 4.13.4 Facades

- (1) The local government shall require the primary street frontage of all buildings to have a facade predominantly constructed of brick, concrete, glass or steel or combinations of those or similar materials acceptable to the local government. Where a proposed structure is to extend from one street frontage to another, as distinct from the double street frontage of a corner lot, the local government shall require a full height facade of the above or similar materials to both street frontages.
- (2) In the case of walls fronting secondary streets, the local government shall require construction to be as above to a minimum height of two metres except that in the case of a building having walls in excess of five metres to plate height the local government shall require the brickwork to be increased to half the vertical height of the wall. The local government shall retain the discretion to determine which is a primary and which is a secondary street.

#### 4.13.5 Fencing and Walls

- (1) It is the local government's intention that in the Industrial Zone, fencing facing any street frontage should be kept to a minimum to allow for an attractive and open streetscape. In special circumstances the local government may allow fencing facing a street frontage, provided that in the opinion of the local government it is of exceptionally high quality, is visually permeable and is visually appealing.
- (2) In the Industrial Zone, fencing shall be constructed only out of high quality materials, such as brick piers with open infill, which open infill may include wrought iron, powder coated steel, painted wood pickets or other comparable materials specifically approved by the local government.

- (3) Notwithstanding the desirability of allowing fencing only if it is visually permeable and otherwise compliant with the provisions of this clause, sections of solid fencing will be permitted where they are required in order for a development to comply with sub-clause 4.13.1(a)(ii). In these cases the local government may require the solid fencing to be visually enhanced and complemented by landscaping areas.
- (4) Notwithstanding clause 4.5, in the Industrial zone:
  - (a) the use of cyclone link mesh fencing is prohibited for any fence other than a boundary fence; and
  - (b) the use of barbed wire in any fence is prohibited.

#### 4.13.6 Subdivision of land in the Kewdale Industrial Estate

When considering applications for the subdivision of land in the Kewdale Industrial Estate the local government shall have regard the provisions of any Local Planning Policy and the objectives which the policy is designed to achieve. Where reticulated sewerage is available the local government will support the creation of lots less than 4000sqm in area provided:

- (a) no lot is less than 2000sqm in area with a minimum frontage of 25 metres;
- (b) an overall plan of subdivision is prepared in order to demonstrate the subdivision potential of the superlot in which the subdivision is to take place;
- (c) no lot is permitted direct access to a regional road;
- (d) vehicular cross easements are granted in appropriate circumstances as determined by the local government; and
- (e) an environmental assessment of the land to be subdivided is carried out.

## 4.14 Public Assembly Zone

- 4.14.1 Proposals for land use and development are to demonstrate good urban design by:
  - (a) the presentation of buildings and facades that are attractive and inviting, and which harmoniously relate with each other, and have regard to climate; and
  - (b) the creation of spaces which encourage pedestrian movement and provide places for pedestrians to congregate.

## 4.14.2 Site and Development Requirements

- (1) The extent of development shall be governed by the local government's requirements for car parking and landscaping determined in the light of the circumstances of any particular application.
- (2) The local government, may as a condition(s) of development approval, require amongst other things the integration of building layout and design with adjoining development and determine car parking layout, vehicular access and pedestrian circulation.
- (3) Setbacks of Buildings from Site Boundaries: No part of any building shall be built upon that area of land between the street alignment and the building setback line

drawn parallel thereto a distance of 15 metres within the site. In regard to any site having more than one street frontage, the minimum setback from the street alignment shall apply to the frontage of the site to the road or roads of higher category as determined by the local government and the setback from the lesser roads shall not be less than a distance of 7.5 metres.

Side or rear setbacks where a Residential land use abuts shall be a minimum of 4 metres and shall have regard to the potential impact of existing and future uses on the amenity of those residents.

(4) Pedestrian and Garden Areas: No less than 3 metres of the building setback area to the primary street frontage and 1 metre to the secondary street frontage must be set aside, developed and maintained as garden space for pedestrian use only. A landscaping and reticulation plan must be submitted to the local government for approval. The landscaping subsequently carried out shall be in accordance with the approved plan. The local government will require as a condition of development approval the reticulation and landscaping of the street verge.

## 4.15 Service Station Zone

#### 4.15.1 Site and Development Requirements:

- (a) External Space: The use of external space for storage or the conduct of any business activity associated with a premises is prohibited.
- (b) Lot Coverage: Building including accessory buildings and petrol bowser shade shall not cover a total of more than 60 percent of the area of any lot.
- (c) Setbacks of Buildings from Site Boundaries: No part of any building shall be built upon that area of land between the street alignment and the building setback line drawn parallel thereto a distance of 15 metres within the site. In regard to any site having more than one street frontage, the minimum setback from the street alignment shall apply to the frontage of the site to the road or roads of higher category as determined by the local government and the setback from the lesser roads shall not be less than a distance of 7.5 metres.

Side or rear setbacks where a Residential land use abuts shall be a minimum of 4 metres and shall have regard to the potential impact of existing and future uses on the amenity of those residents.

(d) Pedestrian and Garden Areas: No less than 3 metres of the building setback area to the primary street frontage and 1 metre to the secondary street frontage must be set aside, developed and maintained as garden space for pedestrian use only. A landscaping and reticulation plan must be submitted to the local government for approval. The landscaping subsequently carried out shall be in accordance with the approved plan. The local government will require as a condition of development approval the reticulation and landscaping of the street verge.

## 4.15.2 Parking and Loading Requirements

Adequate off-street parking shall be provided on site in accordance with the requirements of Table 2, together with adequate parking space for customers and visitors as required by the local government. The local government shall also require adequate space for parking, loading and unloading of trade vehicles to be provided on site.

#### 4.15.3 Vehicular Cross Access

Where necessary vehicular cross access shall be provided over all parcels of land to give customer and service traffic access to streets wherever access to Great Eastern Highway or other important roads is inappropriate or should be minimised. Cross access provision shall take the form of easements in gross or encumbrances on titles granting right of carriageway with the local government a party to the agreements. All documentation costs shall be met by respective owners.

#### 4.15.4 Building Facades

The primary street frontage of all buildings must have a facade predominantly constructed of brick, concrete, glass or steel or a combination of these materials acceptable to the local government. Where a proposed structure is to extend from one street frontage to another, as distinct from the double street frontage of a corner lot, the there must be a full height facade to both street frontages of the above materials or similar materials to acceptable to the local government.

In the cases of walls fronting secondary streets, the construction must be as above to a minimum height of two metres except that in the case of a building having walls in excess of five metres to plate height the construction must be increased to half the vertical height of the wall. The local government shall retain the discretion to determine which is a primary and which is a secondary street frontage.

Where a development abuts residential land use the design must have regard for the minimisation of noise and light spill.

## 4.15.5 External Storage

External Space: The use of external space for storage or the conduct of any business activity associated with a premises is prohibited.

# 4.16 Vehicle Parking and Loading

- 4.16.1 Any person who constructs or substantially reconstructs, alters or adds to a building or changes the use of any land or building shall make provision in accordance with the requirements of subclause 4.16.3 for vehicles used in conjunction with the site (whether by the occupiers, their employees or invitees or other persons) to stand on or, in the opinion of the local government, sufficiently close to the site but not on a street, while being loaded or unloaded or awaiting use.
- 4.16.2 In circumstances where alterations and/or extensions to an existing development give rise to the need for temporary car parking on land not associated with the alterations and or extensions, the local government may approve the establishment of temporary parking subject to such conditions and time limits as it deems appropriate.
- 4.16.3 Size and Location of Car Spaces and Loading Facilities:

When considering any Development Application, the local government must have regard to and impose conditions to the location and design of the car parking spaces and loading facilities. In particular, the local government must take into account, and may impose conditions concerning:

(a) the proportion of spaces to be roofed or covered;

- (b) the proportion of spaces to be below natural ground level;
- (c) the means of access to each space and the adequacy of any manoeuvring area;
- (d) the location of the spaces on the site and their effect if spaces should later be roofed or covered:
- (e) the adequacy of proposed screening or planting;
- (f) the extent to which spaces are located within required setback areas;
- (g) the incorporation of Crime Prevention through Environmental Design principles;
- (h) the location of proposed footpaths and the effect on traffic movement and safety; and
- (i) the location of proposed accessways on and off public roads and the effect on traffic movement and safety.

#### 4.16.4 Number and Layout of Parking and Loading Spaces to be Provided

- (1) The number of spaces to be provided in respect of any particular site shall be determined by the local government, having regard to the nature of the use and the known or likely volume of goods, material or people moving to and from the site. Subject to any provision of the Scheme to the contrary the number of car parking spaces shall be in accordance with the requirements of Table 2.
- (2) Layout of parking spaces shall be in accordance with Schedule No. 7 or, at the discretion of the local government, the relevant Australian Standard.
- (3) When making decisions relative to parking provision, the local government shall at all times have regard to any existing or proposed public parking facilities nearby.

Table 2 - Car Parking Requirements

	Minimum Daukina Dunasinian
Land Use/Use Class	Minimum Parking Provision
Amusement Centre/ Betting	1 space for every 10m <sup>2</sup> of NLA
Agency	Doubles to be at the discustion of the level seven seet
Auction Mart	Parking to be at the discretion of the local government
Automotive & Marine Sales	1 space for every 40m² of vehicle display showroom plus
Premises	1 space per 50m² outdoor trade display.
Caretaker's Dwelling	1 space per dwelling
Child Day Care Centre	1 space for every employee plus 1 space per every 8 children allowed under maximum occupancy.
Civic Use/	1 space for every 4 persons whom the building is
Club Premises/	designed to accommodate.
Night Club/	
Public Amusement/	
Public Worship/	
Reception Centre	
Community Home	1 space per employee or staff member plus 1 space for every 3 beds
Community Purposes	Parking to be negotiated with the local government
Consulting Rooms &	4 spaces for every practitioner
Consulting Rooms - Group	
Convenience Store	6 spaces per 100m <sup>2</sup> of GLA for any shop plus 1 space per
	petrol pump
Corner Shop/ Home Store	5 spaces in addition to residential requirement
Educational Establishment	Education Establishment (Private Primary School)
	1 space per classroom.
	Education Establishment (Private Secondary School)  1 space per classroom, plus 1 space for every 25 students the school is designed to accommodate for the final year of secondary education.
	Education Establishment (Private Tertiary)
	1 space for every 6 students the building is designed to accommodate, plus 1 for each faculty staff member, or employee, plus additional requirements for auditoriums or stadiums.
	Education Establishment (Other)  1 space for every 4 students the establishment is designed to accommodate.
Family Day Care	A paved pick-up area in addition to residential requirements
Fuel Depot/	1 space per 75m <sup>2</sup> GFA or 1 space per employee
Transport Depot	whichever is the greater
Funeral Parlour	6 spaces plus 1 space for every 4 persons where a chapel is included
Health Centre	4 spaces for every practitioner plus 1 bay per 20m² GFA available to the public
Health Studio	1 space for every 20m <sup>2</sup> of NLA
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Land Use/Use Class	Minimum Parking Provision
AMD 1 GG 15/12/2015	
Hospital	1 space for every 4 hospital beds plus 1 space for each staff member on duty
Hotel/Motel	1 space for every 2m <sup>2</sup> of bar and lounge floor area. (1 space for every 4m <sup>2</sup> of seating only areas) plus 1 space for every bedroom. Where other facilities are provided parking is to be negotiated with the local government.
Industry - Cottage	1 space per employee in addition to residential requirements
Industry - Light	1 space for every 50m2 of open space used for industrial purposes, plus 1 space for every 50m² of GFA; or 1 space for each employee, whichever is the greater.
Industry - Service	1 space for every 50m² of open space used for industrial purposes, plus 1 space for every 50m² of GFA; or 1 space for each employee, whichever is the greater.
Industry/Workshop/Factory	1 space for every 50m <sup>2</sup> of open space used for industrial purposes, plus 1 space for every 50m <sup>2</sup> of GFA; or 1 space for each employee, whichever is the greater.
Laundromat	1 per 20m2 of NLA
Lunch Bar	6 spaces for every 100m <sup>2</sup> of NLA
Nursing Homes/Aged Persons Hostels	1 space for every 4 beds 1 space for every employee
Office	1 space for every 30m <sup>2</sup> of NLA or 1 space for each employee, whichever is greater
Plant Nursery	1 space for every 50m <sup>2</sup> of display and sales area
Restaurant	1 space for every 4 seats
Restricted Premises	6 spaces for every 100m <sup>2</sup> NLA
Service Station	1 space for every service bay plus 1 space for every employee (bowser bays shall be excluded from any calculation of parking bays)
Shop	6 spaces per 100m <sup>2</sup> of NLA
Showroom	1 space for every 40m <sup>2</sup> of NLA
Studio	1 space per 40m² display area plus 1 bay per employee
Take Away/ Fast Food Outlet	6 spaces for every 100m <sup>2</sup> of NLA plus 1 space for every 4 seated customers (car queuing areas may be permitted to be calculated as parking spaces)
Tavern	1 space for every 2m <sup>2</sup> of bar and lounge floor area (1 space for every 4m <sup>2</sup> of seating only areas)
Vet Consulting Rooms	4 spaces for every practitioner
Vet Hospital	2 spaces for every employee
Video Store	6 spaces for every 100m <sup>2</sup> NLA
Warehouse	1 space for every 100m <sup>2</sup> of GFA plus 1 space for every 100m <sup>2</sup> of open space used for warehousing purpose.
Warehouse Retail Outlet	6 spaces per 100m <sup>2</sup> of Gross Floor Area (GFA) plus 1 space per 100m <sup>2</sup> of open space used for warehousing (storage) purpose.
Any other use	To be determined by the local government

## 4.16.5 Cash In Lieu of Car Parking

(1) An applicant for development approval may, if the local government so agrees, make a shortfall cash payment to the local government in lieu of the provision of all or any of the number of car parking spaces required by the Scheme.

## (2) In this clause:

"Bay Size" means the area of land, in square metres, which in the opinion of the local government is required for the provision of a standard car parking space;

"Construction Cost" means the estimated cost of constructing and developing a car parking space of the Bay Size including the manoeuvring area and associated services as determined by an engineer or architect appointed by the local government. The construction cost shall include the aisle and reversing space and servicing of the car parking facility;

"Land Value per m2" means the estimated value per square metre of land in the locality as determined by a licensed valuer appointed by the local government;

"Shortfall" means the difference between the number of the car parking spaces required by the Scheme and the number of car parking spaces to be provided by the applicant.

# (3) In this clause:

"Shortfall Cash Payment" means

- (a) the amount fixed as such in a planning policy made by the local government; or
- (b) where the local government has not made such a planning policy within the period of 12 months preceding the grant of the development approval, means an amount calculated according to the following formula: Shortfall x [(Bay Size x Land Value per m2) + Construction Cost]
- (4) In deciding to fix the Shortfall Cash Payment the local government is to have regard to:
  - (a) the amount calculated in accordance with the formula set out in subclause (3); and
  - (b) any factors which the local government considers justify varying that amount including, without limitation, the existence of any specified area rate or service charge imposed by the local government to secure increased public parking areas.
- (5) The local government may make a planning policy specifying the areas within which it may require or accept payment of a Shortfall Cash Payment.
- (6) The monies received by the local government under this clause shall be paid into a reserve fund and shall only be used:
  - (a) for the provision of public parking;
  - (c) for reimbursing the local government for any expenses incurred for the

purpose of paragraph (a) including loan repayments.

(7) An applicant may, prior to the commencement of the development which is the subject of the development approval, refer any dispute concerning the Construction Cost or the Land Value per m2 for determination by a single arbitrator agreed by the parties, or failing agreement, nominated by the Minister at the request of either the applicant or the local government, to be determined in accordance with the *Commercial Arbitration Act 1985*.

# 4.17 Bicycle Parking

- 4.17.1 In the case of Uses listed in Table 3, having regard to the likely demand, the local government may require bicycle parking bays to be provided at the ratio specified in that table in addition to the number of car parking bays to be provided.
- 4.17.2 When considering an application for development approval for development in respect of which bicycle parking bays are required to be provided under the Scheme, the local government shall have regard to, and may impose conditions as to the design and location of such bays.
- 4.17.3 Where bicycle parking spaces are required to be provided for the use of staff, shower and changing facilities shall be provided, the design and location of such facilities being to the satisfaction of the local government, and the number of such facilities being at the following ratios:
  - (a) number of secure well ventilated equipment lockers 1 per cycle parking space.
  - (b) number of showers: 1 male and 1 female shower in separate change rooms per 10 cycle parking spaces, 2 male and 2 female showers in separate change rooms 10-30 cycle parking spaces, 3 male and 3 female showers in separate change room 30+ cycle parking spaces.

Table 3 - Bicycle Parking Requirements

Land Use/Use Class	Employee/Besident	Visitor Parking Space
Land Use/Use Class	Employee/Resident Parking Spaces	Visitor Parking Spaces
District Centre	At discretion of local	1 per 200m <sup>2</sup> NLA for visitors
District Certife	government	I per 20011 NLA for visitors
Amusement Centre /	At discretion of local	At discretion of local
Betting Agency	government	government
Auction Mart	At discretion of local	At discretion of local
/ dollon wart	government	government
Automotive & Marine	At discretion of local	At discretion of local
Sales Premises	government	government
Caretakers Dwelling	N/A	N/A
Child Care Day Centre	At discretion of local	At discretion of local
	government	government
Civic Use/	At discretion of local	At discretion of local
Night Club/	government	government
Public Amusement/	germann	30.000
Public Worship/		
Reception Centre/		
Club Premises	N/A	1 per 40m <sup>2</sup> GFA
Community Home		
Community Purposes	At discretion of local	2 + 1 Per 1500m <sup>2</sup> GFA
, , , , , , , , , , , , , , , , , , , ,	government	
Consulting Rooms &	At discretion of local	1 per 4 practitioners
Consulting Rooms -	government	
Group		
Convenience Store	N/A	1 per 25m² GFA
Corner Shop / Home	N/A	1 per 25m <sup>2</sup> GFA
Store		•
Educational		
Establishment-		
Primary School	At discretion of local	1 per 5 pupils
All other	government	1 per 3 students
	At discretion of local	
	government	
Family Day Care	At discretion of local	At discretion of local
	government	government
Fuel Depot / Transport	At discretion of local	At discretion of local
Depot	government	government
Funeral Parlour	N/A	N/A
Health Care Centre	1 per 400m <sup>2</sup> GFA	1 per 200m <sup>2</sup> GFA
Health Studio	1 space for every 200m <sup>2</sup> of GFA	1 space for every 200m <sup>2</sup>
AMD 1 GG 15/12/2015		
Hotel/Motel	1 per 25m <sup>2</sup> GFA bar floor area	1 per 100m <sup>2</sup> GFA of lounge,
		dining and function areas
Industry - Cottage	At discretion of local	At discretion of local
	government	government
Industry - Light	2 per 1000m <sup>2</sup> NLA	N/A
Workshop/Factory		
Industry - Service	2 per 1000m² NLA	N/A
Industry General	3 per 1000m² NLA	N/A
Laundromat	N/A	At discretion of local
		government

Persons government government  Office 1 per 200m² GFA; 1 per 750m² GFA; Less than 200m² GFA At discretion of local government  Plant Nursery At discretion of local government  Plant Nursery At discretion of local government  Restaurant 1 per 100 seats; 1 per 50 seats; less than <50 seats At discretion of local government  Restricted Premises At discretion of local government  Service Station At discretion of local government  Shop At discretion of local government  Showroom 1 per 1000m² sales floor  Studio At discretion of local government  Takeaway / Fast Food Outlet  Studio 1 per 100m² GFA 1 per 50m² GFA  1 per 50m² GFA  1 per 50m² GFA  1 per 50m² GFA  1 per 50m² GFA  1 per 50m² GFA	Land Use/Use Class	Employee/Resident Parking Spaces	Visitor Parking Spaces
Office 1 per 200m² GFA; 1 per 750m² GFA; Less than 200m² GFA At discretion of local government  Plant Nursery At discretion of local government  Restaurant 1 per 100 seats; 1 per 50 seats; less than 450 seats At discretion of local government  Restricted Premises At discretion of local government  Service Station At discretion of local government  Shop At discretion of local government  Showroom 1 per 1000m² sales floor  Studio At discretion of local government  Takeaway / Fast Food Outlet  1 per 200m² GFA At discretion of local government  1 per 750m² GFA; Less than 750m² GFA At discretion of local government  At discretion of local government  At discretion of local government  1 per 25 GFA of retail component  1 per 25m² GFA  1 per 1000m² sales floor  2 per 1000m² sales floor  3 per 1000m² sales floor  4 discretion of local government  Takeaway / Fast Food Outlet	Nursing Homes / Aged	At discretion of local	At discretion of local
Less than 200m² GFA At discretion of local government  Plant Nursery  At discretion of local government  Restaurant  1 per 100 seats; Less than 100 seats – At discretion of local government  Restricted Premises  At discretion of local government  Service Station  At discretion of local government  Shop  At discretion of local government  Showroom  1 per 1000m² sales floor  Studio  Less than 100 seats – At seats At discretion of local government  1 per 25 GFA of retail component  1 per 25m² GFA  1 per 1000m² sales floor  At discretion of local government  Takeaway / Fast Food Outlet  1 per 100m² GFA  1 per 50m² GFA			
discretion of local government   local government	Office		1 per 750m <sup>2</sup> GFA; Less than
Plant Nursery At discretion of local government  Restaurant  1 per 100 seats; Less than 100 seats – At discretion of local government  Restricted Premises At discretion of local government  Restricted Premises At discretion of local government  Service Station At discretion of local government  Shop At discretion of local government  Shop At discretion of local government  Shop At discretion of local government  Showroom  1 per 1000m² sales floor  At discretion of local government  Takeaway / Fast Food Outlet  At discretion of local government  At discretion of local government  Takeaway / Fast Food Outlet  At discretion of local government  Takeaway / Fast Food Outlet			
Government   Government   Government   Restaurant   1 per 100 seats;   Less than 100 seats – At   Giscretion of local government   Go			
Restaurant  1 per 100 seats; Less than 100 seats – At seats At discretion of local government  Restricted Premises  At discretion of local government  Service Station  At discretion of local government  Shop  At discretion of local government  Showroom  1 per 1000m² sales floor  At discretion of local government  Showroom  1 per 1000m² sales floor  At discretion of local government  Takeaway / Fast Food Outlet  1 per 50 seats; less than <50 seats At discretion of local government  At Discretion Of local government  1 per 25 GFA of retail component  1 per 25m² GFA  1 per 1000m² sales floor  At discretion of local government  Takeaway / Fast Food Outlet	Plant Nursery	At discretion of local	At discretion of local
Less than 100 seats – At discretion of local government  Restricted Premises  At discretion of local government  Service Station  At discretion of local government  Service Station  At discretion of local government  Shop  At discretion of local government  Shop  At discretion of local government  Shop  At discretion of local government  Showroom  1 per 1000m² sales floor  Studio  At discretion of local government  Takeaway / Fast Food Outlet  Less than 100 seats – At seats At discretion Of local government  At Discretion Of local government  1 per 25 GFA of retail component  1 per 25m² GFA  1 per 1000m² sales floor  At discretion of local government  Takeaway / Fast Food Outlet		government	government
discretion of local government   government	Restaurant	1 per 100 seats;	1 per 50 seats; less than <50
Restricted Premises At discretion of local government  Service Station At discretion of local government  Shop At discretion of local government  Shop At discretion of local component  Showroom At discretion of local government  Showroom 1 per 1000m² sales floor  Studio At discretion of local government  Takeaway / Fast Food Outlet  At discretion of local government  At discretion of local government  1 per 50m² GFA  1 per 50m² GFA		Less than 100 seats – At	seats At discretion of local
government   government		discretion of local government	government
Service Station  At discretion of local government  Shop  At discretion of local component  At discretion of local government  Showroom  1 per 1000m² sales floor  Studio  At discretion of local government  Takeaway / Fast Food Outlet  1 per 25 GFA of retail component  1 per 25m² GFA  1 per 1000m² sales floor  At discretion of local government  1 per 1000m² sales floor  At discretion of local government  1 per 50m² GFA	Restricted Premises	At discretion of local	At Discretion Of local
government   component     Shop		government	government
Shop At discretion of local government  Showroom 1 per 1000m² sales floor Studio At discretion of local government  Takeaway / Fast Food Outlet  At discretion of local government 1 per 1000m² sales floor At discretion of local government 1 per 50m² GFA 1 per 50m² GFA	Service Station	At discretion of local	1 per 25 GFA of retail
Showroom 1 per 1000m² sales floor 1 per 1000m² sales floor  Studio At discretion of local government government  Takeaway / Fast Food Outlet 1 per 100m² GFA 1 per 50m² GFA		government	component
Showroom     1 per 1000m² sales floor     1 per 1000m² sales floor       Studio     At discretion of local government     At discretion of local government       Takeaway / Fast Food Outlet     1 per 100m² GFA     1 per 50m² GFA	Shop	At discretion of local	1 per 25m <sup>2</sup> GFA
Studio At discretion of local government government  Takeaway / Fast Food Outlet  At discretion of local government government  1 per 100m² GFA  1 per 50m² GFA		government	
government government Takeaway / Fast Food 1 per 100m² GFA 1 per 50m² GFA Outlet	Showroom	1 per 1000m <sup>2</sup> sales floor	1 per 1000m <sup>2</sup> sales floor
Takeaway / Fast Food	Studio	At discretion of local	At discretion of local
Outlet			government
	,	1 per 100m <sup>2</sup> GFA	1 per 50m² GFA
Tavern 1 per 25m <sup>2</sup> bar area 1 per 100m <sup>2</sup> lounge, dining and	Tavern	1 per 25m <sup>2</sup> bar area	1 per 100m <sup>2</sup> lounge, dining and
function area			function area
Vet Consulting rooms	Vet Consulting rooms	At discretion of local	N/A
government		government	
Vet Hospital At discretion of local N/A	Vet Hospital	At discretion of local	N/A
government		government	
Video Store At discretion of local 1 per 25m <sup>2</sup> GFA	Video Store	At discretion of local	1 per 25m <sup>2</sup> GFA
government		government	
Warehouse 1 per 10 employees; At discretion of local	Warehouse		At discretion of local
Less than 10 employees – At government		Less than 10 employees - At	government
discretion of local government			
Any other use At discretion of local At discretion of local	Any other use	At discretion of local	At discretion of local
government government		government	

## 4.18 Parking of Commercial Vehicles on Residential Land

- 4.18.1 No commercial vehicle shall be parked, cleaned or serviced on any lot within the Residential zone or the Residential and Stables zone, or which is otherwise used exclusively for residential purposes unless:
  - (a) the commercial vehicle is registered in respect of the lot under clause 4.18.3;
  - (b) the lot on which the vehicle is parked and/or cleaned or serviced, contains only a single house with or without associated outbuildings;
  - (c) The vehicle is parked and/or cleaned or serviced entirely on the subject lot and is to be located directly behind the front of the main building on the primary and/or secondary street frontage;
  - (d) the vehicle does not exceed 3 metres in height (including the load);
  - (e) in the case of servicing and cleaning, only minor servicing, including minor mechanical repairs and adjustments, and/or cleaning that generates easily contained liquid waste is carried out on the lot. Liquid waste shall be as defined in the *Environmental Protection (Unauthorised Discharges) Regulations 2004* and shall be disposed of in accordance with those Regulations;
  - (f) while on the lot, there is no transfer of goods or passengers from one vehicle to another, unloading or loading of the vehicle, or storage of goods associated with the use of the vehicle.
- 4.18.2 A commercial vehicle shall be taken to be parked on the land for the purpose of the preceding subclause if:
  - (a) it is present on the land for any period of time without the driver being present and visibly in control of the vehicle; and
  - (b) within seven days of the owner being notified, the City of Belmont is not provided with information in writing sufficient to satisfy it that the vehicle was undergoing servicing or cleaning or delivering goods at the relevant time.

# 4.18.3 Register of Commercial Vehicles

For a commercial vehicle to be parked on residential land without a breach of clause 4.18.1, it shall be registered in accordance with the following provisions. Without affecting the generality of clause 4.18.1 a commercial vehicle may be parked on a lot within the Residential zone or the Residential and Stables zone or on a lot which is otherwise used exclusively for residential purposes if it is registered in accordance with the following provisions:

- (a) Any such commercial vehicle to be parked on residential land must be registered in a register of commercial vehicles parking on residential land maintained at the Administrative Offices of the local government, and the register shall be made available for public inspection during office hours.
- (b) An application to register a vehicle for the purpose of this clause shall be submitted to the local government in writing, and the application shall provide sufficient details to identify the land with certainty; the place where the commercial vehicle is proposed to be parked on the land; the tare weight; the make/model; and the unloaded height of the commercial vehicle.

- (c) Where the local government is satisfied that the commercial vehicle in respect of which application is made will be the only commercial vehicle parking at any time on the subject lot, and is ordinarily driven by a person resident in the house on the lot, and that clause 4.18.1 is otherwise complied with, the local government shall record details of the commercial vehicle on the register.
- 4.18.4 Where a commercial vehicle is registered in respect of residential land in accordance with this clause, no other development approval is required under this Scheme in respect of the parking of that commercial vehicle on that land.
- 4.18.5 The registration of a commercial vehicle under this clause in respect of any residential land is not transferable and shall be valid only while that commercial vehicle continues to be driven and parked on the land by the person nominated in the application as the driver resident in the house on the land.

## 4.19 Development on Great Eastern Highway

- 4.19.1 In addition to other matters to which the local government is required to have regard, in considering applications for multi-storey buildings along Great Eastern Highway, the local government shall also have regard to the following:
  - (a) The purpose of the proposed building;
  - (b) The bulk and height of adjoining and nearby buildings;
  - (c) Potential impact of overlooking and/or overshadowing;
  - (d) Potential impact of the proposal on the existing and proposed streetscape; and
  - (e) The effect of the proposed building on the amenity of adjoining and nearby properties.
- 4.19.2 In addition to other matters to which the local government is required to have regard, in considering applications any development along Great Eastern Highway, the local government shall also have regard to the requirement for a limited number of crossovers to the Highway and shall require any applicant to gain the approval of a vehicular access plan by the responsible authority.

## 4.20 Structures Height Control Contours Map

The height of structures within the Scheme Area shall be subject to the requirements of the Westralia Airports Corporation Structures Height Control Contours Map (Schedule No. 8). Structures erected in the Scheme Area shall not exceed in height the contour level indicated on the map. Where indicated on the map all applications for development shall be referred to the Westralia Airports Corporation for comment.

## 4.21 Prohibited Signage

Within the district the following signs and advertising are prohibited and shall not be approved by the local government:

(a) roof signs; or

(b) any sign, hoarding or advertising device whatsoever which does not comply with any other local law of the local government.

# PART 5 - SPECIAL CONTROL AREAS

# 5.1 Operation of Special Control Areas

- 5.1.1 The following special control areas are shown on the Scheme Map:
  - (a) Development Areas, shown on the Scheme Map as SCA DA followed by a number and included in Schedule 9.
  - (b) Development Contribution Areas, shown on the Scheme Map as SCA DCA followed by a number and included in Schedule 11.
- 5.1.2 In respect of a special control area shown on a Scheme Map, the provisions applying to the special control area apply in addition to the provisions applying to any underlying zone or reserve and any general provisions of the Scheme.

## **5.2 Development Contribution Areas**

## 5.2.1 Interpretation

In clause 5.2, unless the context otherwise requires—

'Administrative costs' means such costs as are reasonably incurred for the preparation and (with respect to standard infrastructure items) implementation of the development contribution plan.

'Administrative items' means the administrative matters required to be carried out by or on behalf of the local government in order to prepare and (with respect to standard infrastructure items) implement the development contribution plan, including legal, accounting, planning engineering, and other professional advice.

'Cost apportionment schedule' means a schedule prepared and distributed in accordance with clause 5.2.10.

'Cost contribution' means the contribution to the cost of infrastructure and administrative costs.

*'Development contribution area'* means shown on the scheme map as DCA - SCA with a number and included in Schedule 11.

'Development contribution plan' means a development contribution plan prepared in accordance with the provisions of State Planning Policy 3.6 Development Contributions for Infrastructure and the provisions of this clause 5 of the scheme (as incorporated in schedule 11 to this scheme).

*'Development contribution plan report'* means a report prepared and distributed in accordance with clause 5.2.10.

'Infrastructure' means the standard infrastructure items (services and facilities set out in appendix 1 of the Western Australian Planning Commission State Planning Policy 3.6) and community infrastructure, including recreational facilities; community centres; child care and after school centres; libraries and cultural facilities and such other services and facilities for which development contributions may reasonably be requested having regard to the objectives, scope and provisions of Western Australian Planning Commission State Planning Policy 3.6. AMD 1 GG 15/12/2015

'Infrastructure costs' means such costs as are reasonably incurred for the acquisition and construction of infrastructure.

'Local government' means the local government or local governments in which the development contribution area is located or through which the services and facilities are provided.

'Owner' means an owner of land that is located within a development contribution area.

## 5.2.2 Purpose

The purpose of having development contribution areas is to:

- (a) provide for the equitable sharing of the costs of infrastructure and administrative costs between owners;
- (b) ensure that cost contributions are reasonably required as a result of the subdivision and development of land in the development contribution area; and
- (c) coordinate the timely provision of Infrastructure.

## 5.2.3 Development contribution plan required

A development contribution plan is required to be prepared for each development contribution area.

5.2.4 Development contribution plan part of scheme

The development contribution plan is incorporated in schedule 11 as part of this scheme.

5.2.5 Subdivision, strata subdivision and development

The local government shall not withhold its support for subdivision, strata subdivision or refuse to approve a development solely for the reason that a development contribution plan is not in effect, there is no approval to advertise a development contribution plan, or that there is no other arrangement with respect to an owner's contribution towards the provision of community infrastructure.

5.2.6 Guiding principles for development contribution plans

The development contribution plan for any development contribution area is to be prepared in accordance with the following principles:

- (a) Need and the nexus
  - The need for the infrastructure included in the plan must be clearly demonstrated (need) and the connection between the development and the demand created should be clearly established (nexus).
- (b) Transparency

Both the method for calculating the development contribution and the manner in which it is applied should be clear, transparent and simple to understand and administer.

(c) Equity

Development contributions should be levied from all developments within a

development contribution area, based on their relative contribution to need.

# (d) Certainty

All development contributions should be clearly identified and methods of accounting for cost adjustments determined at the commencement of a development.

#### (e) Efficiency

Development contributions should be justified on a whole of life capital cost basis consistent with maintaining financial discipline on service providers by precluding over recovery of costs.

# (f) Consistency

Development contributions should be applied uniformly across a development contribution area and the methodology for applying contributions should be consistent.

(g) Right of consultation and review

Owners have the right to be consulted on the manner in which development contributions are determined. They also have the opportunity to seek a review by an independent third party if they believe the calculation of the costs of the contributions is not reasonable.

#### (h) Accountable

There must be accountability in the manner in which development contributions are determined and expended.

## 5.2.7 Recommended content of development contribution plans

#### 5.2.7.1 The development contribution plan is to specify:

the development contribution area to which the development contribution plan applies;

- (a) the infrastructure and administrative items to be funded through the development contribution plan;
- (b) the method of determining the cost contribution of each owner; and
- (c) the priority and timing for the provision of infrastructure.

#### 5.2.8 Period of development contribution plan

A development contribution plan shall specify the period during which it is to operate.

## 5.2.9 Land excluded

In calculating both the area of an owner's land and the total area of land in a development contribution area, the area of land provided in that development contribution area for:

- (a) roads designated under the Metropolitan Region Scheme as primary regional roads and other regional roads;
- (b) existing public open space;
- (c) existing government primary and secondary schools; and
- (d) such other land as is set out in the development contribution plan, is to be excluded.

- 5.2.10 Development contribution plan report and cost apportionment schedule
- 5.2.10.1 Within 90 days of the development contribution plan coming into effect, the local government is to adopt and make available a development contribution plan report and cost apportionment schedule to all owners in the development contribution area.
- 5.2.10.2 The development contribution plan report and the cost apportionment schedule shall set out in detail the calculation of the cost contribution for each owner in the development contribution area, based on the methodology provided in the development contribution plan, and shall take into account any proposed staging of the development.
- 5.2.10.3 The development contribution plan report and the cost apportionment schedule do not form part of the scheme, but once adopted by the local government they are subject to review as provided under clause 5.2.11.
- 5.2.11 Cost contributions based on estimates
- 5.2.11.1 The determination of Infrastructure costs and administrative costs is to be based on amounts expended, but when expenditure has not occurred, it is to be based on the best and latest estimated costs available to the local government and adjusted accordingly, if necessary.
- 5.2.11.2 Where a cost apportionment schedule contains estimated costs, such estimated costs are to be reviewed at least annually by the local government—
  - (a) in the case of land to be acquired, in accordance with clause 5.2.12; and
  - (b) in all other cases, in accordance with the best and latest information available to the local government, until the expenditure on the relevant item of infrastructure or administrative costs has occurred.
- 5.2.11.3 The local government is to have such estimated costs independently certified by appropriate qualified persons and must provide such independent certification to an owner when requested to do so.
- 5.2.11.4 Where any cost contribution has been calculated on the basis of an estimated cost, the local government:
  - (a) is to adjust the cost contribution of any owner in accordance with the revised estimated costs; and
  - (b) may accept a cost contribution, based upon estimated costs, as a final cost contribution and enter into an agreement with the owner accordingly.
- 5.2.11.5 Where an owner's cost contribution is adjusted under clause 5.2.11.4, the local government, on receiving a request in writing from an owner, is to provide the owner with a copy of estimated costs and the calculation of adjustments.
- 5.2.11.6 If an owner objects to the amount of a cost contribution, the owner may give notice to the local government requesting a review of the amount of the cost contribution by an appropriate qualified person ('independent expert') agreed by the local government and the owner at the owner's expense, within 28 days after being informed of the cost contribution.

- 5.2.11.7 If the independent expert does not change the cost contribution to a figure acceptable to the owner, the cost contribution is to be determined—
  - (a) by any method agreed between the local government and the owner; or
  - (b) if the local government and the owner cannot agree on a method pursuant to (a) or on an independent expert, by arbitration in accordance with the *Commercial Arbitration Act 1985*, with the costs to be shared equally between the local government and owner.
- 5.2.12 Valuation
- 5.2.12.1 Clause 5.2.12 applies in order to determine the value of land to be acquired for the purpose of providing Infrastructure.

## 5.2.12.2 In clause 5.2.12:

'Value' means the fair market value of land, at a specified date, which is defined as the capital sum that would be negotiated in an arms length transaction in an open and unrestricted market, assuming the highest and best use of the land with all its potential and limitations (other than the limitation arising from the transaction for which the land is being valued), wherein the parties act knowledgeably, prudently and without compulsion to buy or sell.

The net land value is to be determined by a static feasibility valuation model, using the working sheet model attached to this scheme as schedule 10. As part of that feasibility an appropriate profit and risk factor is to be determined from which a 10 per cent profit factor is to be excluded from the calculation.

'Valuer' means a licensed valuer agreed by the local government and the owner, or, where the local government and the owner are unable to reach agreement, by a valuer appointed by the President of the Western Australian Division of the Australian Property Institute.

- 5.2.12.3 If an owner objects to a valuation made by the valuer, the owner may give notice to the local government requesting a review of the amount of the value, at the owner's expense, within 28 days after being informed of the value.
- 5.2.12.4 If, following a review, the valuer's determination of the value of the land is still not a figure acceptable to the owner, the value is to be determined:
  - (a) by any method agreed between the local government and the owner; or
  - (b) if the local government and the owner cannot agree, the owner may apply to the State Administrative Tribunal for a review of the matter under part 14 of the *Planning and Development Act 2005*.
- 5.2.13 Liability for cost contributions
- 5.2.13.1 An owner must make a cost contribution in accordance with the applicable development contribution plan and the provisions of clause 5.2.
- 5.2.13.2 An owner's liability to pay the owner's cost contribution to the local government arises on the earlier of:

- (a) the Western Australian Planning Commission endorsing its approval on the deposited plan or survey strata plan of the subdivision of the owner's land within the development contribution area;
- (b) the commencement of any development on the owner's land within the development contribution area;
- (c) the approval of any strata plan by the local government or Western Australian Planning Commission on the owner's land within the development contribution area; or
- (d) the approval of a change or extension of use by the local government on the owner's land within the development contribution area.

The liability arises only once upon the earliest of the above listed events.

- 5.2.13.3 Notwithstanding clause 5.2.13.2, an owner's liability to pay the owner's cost contribution does not arise if the owner commences development of the first single house or outbuildings associated with that first single house on an existing lot which has not been subdivided or strata subdivided since the coming into effect of the development contribution plan.
- 5.2.13.4 Where a development contribution plan expires in accordance with clause 5.2.8, an owner's liability to pay the owner's cost contribution under that development contribution plan shall be deemed to continue in effect and be carried over into any subsequent development contribution plan which includes the owner's land, subject to such liability.
- 5.2.14 Payment of cost contribution
- 5.2.14.1 The owner, with the agreement of the local government, is to pay the owner's cost contribution by:
  - (a) cheque or cash;
  - (b) transferring to the local government or a public authority land in satisfaction of the cost contribution;
  - (c) the provision of physical infrastructure;
  - (d) some other method acceptable to the local government; or
  - (e) any combination of these methods.
- 5.2.14.2 The owner, with the agreement of the local government, may pay the owner's cost contribution in a lump sum, by instalments or in such other manner acceptable to the local government.
- 5.2.14.3 Payment by an owner of the cost contribution, including a cost contribution based upon estimated costs in a manner acceptable to the local government, constitutes full and final discharge of the owner's liability under the development contribution plan and the local government shall provide certification in writing to the owner of such discharge if requested by the owner.
- 5.2.15 Charge on land
- 5.2.15.1 The amount of any cost contribution for which an owner is liable under clause 5.2.13, but has not paid, is a charge on the owner's land to which the cost contribution relates, and

- the local government may lodge a caveat, at the owner's expense, against the owner's certificate of title to that land.
- 5.2.15.2 The local government, at the owner's expense and subject to such other conditions as the local government thinks fit, can withdraw a caveat lodged under clause 5.2.15.1 to permit a dealing and may then re-lodge the caveat to prevent further dealings.
- 5.2.15.3 If the cost contribution is paid in full, the local government, if requested to do so by the owner and at the expense of the owner, is to withdraw any caveat lodged under clause 5.2.15.
- 5.2.16 Administration of funds
- 5.2.16.1 The local government is to establish and maintain a reserve account in accordance with the Local Government Act 1995 for each development contribution area into which cost contributions for that development contribution area will be credited and from which all payments for the infrastructure costs and administrative costs within that development contribution area will be paid.
  - The purpose of such a reserve account or the use of money in such a reserve account is limited to the application of funds for that development contribution area.
- 5.2.16.2 Interest earned on cost contributions credited to a reserve account in accordance with clause 5.2.16.1 is to be applied in the development contribution area to which the reserve account relates.
- 5.2.16.3 The local government is to publish an audited annual statement of accounts for that development contribution area as soon as practicable after the audited annual statement of accounts becomes available.
- 5.2.17 Shortfall or excess in cost contributions
- 5.2.17.1 If there is a shortfall in the total of cost contributions when all cost contributions have been made or accounted for in a particular development contribution area, the local government may:
  - (a) make good the shortfall;
  - (b) enter into agreements with owners to fund the shortfall; or
  - (c) raise loans or borrow from a financial institution;
  - but nothing in paragraph 5.2.17.1(a) restricts the right or power of the local government to impose a differential rate to a specified development contribution area in that regard.
- 5.2.17.2 If there is an excess in funds available to the development contribution area when all cost contributions have been made or accounted for in a particular development contribution area, the local government is to refund the excess funds to contributing owners for that development contribution area. To the extent, if any, that it is not reasonably practicable to identify owners and/or their entitled amount of refund, any excess in funds shall be applied, to the provision of additional facilities or improvements in that development contribution area.
- 5.2.18 Powers of the local government

The local government in implementing the development contribution plan has the power to:

- (a) acquire any land or buildings within the scheme area under the provisions of the *Planning and Development Act 2005*; and
- (b) deal with or dispose of any land which it has acquired under the provisions of the *Planning and Development Act 2005* in accordance with the law and for such purpose may make such agreements with other owners as it considers fit.

#### 5.2.19 Arbitration

Subject to clauses 5.2.12.3 and 5.2.12.4, any dispute between an owner and the local government in connection with the cost contribution required to be made by an owner is to be resolved by arbitration in accordance with the *Commercial Arbitration Act 1985*.

# **SCHEDULES**

# **Schedule No. A - Supplemental Provisions to the Deemed Provisions**

#### Part 2 – Local Planning Framework

(2a) Except to the extent that the Local Planning Strategy is inconsistent with the Scheme, determinations of the local government under the Scheme are to be consistent with the Local Planning Strategy.

#### Part 3 – Heritage Protection

- 13A Tree Preservation Orders and Planting of Trees
  - (1) (a) The local government may resolve that an Order ("Tree Preservation Order") be served upon the owner of any land requiring the preservation of a tree or group of trees growing wholly or partly on that land.
    - (b) If portion of the canopy of a tree, the subject of a Tree Preservation Order, overhangs property adjoining that on which the tree is growing, the local government may also resolve that a Tree Preservation Order be served upon the owner of that adjoining property and the order with any necessary modification shall have the same effect in respect of that owner as the order referred to in paragraph (a).
    - (c) If the owner of land on which any tree affected by a Tree Preservation Order is situated or the owner of land which any such tree overhangs is not also the occupier of that land the local government may additionally or alternatively serve a copy of the order on the occupier of such land, and the order shall have the same effect in respect of the occupier as the orders referred to in paragraph (a) and (b) in relation to owners.
    - (d) No person served with an order or a copy of an order in accordance with this subclause, and no person who has knowledge of such an order may remove or otherwise destroy, or cut, break or otherwise damage any part of a tree affected by the order, or cause or permit such removal, destruction, cutting, breaking or damaging except with the prior approval in writing of the local government.
  - (2) An owner of land affected by a Tree Preservation Order may, within 28 days of the serving of the order on that owner, request in writing that local government reconsider the Tree Preservation Order and revoke or modify the Order in a manner specified in the request. If the local government within 60 days of receipt of such a request fails or refuses to revoke or modify the order as requested, the owner may apply for review of the decision to the State Administrative Tribunal.
  - (3) (a) The local government may at any time reconsider a Tree Preservation Order and either as a result of a request by an owner or of its own motion, may resolve to revoke or modify the order.
    - (b) A revocation or modification under the preceding paragraph may be in relation to any one or more of all the lands affected by the original Tree Preservation Order.

- (b) The fact of a Tree Preservation order being revoked or modified shall be notified to any owner and occupier of land affected by the original Tree Preservation Order.
- (4) The local government may keep and maintain a register of trees which includes the description and location of any tree or group of trees the subject of a Tree Preservation Order, and the name and address of the landowner or occupier on whom any order has been served in accordance with this clause. The register shall be kept available for public inspection at the administrative office of the local government.
- (5) When the local government resolves to serve a Tree Preservation Order on the owner or occupier of any land, the local government may without any further resolution deliver a copy of the Tree Preservation Order to the Registrar of Titles.
- (6) Upon the revocation or modification of a Tree Preservation Order which has the effect that any land is no longer affected by the Order, the local government shall deliver a notification of the fact that the land is no longer affected in the same way and to the same officers as a copy of an order is delivered under the preceding subclause. Such notification shall specify the date on which the revocation or modification took place and the land to which it relates.
- (7) (a) On receiving a copy of a Tree Preservation Order delivered under the provisions of this clause, the Registrar of Titles shall register the order and endorse or note accordingly the appropriate register book in respect of the land stipulated in the order as the land to which it relates.
  - (b) On receiving a notification of revocation or modification of a Tree Preservation Order delivered under the provisions of this clause, the Registrar of Titles shall cancel or modify (as the notification requires) the registration of the Tree Preservation Order in relation to the land specified and endorse or note accordingly the appropriate register books in respect of the land so specified.
  - (c) The local government may require assessment or certification by an arboriculturist to be carried out prior to issuing an approval to cut, break or remove or damage a tree subject to a Preservation Order, in accordance to Clause 13A(1)(d).
- (8) Without affecting the generality of any other provision in this Scheme giving power to make agreements, the local government may enter into agreements with any applicant or owner or occupier of land for the purpose of supporting or reinforcing a Tree Preservation Order.

#### Part 7 – Requirements for Development Approval

- 61 Development for Which Development Approval is not Required
  - (1) (c) (vi) partially or wholly within the 25-30 ANEF (Aircraft Noise Exposure Forecast) contours; or where the extension to an existing single house, partially or wholly within the 25-30 ANEF contours, involves more than two additional habitable rooms or more than 25% increase of habitable floorspace;

- (k) the demolition of any building or structure not mentioned in paragraph (e) unless the demolition is
  - (i) located in a place that is entered in the Register of Heritage Place under the *Heritage of Western Australia Act 1990*; or
  - (ii) the subject of an order under the *Heritage of Western Australia Act* 1990 Part 6; or
  - (iii) included on a heritage list prepared in accordance with this Scheme; or
  - (iv) located within an area designated under this Scheme as a heritage area; or
  - (v) the subject of a heritage agreement entered into under the *Heritage* of Western Australia Act 1990 section 29;
- (I) the signage and advertisements contained in Schedule 5 of this Scheme;
- (m) the erection of a boundary fence in a zone where no residential density code applies to land on either side of the boundary, except where the boundary fence is
  - (i) located in the street setback area of the lot; or
  - (ii) within the Residential and Stables zone, located within the street setback; and exceeds 1.8 metres in height or is not visually permeable above 1.2 metres above natural ground level.
- (n) the erection of a patio or pergola on the same lot as a single house, a grouped or a multiple dwelling, whether or not the patio or pergola satisfies the deemed-to-comply requirements of the Residential Design Codes; except where the patio or pergola is located within the primary street setback area of the lot.
- (o) the erection of a carport associated with a single house, grouped or multiple dwelling, provided that:
  - (i) the carport is located no closer than 2.0 metres from the primary street boundary; or
  - (ii) the pitch and colour of the roof of the carport matches that of the dwelling to which it relates.
- (p) the carrying out of works urgently necessary for public safety, the safety or security of plant or equipment, or the maintenance of essential services.
- (q) small scale receiving and transmission equipment that meets the following criteria-
  - (i) satellite dishes that are not visible from the public realm, are located behind the front setback, do not exceed a maximum diameter of 1.0 metre; and that do not project above the ridge line of a building located in the same property.

- (ii) microwaves antennas and aerials that do not exceed a maximum width or diameter of 500mm; and do not project more than 3.0 metres above the ridge line of a building located in the same property.
- (r) the installation of security shutters and grills on non-residential buildings provided that
  - (i) the shutters or grills are visually permeable to a minimum of 50% when viewed from the public realm;
  - (ii) the shutters or grills are compatible with colours and materials of existing development on the lot;
  - (iii) the shutter box is fixed internally;
  - (s) the erection of a cubby house on the same lot as a single house or grouped dwelling provided that
    - (i) the maximum wall height is not more than 1.6 metres above natural ground level;
    - (ii) the maximum floor area is not greater than 9 square metres; and
    - (iii) the floor is not more than 600mm above natural ground level.

# Schedule No. 1 - Dictionary of Defined Words and Expressions

Schedule No. 1	
General Definitions	Meaning
authorised officer:	means an officer of the local government, authorised by the local government to exercise all or some of the powers of the local government under this Scheme;
building envelope:	means the area of land within which all buildings and effluent disposal facilities on a lot must be contained; AMD 7 GG 1/11/2016
building height:	In relation to a building – If the building is used for residential purposes, has the meaning given in the R-Codes; or If the building is used for purposes other than residential purposes, means the maximum vertical distance between the natural ground level and the finished roof height directly above, excluding minor projections as that term is defined in the R-Codes; AMD 7 GG 1/11/2016
building line:	means the line between which and any public place or public reserve a building may not be erected except by or under the authority of a written law and the term "building set-back line" has a like meaning;
cabin:	means a dwelling forming part of a tourist development or caravan park that is —  (a) an individual unit other than a chalet; and  (b) designed to provide short-term accommodation for guests;  AMD 7 GG 1/11/2016
chalet:	<ul> <li>means a dwelling forming part of a tourist development or caravan park that is —</li> <li>(a) a self-contained unit that includes cooking facilities, bathroom facilities and separate living and sleeping areas; and</li> <li>(b) designed to provide short-term accommodation for guests;</li> <li>AMD 7 GG 1/11/2016</li> </ul>
commencement day:	means the day this Scheme comes into effect under section 87(4) of the Act; AMD 7 GG 1/11/2016
commercial vehicle:	means a vehicle, whether licenced or not, that has a gross vehicle mass of greater than 4.5 tonnes including —  (a) a utility, van, truck, tractor, bus or earthmoving equipment; and (b) a vehicle that is, or is designed to be an attachment to a vehicle referred to in paragraph (a); AMD 7 GG 1/11/2016
conservation:	has the same meaning as in the Heritage of Western Australia Act 1990;
local government: curtilage:	means the executive body of the City of Belmont; in relation to a dwelling means the yard of the dwelling, or an area in the immediate vicinity of the dwelling on the same lot used for purposes ancillary to the dwelling. The curtilage shall not include the area located between the street frontage of the lot and the dwelling thereon except with the special approval of the local government. The term has a like meaning in relation to land around buildings other than dwellings.
facade:	means an exposed face of a building orientated towards a road or other public land or the outward appearance of the building where it does not face a road or other public land;
floor area:	has meaning given in the Building Code; AMD 7 GG 1/11/2016
(gross floor area)	Sandada o a la della
frontage:	<ul> <li>in relation to a building —</li> <li>(a) if the building is used for residential purposes, has the meaning given in the R-Codes; or</li> <li>(b) if the building is used for purposes other than residential purposes, means the line where a road reserve and the front of a lot meet and, if a lot abuts 2 or more road reserves, the one to which the building or proposed building faces; AMD 7 GG 1/11/2016</li> </ul>
gazettal date:	means the date on which notice of the Minister's approval of this Scheme is published in the Government Gazette;
ground level:	the level of the ground on the site prior to any building work other than
(natural ground level)	subdivisional site works, and average natural ground level is the average

Schedule No. 1	
General Definitions	Meaning
	level ascertained by averaging the natural ground levels at all points of intersection of lines drawn from opposite corners of the site. For a site with odd numbers of corners, the corner furthest from the major street boundary is ignored. For the height of a wall, the measurement is taken from the natural ground level at the centre point of the wall to the highest point of the wall whether that be above the centre point or not.
health consultant:	a legally qualified medical doctor, dentist, physiotherapist, podiatrist, acupuncturist, naturopath, or other persons ordinarily associated with medical practises, in the prevention or treatment of physical or mental injuries or ailments.
height:	height when used in relation to a building that is used for:  (a) residential purposes, has the same meaning as in the Residential Design Codes; or  (b) purposes other than residential purposes, means the maximum vertical distance between the ground level and the finished roof height directly above;
hoarding:	means a detached or detachable structure other than a pylon sign that is erected for the sole purpose of displaying advertising, but does not include a hoarding within the meaning of section 377 of the Local Government (Miscellaneous Provisions) Act 1960 unless such a hoarding is used for the purpose of displaying advertising;
incidental use Minerals:	means a use of premises which is consequent on, or naturally attaching, appertaining or relating to, the predominant use; <i>AMD 7 GG 1/11/2016</i> has the meaning given in the <i>Mining Act 1978</i> section 8(1); <i>AMD 7 GG</i>
	1/11/2016  means the area of all floors within the internal finished surfaces of
net lettable area (nla):	permanent walls but does not include the following areas —  (a) stairs, toilets, cleaner's cupboards, lift shafts and motor rooms, escalators, tea rooms and plant rooms, and other service areas;  (b) lobbies between lifts facing other lifts serving the same floor;  (c) areas set aside as public space or thoroughfares and not for the exclusive use of occupiers of the floor or building;  (d) areas set aside for the provision of facilities or services to the floor or building where those facilities are not for the exclusive use of occupiers of the floor or building; AMD 7 GG 1/11/2016
non-conforming use:	has the meaning given in the <i>Planning and Development Act 2005</i> section 172; <i>AMD 7 GG 1/11/2016</i>
park:	in relation to vehicles, means to leave stationary on land, and inflexions of the term have corresponding meanings;
plot ratio:	means the ratio of the floor area of a building to an area of land within the boundaries of the lot or lots on which the building is located;  AMD 7 GG 1/11/2016
precinct:	means a definable area where particular planning policies, guidelines or standards apply; AMD 7 GG 1/11/2016
predominant use:	means the primary use of premises to which all other uses carried out on the premises are incidental; AMD 7 GG 1/11/2016
public utility:	means any work or undertaking constructed or maintained by a public authority or the City as may be required to provide water, sewerage, electricity, gas, drainage, communications or other similar services;
pylon sign:	means a sign supported by one or more piers and not attached to a building and includes a detached sign framework supported by one or more piers to which sign infills may be added;
retail:	retail means the sale or hire of goods or services to the public;
roof sign:	means a sign erected or mounted on the roof of a building and means the same as "roof mounted sign";
Schedule:	means a Schedule to the Scheme;
short-term accommodation:	means temporary accommodation provided either continuously or from time to time with no guest accommodated for periods totalling more than 3 months in any 12 month period; AMD 7 GG 1/11/2016

Schedule No. 1	
General Definitions	Meaning
site coverage	means the area of a lot occupied by any building at ground level, together with the area of any overhang, veranda or balcony more than 0.5m above natural ground level in excess of 10% of the site area or 50m², whichever is the lesser. "Site cover" and "lot coverage" have the same meaning;
special facility:	means land and/or any building approved or licensed or intended to be approved or licensed as a special facility under the liquor control legislation;
substantial work:	building work progressed beyond site works and footings over the whole site, or in the case of a staged development, building work has progressed beyond site works and footings for a complete stage or stages representing at least 50% of the ground floor area of the total development;
wall height:	in relation to a wall of a building —  (a) if the building is used for residential purposes, has the meaning given in the R-Codes; or  (b) if the building is used for purposes other than residential purposes, means the vertical distance from the natural ground level of the boundary of the property that is closest to the wall to the point where the wall meets the roof or parapet; AMD 7 GG 1/11/2016
wholesale:	means the sale of any goods to any person or persons other than the ultimate consumer of those goods;

Schedule No. 1		
Land Use Definitions	Meaning	
amusement facility:	means premises —	
	(a) that are open to the public; and	
	(b) that are used predominantly for amusement by means of	
	amusement machines including computers; and	
	(c) where there are 2 or more amusement machines;	
amusement parlour:	means any land or building, open to the public, where the predominant	
	use is amusement or recreation by amusement machines and where	
	there are more than two amusement machines operating within the	
	premises;	
	AMD 7 GG 1/11/2016	
ancillary dwelling:	has the same meaning given to it in the Residential Design Codes;	
animal establishment:	means premises used for the breeding, boarding, training or caring of	
	animals for commercial purposes but does not include animal husbandry	
	— intensive or veterinary centre; AMD 7 GG 1/11/2016	
art gallery:	means premises —	
	(a) that are open to the public; and	
	(b) where artworks are displayed for viewing or sale; AMD 7 GG	
	1/11/2016	
auction mart:	means any land or building on or in which goods are exposed or offered	
	for sale by auction, but does not include a place used for the sale by	
	auction of fresh food, fruit, vegetables, livestock or motor vehicles;	
bed and breakfast	means a dwelling —	
	(a) used by a resident of the dwelling to provide short-term	
	accommodation, including breakfast, on a commercial basis for not	
	more than 4 adult persons or one family; and	
	(b) containing not more than 2 guest bedrooms; AMD 7 GG 1/11/2016	
hatting against		
betting agency:	means an office or totalisator agency established under the <i>Racing and</i>	
huassam.	Wagering Western Australia Act 2003; AMD 7 GG 1/11/2016	
brewery:	means premises the subject of a producer's licence authorising the	
	production of beer, cider or spirits granted under the <i>Liquor Control Act</i>	
	1988; AMD 7 GG 1/11/2016	

Schedule No. 1	
Land Use Definitions	Meaning
camping area:	means any land used for the accommodation of persons in tents or other
	temporary shelter;
caravan:	means any vehicle whether currently equipped with wheels or not,
	together with attachments, designed or adapted for human habitation and includes a mobile home which fits the foregoing description;
caravan park:	means premises that are a caravan park as defined in the <i>Caravan</i>
Garavan parki	Parks and Camping Grounds Act 1995 section 5(1);
	AMD 7 GG 1/11/2016
caretaker's dwelling:	means a dwelling on the same site as a building, operation, or plant, and
	occupied by a supervisor of that building, operation or plant;
carpark:	means premises used primarily for parking vehicles whether open to the
	public or not but does not include —  (a) any part of a public road used for parking or for a taxi rank; or
	(b) any premises in which cars are displayed for sale;
	AMD 7 GG 1/11/2016
child care premises	means premises where —
_	(a) an education and care service as defined in the Education and
	Care Services National Law (Western Australia) section 5(1), other
	than a family day care service as defined in that section, is
	provided; or (b) a child care service as defined in the <i>Child Care Services Act 2007</i>
	section 4 is provided; AMD 7 GG 1/11/2016
child family day care:	family day care means premises where a family day care service as
	defined in the Education and Care Services National Law (Western
	Australia) is provided; AMD 7 GG 1/11/2016
cinema/theatre:	means premises where the public may view a motion picture or theatrical
civic use:	production; AMD 7 GG 1/11/2016
civic use:	means land or buildings used by a Government Department, an agency or instrumentality of the State, or the local government, for
	administrative, recreational or other purpose;
club premises:	means any land or buildings used or designed for use by a legally
	constituted club or association or other body of persons united by a
	common interest;
commercial vehicle:	means a vehicle whether licensed or not and which is used or designed
	for use for business purposes or otherwise in conjunction with a trade or profession and without limiting the generality of the foregoing includes
	any trailer, tractor and an attachment to any of them and any bus or
	earthmoving machine whether self propelled or not but shall not include
	a vehicle designed for use as a passenger car, a van, utility or light truck
	which is rated by the manufacturer as being suitable to carry loads of up
commercial vahiala parking:	to 1.5 tonnes;
commercial vehicle parking:	means premises used for parking of one or 2 commercial vehicles but does not include —
	(a) any part of a public road used for parking or for a taxi rank; or
	(b) parking of commercial vehicles incidental to the predominant use of
	the land; AMD 7 GG 1/11/2016
community home:	means a building used primarily for living purposes by a group of
	physically or intellectually handicapped or socially disadvantaged
	persons living together with or without paid supervision or care and by a constituted community based organisation, a recognised voluntary,
	charitable or religious organisation, a government department or an
	agency or instrumentality of the State or a local government body;
community purpose:	means the use of any land or building primarily for the provision of
	educational, social or recreational facilities or services by organisations
	involved in activities for community benefit;
consulting rooms:	means premises used by no more than 2 health practitioners at the same time for the investigation or treatment of human injuries or ailments
	and for general outpatient care; <i>AMD 7 GG 1/11/2016</i>
convenience store:	means premises —
	Timestic Browniago

Schedule No. 1	
Land Use Definitions	Meaning
	<ul> <li>(a) used for the retail sale of convenience goods commonly sold in supermarkets, delicatessens or newsagents; and</li> <li>(b) operated during hours which include, but may extend beyond, normal trading hours; and</li> </ul>
	(c) the floor area of which does not exceed 300 m2 net lettable area; AMD 7 GG 1/11/2016
corrective institution	means premises used to hold and reform persons committed to it by a court, such as a prison or other type of detention facility;
cultural use:	means any use aimed at the improvement or refinement of people by entertainment and/or education;
display home centre:	means a group of two or more dwellings which are intended to be open for public inspection for a limited period of time;
dog kennels:	means any land and building used for the boarding or breeding of dogs where such premises are registered or required to be registered by the local government under any written law; and may include the sale of dogs where such use is incidental to the predominant use;
dry cleaning premises:	means any land and building used for the cleaning of garments and other fabrics by chemical processes;
educational establishment:	means premises used for the purposes of education and includes a school, tertiary institution, business college, academy or other educational centre;
exhibition centre:	means premises used for the display, or display and sale, of materials of an artistic, cultural or historical nature including a museum; <i>AMD 7 GG</i> 1/11/2016
fast food/take-away outlet:	fast food outlet/lunch bar means premises, including premises with a facility for drive-through service, used for the preparation, sale and serving of food to customers in a form ready to be eaten —  (a) without further preparation; and (b) primarily off the premises; AMD 7 GG 1/11/2016
fuel depot:	means premises used for the storage and sale in bulk of solid or liquid or gaseous fuel but does not include premises used —  (a) as a service station; or  (b) for the sale of fuel by retail into a vehicle for use by the vehicle; AMD 7 GG 1/11/2016
funeral parlour:	means premises used —  (a) to prepare and store bodies for burial or cremation;  (b) to conduct funeral services; AMD 7 GG 1/11/2016
garden centre:	means premises used for the propagation, rearing and sale of plants, and the storage and sale of products associated with horticulture and gardens; AMD 7 GG 1/11/2016
health centre:	means any building used as a maternity centre, a district clinic, an x-ray or ultrasound centre, or pathology centre, or the like of any of those, but does not include the consulting rooms or a medical centre use classes;
health studio:	means any land and building designed and equipped for physical exercise, recreation or sporting activities, but does not include the private recreation or public recreation use classes;
Holiday accommodation	means 2 or more dwellings on one lot used to provide short term accommodation for persons other than the owner of the lot; AMD 7 GG 1/11/2016
Holiday house	means a single dwelling on one lot used to provide short-term accommodation but does not include a bed and breakfast;  AMD 7 GG 1/11/2016
home business	means a dwelling or land around a dwelling used by an occupier of the dwelling to carry out a business, service or profession if the carrying out of the business, service or profession —  (a) does not involve employing more than 2 people who are not members of the occupier's household; and  (b) will not cause injury to or adversely affect the amenity of the neighbourhood; and  (c) does not occupy an area greater than 50 m2; and

Schedule No. 1		
Land Use Definitions	Meaning	
	<ul> <li>(d) does not involve the retail sale, display or hire of any goods unless the sale, display or hire is done only by means of the Internet; and does not result in traffic difficulties as a result of the inadequacy of parking or an increase in traffic volumes in the neighbourhood; and</li> </ul>	
	(f) does not involve the presence, use or calling of a vehicle of more than 4.5 tonnes tare weight; and	
	(g) does not involve the use of an essential service that is greater than the use normally required in the zone in which	
	the dwelling is located;  AMD 7 GG 1/11/2016	
home occupation:	means a dwelling or land around a dwelling used by an occupier of the dwelling to carry out an occupation if the carrying out of the occupation that —	
	<ul><li>(a) does not involve employing a person who is not a member of the occupier's household; and</li><li>(b) will not cause injury to or adversely affect the amenity of the</li></ul>	
	<ul><li>(b) will not cause injury to or adversely affect the amenity of the neighbourhood; and</li><li>(c) does not occupy an area greater than 20 m2; and</li></ul>	
	(d) does not involve the display on the premises of a sign with an area exceeding 0.2 m2; and	
	(e) does not involve the retail sale, display or hire of any goods unless the sale, display or hire is done only by means of the Internet; and	
	<ul> <li>(f) does not —         <ul> <li>(i) require a greater number of parking spaces than normally required for a single dwelling; or</li> <li>(ii) result in an increase in traffic volume in the neighbourhood;</li> </ul> </li> </ul>	
	and  (g) does not involve the presence, use or calling of a vehicle of more	
	than 4.5 tonnes tare weight; and  (h) does not include provision for the fuelling, repair or maintenance of	
	motor vehicles; and (i) does not involve the use of an essential service that is greater than the use normally required in the zone in which the dwelling is located; <i>AMD 7 GG 1/11/2016</i>	
home office	means a dwelling used by an occupier of the dwelling to carry out a	
	home occupation if the carrying out of the occupation —  (a) is solely within the dwelling; and	
	(b) does not entail clients or customers travelling to and from the dwelling; and	
	<ul> <li>(c) does not involve the display of a sign on the premises; and</li> <li>(d) does not require any change to the external appearance of the dwelling; AMD 7 GG 1/11/2016</li> </ul>	
home store:	means a shop attached to a dwelling that —  (a) has a net lettable area not exceeding 100 m2; and  (b) is operated by a person residing in the dwelling; AMD 7 GG  1/11/2016	
hospital:	means premises used as a hospital as defined in the Hospitals and Health Services Act 1927 section 2(1); AMD 7 GG 1/11/2016	
hotel:	means premises the subject of a hotel licence other than a small bar or tavern licence granted under the <i>Liquor Control Act 1988</i> including any betting agency on the premises; <i>AMD 7 GG 1/11/2016</i>	
industry:	means premises used for the manufacture, dismantling, processing, assembly, treating, testing, servicing, maintenance or repairing of goods, products, articles, materials or substances and includes facilities on the premises for any of the following purposes —  (a) the storage of goods;  (b) the work of administration or accounting;	
	<ul><li>(c) the selling of goods by wholesale or retail;</li><li>(d) the provision of amenities for employees;</li></ul>	

Schedule No. 1	
Land Use Definitions	Meaning
	(e) incidental purposes; AMD 7 GG 1/11/2016
industry – cottage: (or cottage industry)	means a trade or light industry producing arts and crafts goods which does not fall within the definition of home occupation and which:
	(a) does not cause injury to or adversely affect the amenity of the neighbourhood;
	(b) where operated in a residential zone, does not employ any person other than a member of the occupier's household;
	(c) is conducted in a building or an out-building which is compatible with the principal uses to which land in the zone in which it is located may be put;
	<ul><li>(d) does not occupy an area in excess of 50 square metres; and</li><li>(e) does not display a sign exceeding 0.2 square metres in area;</li></ul>
industry - extractive:	means an industry which involves the extraction, quarrying or removal of
(or extractive industry)	sand, gravel, clay, hard rock, stone or similar material from the land and includes the treatment and storage of those materials, or the manufacture of products from those materials on, or adjacent to, the land from which the materials are extracted, but does not include industry mining;
industry - general:	means an industry other than an extractive, hazardous, light, noxious,
(or general industry)	rural or service industry;
industry - hazardous:	means an industry which, when in operation and when all measures
(or hazardous industry)	proposed to minimise its impact on the locality have been employed
	(including measures to isolate the industry from existing or likely future development on other land in the locality), would pose a significant risk in
	relation to the locality, to human health, life or property, or to the
	biophysical environment. Examples of such industry include oil refineries
	and chemical plants but would generally exclude light, rural or service
industry light.	industries;
industry - light: (or light industry)	means an industry:  (a) in which the processes carried on, the machinery used, and the
(or light industry)	goods and commodities carried to and from the premises do not
	cause any injury to or adversely affect the amenity of the locality;
	(b) the establishment or conduct of which does not, or will not, impose
	an undue load on any existing or proposed service for the supply or provision of essential services;
industry – noxious:	means any industry which causes premises to be defined as "Prescribed"
(or noxious industry)	Premises" or "Premises Subject to Registration" under the <i>Environment Protection Act 1986</i> ;"
industry – service:	means:
(or service industry)	(a) an industry - light carried out from premises which may have a retail shop front and from which goods manufactured on the premises
	may be sold; or (b) premises having a retail shop front and used as a depot for
	receiving goods to be serviced;
laundromat:	means any land and building, open to the public in which washing machines, with or without provision for drying clothes, are available for use;
liquor store:	liquor store — large means premises the subject of a liquor store
	licence granted under the Liquor Control Act 1988 with a net lettable
	area of more than 300 m2;
	<b>liquor store</b> — <b>small</b> means premises the subject of a liquor store licence granted under the <i>Liquor Control Act 1988</i> with a net lettable
	area of not more than 300 m2; AMD 7 GG 1/11/2016
lodging house:	has the meaning given to the term in the Health Act 1911, save that in
	this Scheme the term also includes premises providing board or lodging
	to 6 persons or less, but the term does not include the bed and breakfast use;
logistics centre	means land within which all activities relating to transport, logistics,

Schedule No. 1	
Land Use Definitions	Meaning
	warehousing and the distribution of goods are carried out and includes premises on the same land used for:  (a) the work of administration or accounting; (b) the provision of amenities for employees, incidental to any of those logistics operations;
	incidental to any of those logistics operations,
lunch bar:	means premises or part of premises used for the sale of takeaway food (in a form ready to be consumed without further preparation) within industrial or commercial areas;
marina:	means —  (a) premises used for providing mooring, fuelling, servicing, repairing, storage and other facilities for boats, including the associated sale of any boating gear or equipment; and  (b) all jetties, piers, embankments, quays, moorings, offices and storerooms used in connection with the provision of those services; AMD 7 GG 1/11/2016
marine filling station:	means premises used for the storage and supply of liquid fuels and lubricants for marine craft;
market:	means premises used for the display and sale of goods from stalls by independent vendors;
massage parlour	means a use of land involving the massaging manipulation or other treatment of body parts for therapeutic or remedial purposes, of a kind generally administered in association with medical treatment. The term does not include the provision of any sexual services;
medical centre:	means premises other than a hospital used by 3 or more health practitioners at the same time for the investigation or treatment of human injuries or ailments and for general outpatient care;  AMD 7 GG 1/11/2016
mining operations:	means premises where mining operations, as that term is defined in the <i>Mining Act 1978</i> section 8(1) is carried out; <i>AMD 7 GG 1/11/2016</i>
mobile phone tower & associated facilities	mobile phone towers and associated facilities that are considered "non low impact" under the <i>Telecommunications Act 1997</i> ;
motel:	means premises, which may be licenced under the <i>Liquor Control Act</i> 1988 —
	(a) used to accommodate guests in a manner similar to a hotel; and (b) with specific provision for the accommodation of guests with motor vehicles; AMD 7 GG 1/11/2016
motor vehicle, boat or caravan sales:	means premises used to sell motor vehicles, boats or caravans;
motor vehicle hire:	means any land or building used for the hiring out of motor vehicles and when conducted on the same site, the storage and cleaning of motor vehicles for hire but does not include mechanical repair or servicing of such vehicles;
motor vehicle repair:	<ul> <li>means premises used for or in connection with —         <ul> <li>(a) electrical and mechanical repairs, or overhauls, to vehicles other than panel beating, spray painting or chassis reshaping of vehicles; or</li> <li>(b) repairs to tyres other than recapping or re-treading of tyres;</li> </ul> </li> <li>AMD 7 GG 1/11/2016</li> </ul>
motor vehicle wash:	means premises where the primary use is the washing of motor vehicles;
motor vehicle wrecking:	means any land or building used for the storage, breaking up or dismantling of motor vehicles and includes the sale of second-hand motor vehicle accessories and spare parts on the same premises;
night club:	means premises the subject of a nightclub licence granted under the Liquor Control Act 1988; AMD 7 GG 1/11/2016
nursing home:	means any building used for the medical treatment and/or care of sick, aged or handicapped persons, whether resident or not, but does not include the hospital, consulting rooms, medical centre or health centre

Schedule No. 1		
Land Use Definitions	Meaning	
	uses;	
office:	means premises used for administration, clerical, technical, professional or other like business activities;	
open air display:	means the use of land as a site for the display and/or sale of goods and equipment;	
outbuilding:	a non habitable building being a private garage, carport, shed or the like associated with a residential use, but detached from the main dwelling unit;	
park home park:	means premises used as a park home park as defined in the Caravan Parks and Camping Grounds Regulations 1997 Schedule 8; AMD 7 GG 1/11/2016	
pet day care:	involves the use of any land or building for the day time care of pet animals, but does not involve the boarding of animals overnight, or the provision of substantial meals. The term may include limited and incidental provision of food by way of "treats" as part of play or training, or as an adjunct to the administration of an animal's requirement for medication. The term does not include a use that involves injury or risk of injury or prejudicial impact upon the amenity of the relevant locality by reason of the emission noise, smell, waste water or other waste products;	
place of worship:	means premises used for religious activities such as a church, chapel, mosque, synagogue or temple;	
public amusement:	means the use of any land or building for the amusement or entertainment of the public, with or without charge, but the term does not include an amusement parlour;	
public assembly - place of:	means any special place of assembly, , hall, grounds for athletics, all	
(or place of public assembly)	sports grounds with provision for spectators, a racecourse, a trotting track, stadia and/or showgrounds;	
radio or TV installation:	means any land or building used to accommodate any part of the infrastructure of a radio or TV network and includes any line, equipment, apparatus, tower, antenna, tunnel, duct, hole, pit or other structure used, or intended for use in or in connection with, a radio or TV network;	
reception centre:	means premises used for hosted functions on formal or ceremonial occasions; <i>AMD 7 GG 1/11/2016</i>	
recreation private:	means premises that are —	
(or private recreation)	<ul> <li>(a) used for indoor or outdoor leisure, recreation or sport; and</li> <li>(b) not usually open to the public without charge; AMD 7 GG 1/11/2016</li> </ul>	
recreation public:	means any land and buildings used for a public park, public gardens,	
(or public recreation)	foreshore reserve, playground or other grounds for recreation which are	
residential building:	usually open to the public without charge; has the same meaning as in the Residential Design Codes;	
residential building: restaurant:	restaurant/cafe means premises primarily used for the preparation, sale and serving of food and drinks for consumption on the premises by customers for whom seating is provided, including premises that are licenced under the Liquor Control Act 1988; AMD 7 GG 1/11/2016	
restricted premises:	<ul> <li>means premises used for the sale by retail or wholesale, or the offer for hire, loan or exchange, or the exhibition, display or delivery of — <ul> <li>(a) publications that are classified as restricted under the Classification (Publications, Films and Computer Games) Act 1995 (Commonwealth); or</li> <li>(b) materials, compounds, preparations or articles which are used or intended to be used primarily in or in connection with any form of sexual behaviour or activity; or</li> <li>(c) smoking-related implements; AMD 7 GG 1/11/2016</li> </ul> </li> </ul>	
retirement village:	means any land and buildings containing accommodation for aged and/or dependent persons together with ancillary facilities;	
salvage yard:	means any land or building used for the storage or sale of materials salvaged from the erection, demolition, dismantling or renovating of, or fire or flood damage to structures including (but without limiting the	

Schedule No. 1		
Land Use Definitions	Meaning	
	generality of the foregoing) buildings, machinery, and motor and marine vehicles;	
service station:	means premises other than premises used for a transport depot, panel beating, spray painting, major repairs or wrecking, that are used for —  (a) the retail sale of petroleum products, motor vehicle accessories and goods of an incidental or convenience nature; and/or  (b) the carrying out of greasing, tyre repairs and minor mechanical repairs to motor vehicles;  AMD 7 GG 1/11/2016	
serviced apartment:	means a group of units or apartments providing —  (a) self-contained short stay accommodation for guests; and (b) any associated reception or recreational facilities; AMD 7 GG 1/11/2016	
shop:	means premises other than a bulky goods showroom, a liquor store — large or a liquor store — small used to sell goods by retail, to hire goods, or to provide services of a personal nature, including hairdressing or beauty therapy services; AMD 7 GG 1/11/2016	
short stay accommodation	means the accommodation of short stay guests providing on site facilities for the convenience of guests and, management of the development, where occupation by any person is limited to a maximum of three months in any 12 month period, and excludes any other use falling within a use class specifically defined in this scheme;	
showroom:	means premises used to display, sell by wholesale or retail, or hire, automotive parts and accessories, camping equipment, electrical light fittings, equestrian supplies, floor coverings, furnishings, furniture, household appliances, party supplies, swimming pools or goods of a bulky nature;	
single house:	has the same meaning as in the Residential Design Codes;	
Small bar:	means premises the subject of a small bar licence granted under the Liquor Control Act 1988; AMD 7 GG 1/11/2016	
stable:	means any land, building or structure used for the housing, keeping or feeding of horses, asses or mules and associated incidental activities;	
studio:	means a building or part of a building used as a work-room by a painter, photographer, sculptor or craftsperson in the conduct of his/her profession and includes incidental display and sale of things made, decorated or adapted therein;	
tavern:	means premises the subject of a tavern licence granted under the <i>Liquor Control Act 1988</i> ; <i>AMD 7 GG 1/11/2016</i>	
telecommunications infrastructure:	means premises used to accommodate the infrastructure used by or in connection with a telecommunications network including any line, equipment, apparatus, tower, antenna, tunnel, duct, hole, pit or other structure related to the network; AMD 7 GG 1/11/2016	
tourist development:	means a building, or a group of buildings forming a complex, other than a bed and breakfast, a caravan park or holiday accommodation, used to provide —  (a) short-term accommodation for guests; and (b) onsite facilities for the use of guests; and (c) facilities for the management of the development; AMD 7 GG 1/11/2016	
trade display:	means premises used for the display of trade goods or equipment for the purpose of advertising;	
trade supplies:	means premises used to sell by wholesale or retail, or to hire, assemble or manufacture any materials, tools, equipment, machinery or other goods used for the following purposes including goods which may be assembled or manufactured off the premises —  (a) automotive repairs and servicing;  (b) building including repair and maintenance;  (c) industry;  (d) landscape gardening;	

Schedule No. 1				
Land Use Definitions	Meaning			
	(e) provision of medical services;			
	(f) primary production;			
	(g) use by government departments or agencies, including local government; AMD 7 GG 1/11/2016			
transport depot:	means premises used primarily for the parking or garaging of 3 or more			
transport depot.	commercial vehicles including —			
	(a) any ancillary maintenance or refuelling of those vehicles; and			
	(b) any ancillary storage of goods brought to the premises by those			
	vehicles; and			
	(c) the transfer of goods or persons from one vehicle to another;			
	AMD 7 GG 1/11/2016			
truck stop	means land used for the parking of commercial vehicles and may include			
	facilities for ancillary accommodation of drivers and the retail sale of			
	petroleum products and convenience goods, but excludes the garaging,			
	maintenance, management, and repair of any motor vehicle;			
veterinary centre:	means premises used to diagnose animal diseases or disorders, to surgically or medically treat animals, or for the prevention of animal			
	diseases or disorders; AMD 7 GG 1/11/2016			
vet consulting room:	means any land and/or building where a veterinary surgeon or			
vet consuming room.				
	veterinarian treats the minor ailments of domestic animals and household pets as patients but in which animals or pets do not remain			
	overnight, and may include a dispensary of medications incidental			
	thereto; AMD 7 GG 1/11/2016			
veterinary hospital:	means any land and/or building used in connection with the treatment of			
	animal injuries and ailments, and includes the care and accommodation			
	of animals during or after such treatment; means any building, or part of a building used for the purpose of sale by			
video store:				
	retail or wholesale, or the offer for hire, loan or exchange, or the			
warehouse:	exhibition, display or delivery of pre-recorded video material;			
warenouse:	means premises used to store or display goods and includes premise on the same land used for::			
	(a) the work of administration or accounting;			
	(b) the selling of goods by wholesale; or			
	(c) the provision of amenities for employees,			
	incidental to any of those warehouse operations;			
waste storage facility:	means premises used to collect, consolidate, temporarily store or sort			
	waste before transfer to a waste disposal facility or a resource recovery			
	facility on a commercial scale; AMD 7 GG 1/11/2016			

#### Schedule No. 2 - Additional Uses

## No. **Location and Additional Uses** 1(a) For all land included in the town centre frame which is zoned Residential R20/50/100 and detailed in the plan below, the local government may approve the following additional land uses: art gallery consulting rooms office studio Town Centre Town Centre Wright Street Belmont Forum carpark carpark 'illage District Shopping Centre Centre Town Belmont Forum Cinema extensions Precinct D When considering applications for the establishment of the abovementioned uses, the following shall be taken into consideration: (i) Opportunities for upgrading and renovations of existing buildings to achieve a high architectural standard: Development or redevelopment to have a high level of regard for the (ii) recommendations of the Town Centre Frame Study; (iii) The potential impact of any proposal on the amenity of abutting and nearby residential properties; The extent to which the design of the proposal harmonises with adjoining and (iv) nearby buildings and the general streetscape within the same precinct area. 1(b) For land within Precinct A, the local government may approve the following two additional land uses: showroom

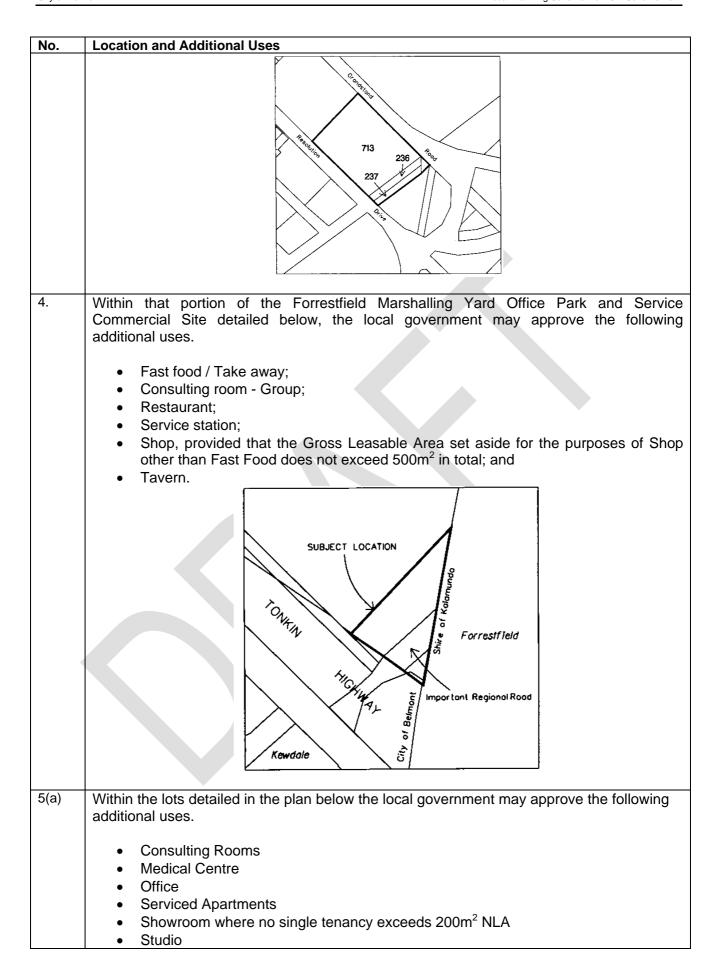
When considering applications for the establishment of showroom or video outlet uses, the

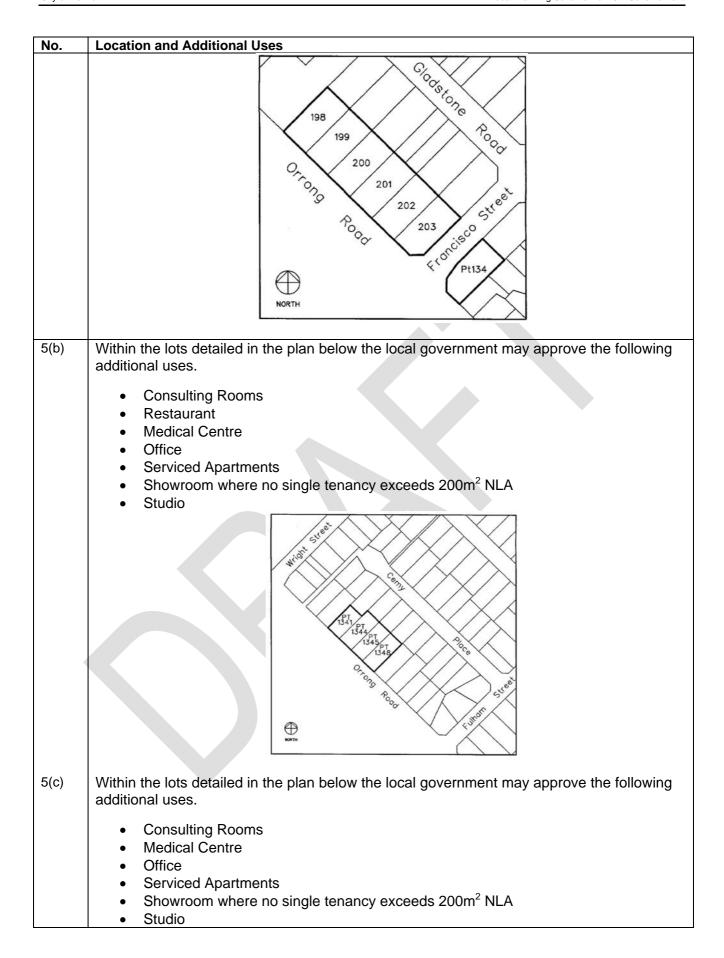
video outlet

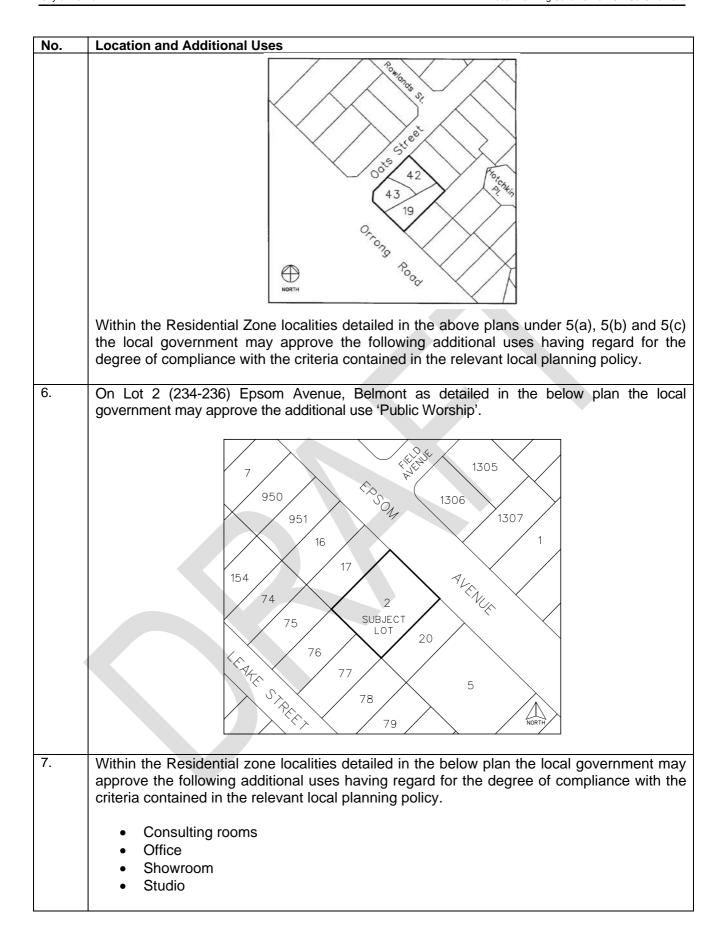
No.	Locat	ion and Additional Uses		
	following requirements shall be met:			
	(i)	Lots shall be amalgamated to achieve a minimum lot size of 1,500m <sup>2</sup> and all existing improvements on the property shall be demolished;		
	(ii)	Development or redevelopment to have a high level of regard for the recommendations of the Town Centre Frame Study;		
	(iii)	The potential impact of any proposal on the amenity of abutting and nearby residential properties;		
	(iv)	The extent to which the design of the proposal harmonises with adjoining and nearby buildings and the general streetscape within the same precinct area;		
	(v)	Demonstration that adequate car parking can be provided on site in accordance with Table 2 of the City of Belmont Local Planning Scheme No. 15;		
	(vi)	Prior to any development of a site, the local government may require the formulation of vehicular access plan for the property and abutting sites;		
	(vii)	As a condition of development approval, the local government may require that a public easement in gross be provided to promote shared vehicular access between lots;		
	(viii)	For showroom uses, provision of a loading/unloading bay and provision of a truck movement plan demonstrating adequate manoeuvrability and turning circles;		
	(ix)	Adequate landscaping along the frontage of the lot with a minimum width of 1.5 metres;		
	(x)	Landscaping plans to use species consistently used for existing developments in the same precinct.		
1(c)	For land within Precinct C, the local government may approve retail floor space for new development that meets the following criteria:			
	(i)	Lots shall be amalgamated to achieve a minimum lot size of 1,000m <sup>2</sup> and all existing improvements on the property shall be demolished;		
4	(ii)	The total area of retail floor space shall not exceed 35% of the gross floor area of the development, and the gross floor area shall not include any portion of the development to be used for residential purposes;		
	(iii)	Development or redevelopment to have a high level of regard for the recommendations of the Town Centre Frame Study;		
	(iv)	parking, vehicular access and easements in gross to be in accordance with the City of Belmont Access Policy Plan for Fulham Street (Belmont Avenue to Robinson Avenue).		
1(d)	space	and within Precinct E, the local government may approve a percentage of retail floor e for mixed use developments that include a substantial residential component up to a num density of R60, subject to the following requirements being met:		
	(i)	The total area of retail floor space shall not exceed 35% of the gross floor area of the development. For the purpose of calculating permissible retail floor space, the gross floor area shall not include any portion of the development to be used for residential purposes;		
	(ii)	Lots shall be amalgamated to achieve a minimum area of 1,000m <sup>2</sup> and all existing improvements on the property shall be demolished;		
	(iii)	Development or redevelopment to have a high level of regard for the		

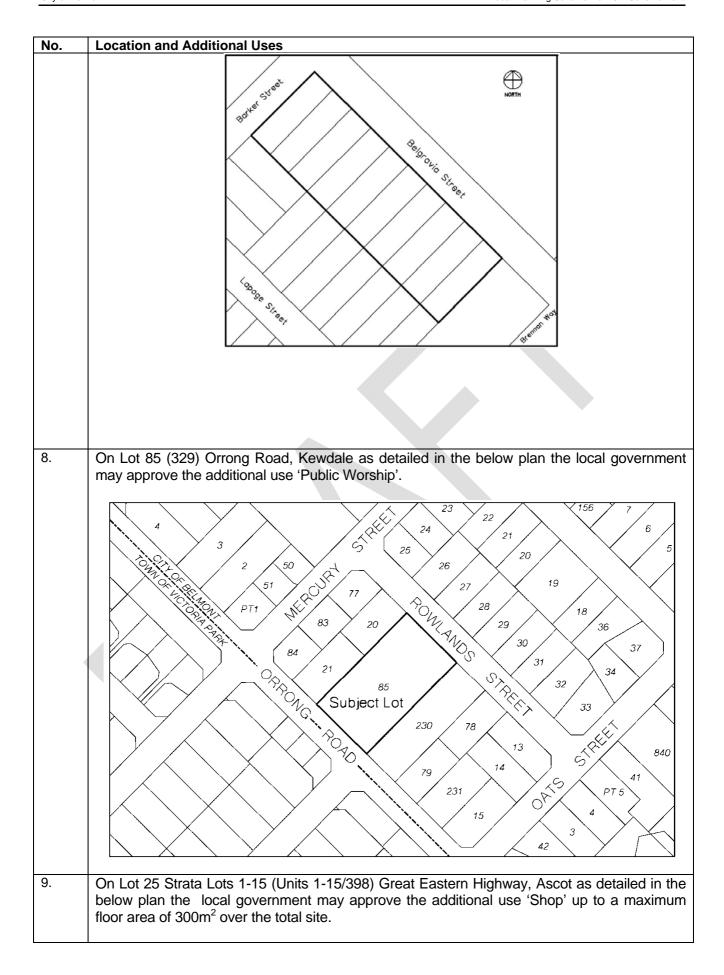
No.	recommendations of the Town Centre Frame Study;			
	/i. A			
	(iv)	Development shall have a minimum of two storeys;		
	(v)	Development shall incorporate a mixture of land uses and in the opinion of the local government, include a substantial residential component;		
	(vi)	Development to achieve a high standard of development through imaginative layout, variation in roof height and building design;		
	(vii)	Car parking shall be calculated separately where there is a mixture of land uses (eg office and residential). The local government may consider car parking dispensations where it is satisfied that on street car parking will be available for use outside normal business hours, or where it can be demonstrated that there is a certain amount of reciprocity between land uses;		
	(viii)	All residential dwelling units shall be provided with private open space in the form of a balcony, or ground floor open space, not less than 1.5 metres depth and a minimum area of 4sqm;		
	(ix)	All dwellings shall be provided with a 4sqm storeroom with a minimum internal width of 1.5 metres.		
		considering applications for the establishment of the above uses the following is shall be taken into consideration:		
	(a)	The need to limit vehicular access to premises, particularly Abernethy Road by the provision of reciprocal rights of carriageway.		
	(b)	The desirability of retaining some residential development in order to achieve a mix of residential and office use.		
	(c)	The desirability and practicability of converting existing residences to offices in preference to pursuing total redevelopment.		
	(d)	The standards normally applied to such land uses, including car parking, shall generally be in accordance with the relevant provisions of Local Planning Scheme No. 15.		
•	(e)	The need for the local government to invoke provisions contained in Schedule 2 Part 8, Clause 64 of the of the <i>Planning and Development (Local Planning Schemes) Regulations 2015</i> , thereby requiring notice of proposed developments or initial changes of use to be advertised.		
2.		the Residential R20/R100B zone detailed below the local government may approve lowing additional uses:		
	•	Consulting Rooms Fast Food/ Takeaway Medical Centre Studio Tavern Video Store Shop Office Restaurant		

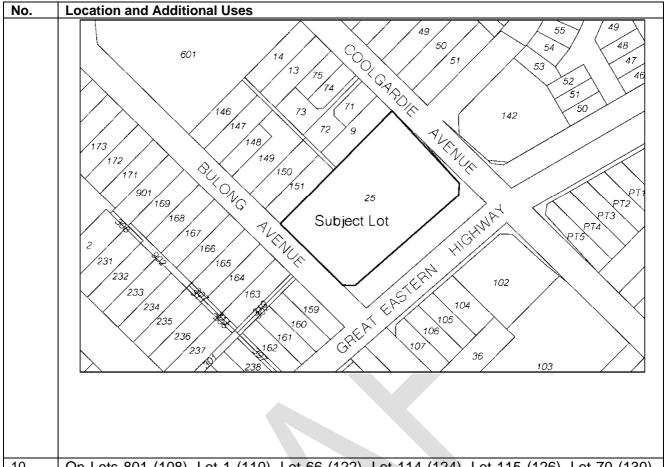
No.	ocation and Additional Uses
	Then considering applications for the establishment of the above uses, the following shall
	<ul> <li>Applications to use or develop the land for the use of 'Tavern' shall be advertised in accordance with the provision contained in Schedule 2 Part 8, Clause 64 of the of the Planning and Development (Local Planning Schemes) Regulations 2015.</li> </ul>
	The potential impact of any proposal on the amenity of abutting and nearby residential properties.
	The extent to which the design of the proposal harmonises with adjoining and nearby buildings and the theme of the marina environment.
	v) The robustness of the land use and associated building, and the ability of the land use to change without adverse impact on the precinct.
	The development standards which shall apply to such uses shall generally be in accordance with relevant Local Planning Policies adopted under this Scheme in accordance with provisions contained in Part 2, Division 2 of the of the <i>Planning and Development (Local Planning Schemes) Regulations 2015</i> Schedule 2.
3.	/ithin the Mixed Use Zone detailed below the local government may approve a shop as n additional use.







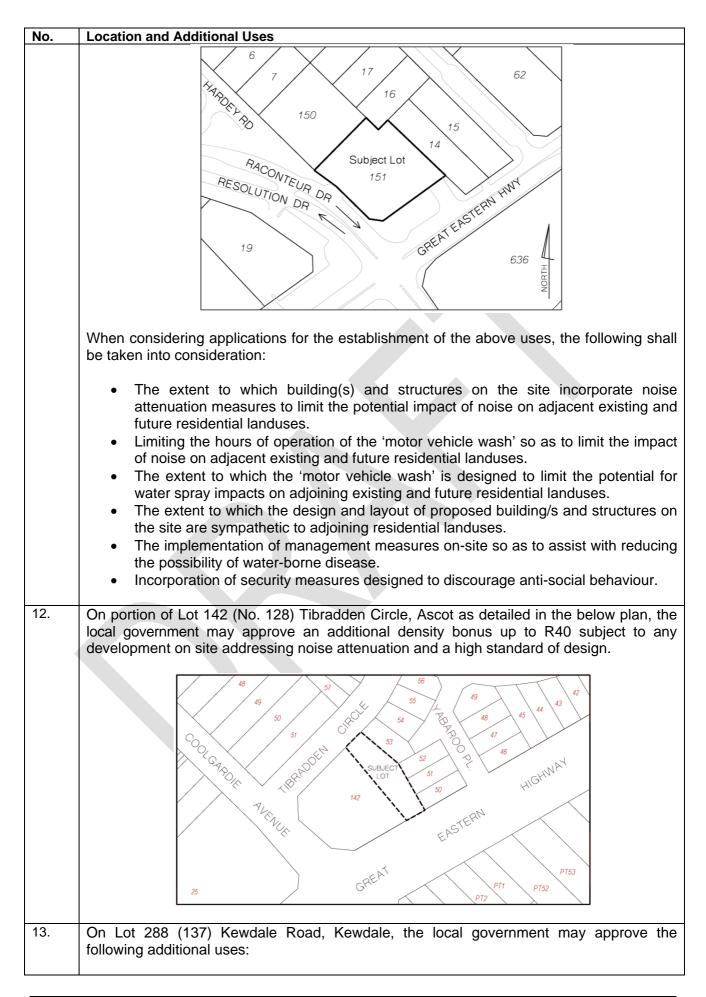




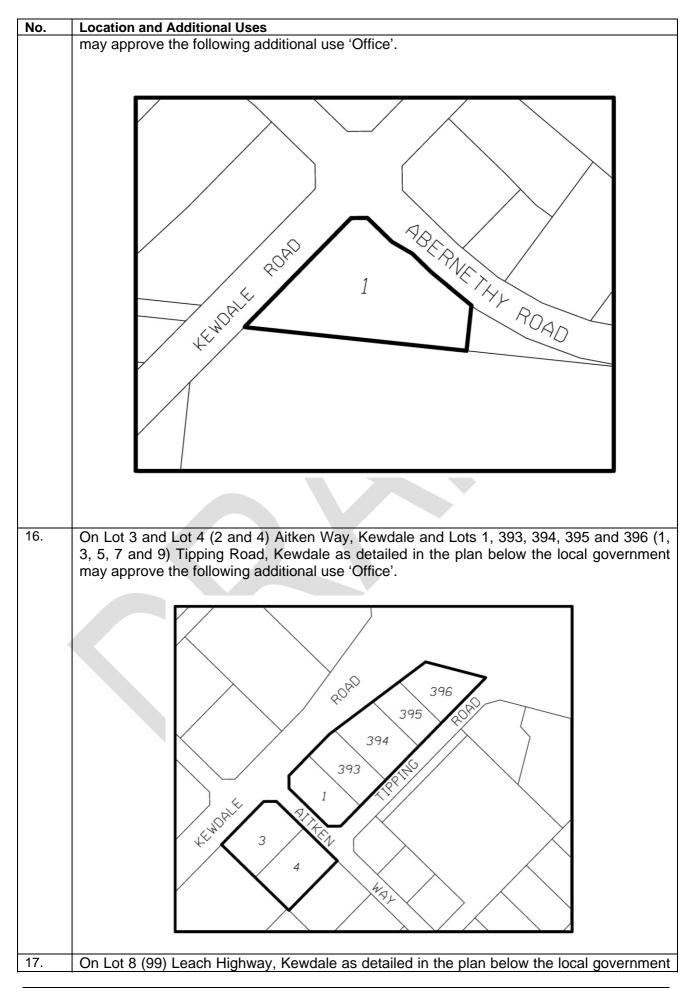
10. On Lots 801 (108), Lot 1 (110), Lot 66 (122), Lot 114 (124), Lot 115 (126), Lot 70 (130), Lot 98 (132), Lot 10 (136) Belmont Avenue and Lot 11 (185) Alexander Road, the local government may consider approving the additional uses of office, consulting rooms and studio where the development incorporates a residential component.



11. On Lot 151 (210) Great Eastern Highway, Ascot as detailed in the below plan, the local government may approve the additional uses of 'convenience store' and 'motor vehicle wash'. *AMD 1 GG 15/12/2015* 

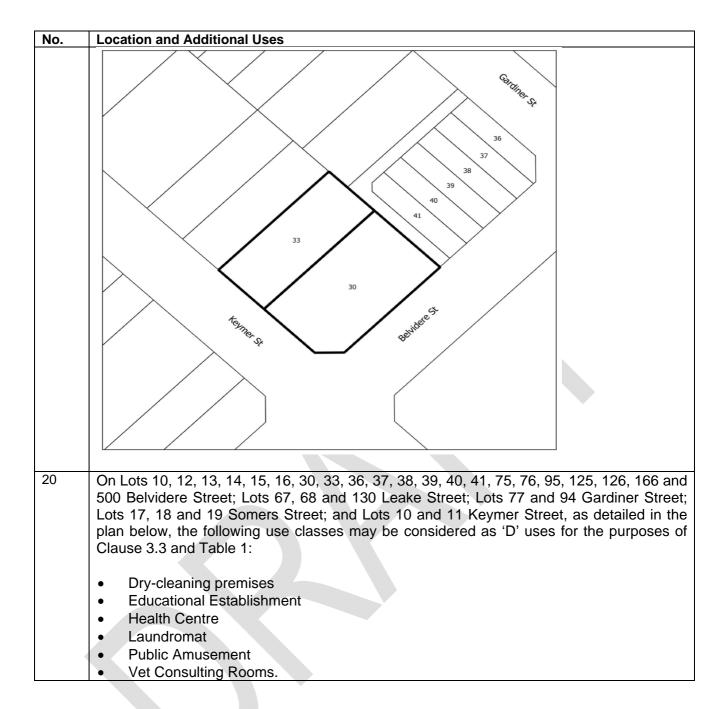


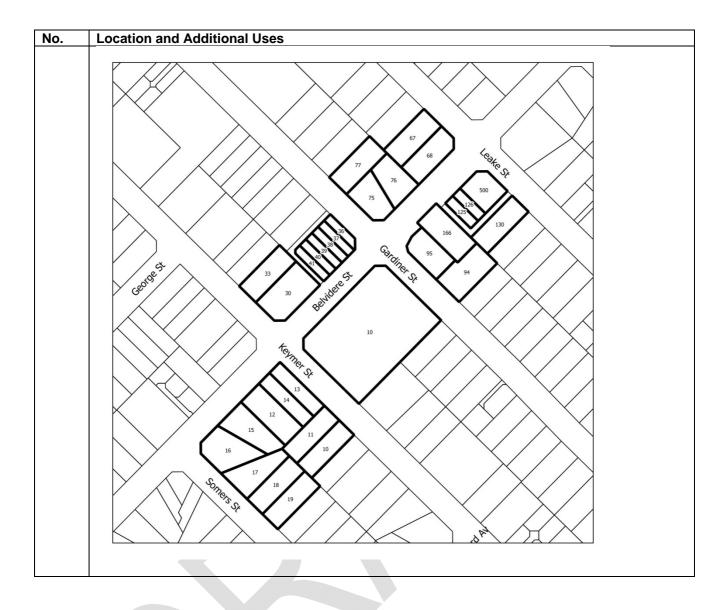
No.	Location and Additional Uses
1101	Consulting Rooms
	Health Centre
	Liquor Store
	Lunch Bar
	Medical Centre
	Office
	Restaurant
	Tavern
	R 38447 Drainage 10478
	LOC 10553
	451
	\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\
	401
	402 KENDRET LOT
	288
	Kewdale Freight Terminal
	1 STREET
	451
	NORTH
	A
14.	On Late 44, 40, 975 and 4 (449, 490, 490 and 499) Kauydala Daad, Kauydala as datailad in
14.	On Lots 11, 10, 275 and 1 (118, 126, 128 and 133) Kewdale Road, Kewdale as detailed in the plan below the local government may approve the following additional use 'Office'.
	AMD 1 GG 15/12/2015
	711112 1 33 13,12/2010
	MILES
	Pogo Rote Eling
	POSID REST STREET
	275 STREET
	Land Control of the C
	11 LEWISTE 1
1	On Lot 1 (524) Kewdale Road, Kewdale as detailed in the plan below the local government



## **Location and Additional Uses** No. may approve the following additional use 'Office'. HCHMPY JBERNETHY PORD 8 FRCH On Lot 9002 and Lot 7705 (71) Grandstand Road and Lot 100 (1) Raconteur Drive, the 18. local government may consider the following additional uses: Horse Sales Stables SUBJECT LOCATION 19 On Lots 30 and 33 Belvidere Street as detailed in the plan below, the use class of 'Service

Station' may be considered as a 'D' Use for the purpose of clause 4.3 and Table 1.





## **Schedule No. 3 - Restricted Uses**

There are no restricted uses which apply to the Scheme.

## Schedule No. 4 - Special Use Zones

There are no Special Use Zones which apply to the Scheme.

## **Schedule No. 5 - Exempted Advertisements**

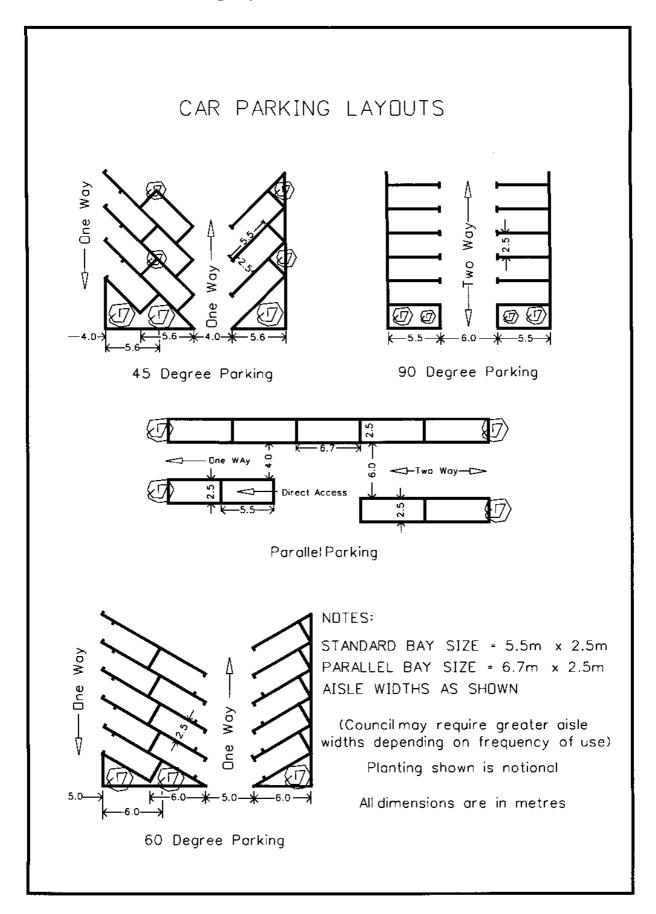
LAND USE AND/OR DEVELOPMENT REQUIRING ADVERTISEMENT	EXEMPTED SIGN TYPE AND NUMBER (includes the change of posters on poster signs and applies to non-illuminated signs unless otherwise stated)	MAXIMUM AREA OF EXEMPTED SIGN
DWELLINGS	One professional name-plate as appropriate.	0.2m <sup>2</sup>
HOME OCCUPATION	One advertisement describing the nature of the home occupation.	0.2m <sup>2</sup>
PUBLIC PLACES AND RESERVES	(a) Advertisement signs (illuminated and non- illuminated) relating to the functions of government, a public authority or a local government, excluding those of a promotional nature constructed or exhibited by or on behalf of any such body; and	Not applicable
	(b) Advertisement signs (illuminated and non- illuminated) required for the management or control of traffic on any public road, car park, cycleway, railway, or waterway where such advertisement has been constructed or exhibited by or at the direction of a government department, public authority, or a local government; and	Not applicable
	(c) Advertisement signs (illuminated or non-illuminated) required to be exhibited by or pursuant to any statute or regulation or the like made pursuant to powers contained within a Statute provided that any such advertisement is constructed and/or exhibited strictly in accordance with the requirements specified herein.	Not applicable
	(d) Advertisement signs (illuminated or non- illuminated) to promote a significant project and/or public work undertaken by the City of Belmont.	15m <sup>2</sup>
RAILWAY PROPERTY AND RESERVES	Advertisement signs exhibited on such land provided that each such advertisement is directed only at persons at or upon a railway station.	No sign shall exceed 2m <sup>2</sup> in area
ADVERTISEMENTS WITHIN BUILDINGS	All advertisements placed or displayed within buildings which cannot ordinarily be seen by a person outside of those buildings.	Not applicable
BUILDING CONSTRUCTION SITES (ADVERTISEMENT SIGNS DISPLAYED ONLY FOR THE DURATION OF THE CONSTRUCTION AS FOLLOWS):	(i) <u>Dwellings</u> One advertisement per street frontage containing details of the project and professional consultants and the contractors undertaking the construction work.	2m²
,	(ii) Multiple dwellings, shops, commercial and industrial projects One sign as for i) above.	5m <sup>2</sup>
	(iii) <u>Large development/re-development projects</u> involving shopping centres, offices, or other	

LAND USE AND/OR DEVELOPMENT REQUIRING ADVERTISEMENT	EXEMPTED SIGN TYPE AND NUMBER (includes the change of posters on poster signs and applies to non-illuminated signs unless otherwise stated)	MAXIMUM AREA OF EXEMPTED SIGN
	<ul> <li>buildings exceeding three stories in height.</li> <li>One sign as for i) above, and</li> <li>One additional sign showing the name of the project building.</li> </ul>	5m <sup>2</sup> 10m <sup>2</sup>
PROPERTY TRANSACTIONS	Advertisement signs displayed for the duration of the period over which property transactions are offered and negotiated, as follows:	
	(i) <u>Dwellings</u> One sign per street frontage for each property relating to the sale, leasing or impending auction of the property at or upon which the sign is or the signs are displayed.	Each sign shall not exceed an area of 2m <sup>2</sup>
	(ii) Multiple dwellings, shops, commercial and industrial properties One sign as for (i) above	Each sign shall not exceed an area of 5m <sup>2</sup>
	(iii) <u>Large properties</u> comprised of shopping centres, buildings in excess of four storeys, and rural properties in excess of 5ha One sign as for (i) above.	Each sign shall not exceed an area of $10\text{m}^2$
DISPLAY HOMES	Advertisement signs displayed for the period which homes are on display for public inspection	
	(i) One sign for each dwelling on display	2m <sup>2</sup>
	(ii) In addition to (i) above, one sign for each group of dwellings displayed by a single project builder giving details of the project building company and details of the range of dwellings on display.	5m <sup>2</sup>

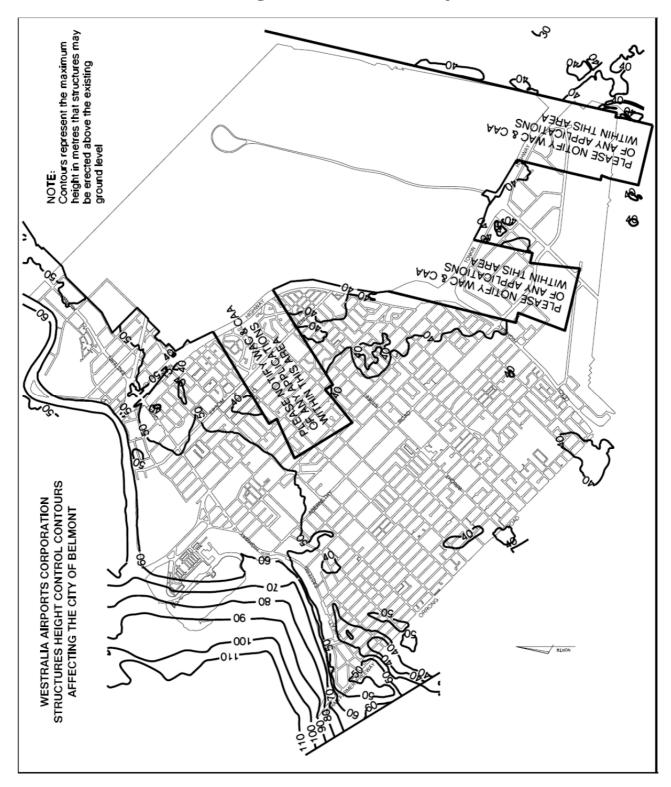
## **Schedule No. 6 - Environmental Conditions**

There are no environmental conditions imposed by the Minister for Environment which apply to the Scheme.

#### Schedule No. 7 - Car Parking Layout



### **Schedule No. 8 - Structures Height Control Contours Map**



## Schedule No. 9 - Special Control Areas - Development Areas

REF. NO.	AREA	PROVISIONS
SCA - DA3	Land fronting Kew Street and abutting Dod Reserve.	An approved Structure Plan together with all approved amendments shall apply to the land in order to guide subdivision and development.
SCA - DA4	Land bounded by Ryans Court, Morrison Street, Stanton Road and Tonkin Highway Redcliffe.	An approved Structure Plan together with all approved amendments shall apply to the land in order to guide subdivision and development.
		2. To provide for residential development.
		3. Not less than seventy-five percent (75%) of all land within the Residential Zone shall be developed for the purpose of single houses.
SCA - DA5	Land bounded by Stanton Road, Morrison Street, Greenshields Way and Tonkin Highway.	An approved Structure Plan together with all approved amendments shall apply to the land in order to guide subdivision and development.
		2. To provide for residential development.
SCA - DA6	Land bounded by Tonkin Highway, Great Eastern Highway, Coolgardie Avenue, Redcliffe Road, Fauntleroy Avenue and Precincts 1A and 1B as defined under the Perth Airport Masterplan 2004.	An approved Structure Plan together with all approved amendments shall apply to the land in order to guide subdivision and development.
SCA - DA7	Land bounded by Tibradden Circle, Coolgardie Parade, Central Avenue, Garvey Park, and the back of lots 57 to 75 Fernridge Cove.	<ol> <li>An approved Structure Plan together with all approved amendments shall apply to the land in order to guide subdivision and development.</li> <li>To provide for residential development.</li> </ol>
SCA - DA8	Land bounded by Hay Road, Lilian Grove, Fauntleroy Avenue and the eastern boundary of Lot 258 Fauntleroy Avenue.	An approved Structure Plan together with all approved amendments shall apply to the land in order to guide subdivision and development.
	-	2. To provide for residential development.
SCA - DA9	Land bounded by Fauntleroy Avenue, Hay Road, Lot 185 Hay Road and the rear of the Mixed Use zoned lots fronting Great Eastern Highway.	An approved Structure Plan together with all approved amendments shall apply to the land in order to guide subdivision and development.
		2. To provide for residential development.
SCA - DA10	Lot 120 Epsom Avenue and	1. An approved Structure Plan together with all

REF. NO.	AREA	PROVISIONS
	Lot 111 Nisbet Street, Ascot (Ascot Inn)	<ul><li>approved amendments shall apply to the land in order to guide subdivision and development.</li><li>To incorporate the existing heritage place in</li></ul>
		any structure plan for the area.
SCA - DA11	The Springs bounded by the Swan River, Brighton Road, Great Eastern Highway and Graham Farmer Freeway	An approved Structure Plan together with all approved amendments shall apply to the land in order to guide subdivision and development.

#### Schedule No. 10 - Statutory Static Feasibility Assessment Model

#### STATUTORY STATIC FEASIBILITY ASSESSMENT MODEL

Gross realisation			
Net lot yield @ average market value per lot	•		(4)
"X" lots @ "\$Y" per lot	\$		(1)
Less GST @ standard / normal rates	•		(-)
(1) Multiplied by GST rate/(100+GST rate) (1-2)	<u>\$</u>	\$	(2) (3)
Less selling, marketing, advertising & settlement fees			
@ market % multiplied by (1) Add back Input Tax Credit on selling fees	\$		(4)
(4) Multiplied by GST rate/(100+GST rate) (4-5)	<u>\$</u>	<u>\$</u> \$	(5) (6)
Balance after selling costs etc & Input Tax Credit (3-6)		\$	(7)
Less adjusted profit & risk allowance as per SPP 3.6			
Market determined profit & risk allowance %			(8)
Less fixed profit allowance per SPP3.6  Risk rate applied (8-9)  = %			(9) (10)
EXPLANATION: (10) to be expressed as a whole number eg 15% = 15			(10)
ie Risk = (7) multiplied by (10)/100+(10)		<u>\$</u> \$	(11)
Balance after profit & risk factor (7-11)		\$	(12)
Less development costs @ "X" lots multiplied by "\$Z" per lot	\$		(13)
Add back Input Tax Credit on (13) (13) Multiplied by GST rate/(100+GST rate) Development cost after Input Tax Credit (13-14)	<u>\$</u> \$		(14) (15)
Add interest on net development costs (15)	•		(1-5)
For 1/2 development and 1/2 selling term			
@ Applicable market rates			
(15) Multiplied by % rate	<u>\$</u>	¢	(16) (17)
(15+16) Balance after deduction of development costs & interest (12-17)		<u>\$</u> \$	(17) (18)
Less interest on land value, rates & taxes and stamp duty		•	(1-5)
Assessed over 1/2 development and 1/2 selling term  @ Applicable market rates			
(18) Multiplied by (% rate/100+%rate)		\$	(19)
Balance after interest on the land (18-19)		<u>\$</u> \$	(20)
Less rates & taxes		\$	(21)
Balance after rates & taxes (20-21)		\$	(22)
Less Stamp Duty @ current statutory rates			
(22) Multiplied by stamp duty rate/(100+stamp duty rate)		<u>\$</u> \$	(23)
Residual Land Value prior to GST considerations (22-23)			(24)
Add GST (24) + GST at prevailing statutory rate		\$	(25)
ASSESSED STATUTORY CONTRIBUTION PER SPP 3.6 (22+23)	\$		

#### The Static Feasibility Model is based upon:

- (i) The number of lots yielded from the land will have a gross sale price which, when multiplied by the number of lots created, establishes the Gross Realisation (i).
- (ii) GST will be calculated by the standard/normal method.
- (iii) Selling, marketing, advertising and settlement fees expressed as a percentage shall be added and then expressed as a total percentage against the gross realisation.
- (iv) The adjusted risk component applied in the model is the established market profit and risk at the date of valuation less the fixed 10 per cent profit applied in SPP 3.6.
- (v) Development costs will be established as an appropriate servicing cost per lot at the date of valuation, multiplied by the lots realised from the land.
- (vi) Interest against the development costs will be established by the application of bank lending rates for such projects at the date of valuation.
- (vii) Interest against the land in development will be established by the application of bank lending rates for such development acquisitions at the date of valuation.
- (viii) Rates and taxes will be applied for the full term of acquisition, development and sale.
- (ix) Stamp Duty will be applied at the statutory rate as applicable at the date of valuation.
- (x) GST will be applied at the appropriate rate adopted at the date of valuation.

## Schedule No. 11 - Special Control Areas - Development Contribution Areas

Reference No	Development Contribution Area 1						
Area Name:	The Springs Special Development Precinct (The Development						
	Contribution Area comprises all the land referred to as The Springs Special Development Precinct identified by Scheme Maps as (SCA - DCA1)).						
Relationship to other planning instruments:	The development contribution plan generally conforms to the following endorsed plans:						
	City of Belmont Strategic Community Plan 2016 - 2036;						
	Local Planning Scheme No. 15;						
	Western Australian Planning Commission State Planning Policy 3.6 (Development Contributions for Infrastructure); and						
	The Springs Structure Plan.						
Infrastructure and administrative costs to be funded:	Contributions shall be made towards the following items by all landowners:						
	Civil construction costs relating to the provision and upgrading of necessary and shared public infrastructure, specifically:						
	<ul> <li>Mobilisation</li> <li>Site works</li> <li>Retaining walls</li> <li>Sewer reticulation</li> <li>Stormwater and drainage</li> <li>Water reticulation</li> <li>Road works to existing roads (excluding Riversdale Road east of Rowe Avenue and west of Brighton Road).</li> </ul>						
	Electrical infrastructure costs, including high voltage reinforcement where necessary.						
	3. Landscaping construction and remediation costs, specifically:						
	<ul><li>Public open space</li><li>Streetscape and public realm.</li></ul>						
	4. Professional and administrative fees relating to:						
	<ul> <li>Environmental Remediation fees associated with the (remediation of public open space only);</li> <li>Civil Engineering fees associated with: (relating to civil design and public utility upgrades);</li> <li>Civil and landscaping design;</li> <li>Infrastructure upgrades;</li> <li>Hydrological and urban water management; and</li> </ul>						

Reference No	Development Contribution Area 1					
	<ul> <li>Parking &amp; Traffic Impact System.</li> <li>Urban Water Management</li> <li>Landscape Architecture (associated with public open space, streetscape and public realm)</li> <li>Civil Construction (relating to management of civil works).</li> </ul>					
Method for Calculating Contributions:	The development contribution for each lot within The Springs shall be calculated on the basis of Infrastructure Costs+Electricity Upgrade Costs, as follows:					
	<ul> <li>Infrastructure Cost: The contribution for individual lots for Infrastructure Costs shall be apportioned pro-rata based on the square meterage of each lot;</li> </ul>					
	Electricity Costs: The contribution for individual lots for Electricity Upgrade Costs shall be calculated pro-rata based on the anticipated demand generated by each lot (based on development potential) less the current electricity capacity and					
	High Voltage electricity Reinforcement: The contribution for high voltage electricity reinforcement shall be apportioned to lots designated as 'Mixed Use' under The Springs Structure Plan and calculated pro-rata based on the anticipated demand generated by each lot (based on development potential) less the current electricity capacity					
	The following areas shall be excluded from the land area calculations of both the total land area in the Development Contribution Area and the Owner's land in the Development Contribution Area:					
	Roads designated under the Metropolitan Region Scheme as Primary Regional Roads and Other Regional Roads					
	Existing public open space					
	Drainage reserves					
	Public utility sites					
	Other land required for Infrastructure Works					
Period of operation:	The Development Contribution Plan shall operate for a period of 5 years from the date of gazettal.					
Priority and timing:	Clearing and Earthworks (Complete)					
	Drainage Basin Retaining Walls (Complete)					

Reference No	Development Contribution Area 1				
	Roads (Complete)				
	Drainage (Complete)				
	Water Reticulation (Complete)				
	Sewer Reticulation (Complete)				
	Street Lighting and Power ( <b>Complete</b> )				
	Landscaping (Stage 2)				
Review process:	The Development Contribution Plan will be reviewed 5 years from the date of gazettal of the amendment to incorporate the plan, or earlier should the local government consider it appropriate having regard to the rate of development in the area and the degree of development potential still existing.				
	The estimated infrastructure costs contained in the cost apportionment schedule will be reviewed at least annually to reflect changes in funding and revenue sources and indexed based on the Building Cost Index or other appropriate index as approved by the qualified person undertaking the certification of costs referred to in Clause 5.2.11.3 of Local Planning Scheme No 15.				

AMD 2 GG 07/02/2017

Approved by Resolution of the Council of the City of Belmont at the Special Meeting of Council held on the 25th day of November 2008.

Mayor	Mgadfrey  Executive Officer	/o./o. t/ Date  /o./o. t/ Date			
	- /	2413			
FINAL	. APPROVAL				,
1.	Adopted by Resolution of the C Council held on 14 <sup>th</sup> day of Feb the Resolution hereto in the pre	oruary 2011 and the	of Belmo e seal of	nt at the Special Meeti the Municipality was pu	ng of the irsuant to
	Mayor Chief Executive Officer	10.10.	Date //		2100010
2.	Recommended/Submitted for Commission.	Final Approval	by the	Western Australian	Planning
	Chairperson, Western Austral Planning Commission	lian	. 1	Date	
3.	Final Approved granted.				1 - 1
		•			

It is Ministeriffedriffedriffedrig is in the copy of the Scheme/Amendment, final approval to which was endorsed by the Minister for Planning on 4/11/11.

Certified by .....

CM Sanders

Date



# Ordinary Council Meeting 26/02/19

Item 12.4 refers

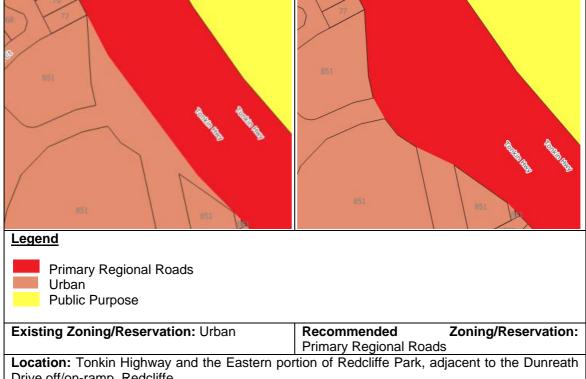
## **Attachment 13**

## Recommended Metropolitan region Scheme Map Amendments



#### <u>ATTACHMENT 4 – RECOMMENDED MRS SCHEME MAP AMENDMENTS</u>

#### Area 1 - Tonkin Highway



Drive off/on-ramp, Redcliffe.

Mapping Amendment Description: Rezoning a portion of the Tonkin Highway road reserve in Redcliffe from 'Urban' to 'Primary Regional Roads'.

Justification: To appropriately reserve land that encompasses the Tonkin Highway road carriageway.

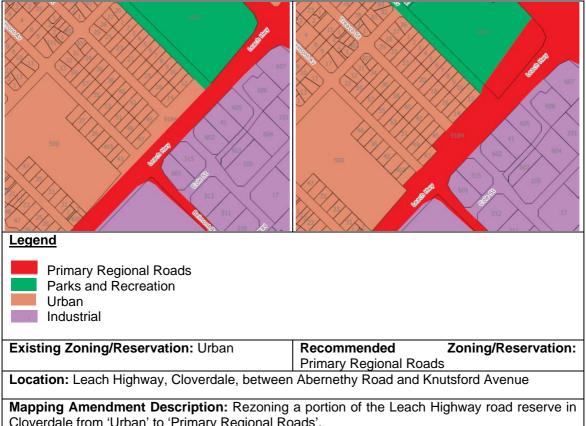
#### Area 2 - Leach Highway



Mapping Amendment Description: Rezoning a portion of the Leach Highway road reserve in Cloverdale from 'Urban' to 'Primary Regional Roads'.

Justification: To appropriately reserve land that encompasses the Leach Highway road carriageway and principal shared path.

### Area 3 - Leach Highway



Cloverdale from 'Urban' to 'Primary Regional Roads'.

Justification: To appropriately reserve land that encompasses the Leach Highway road carriageway and principal shared path.

# Area 4 – Leach Highway

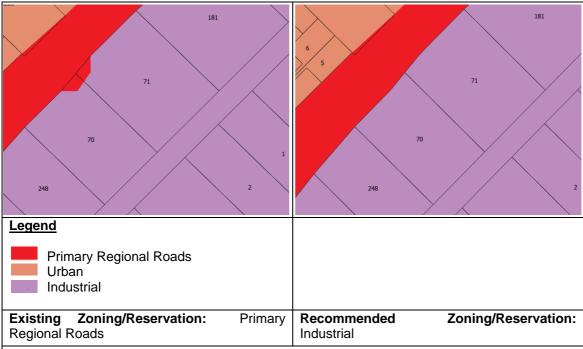


Location: Leach Highway, Kewdale, between Jeffrey Street and Armitt Street

**Mapping Amendment Description:** Rezoning a portion of the Leach Highway road reserve in Kewdale from 'Urban' to 'Primary Regional Roads'.

**Justification:** To appropriately reserve land that encompasses the Leach Highway principal shared path.

# Area 5 – Leach Highway



Location: Lots 70 (No. 11-13) and 71 (No. 17-19) Ballantyne Road, Kewdale.

**Mapping Amendment Description:** Rezoning a portion of Lots 70 (No. 11-13) and 71 (No. 17-19) Ballantyne Road, Kewdale, from 'Primary Regional Roads' to 'Industrial'.

**Justification:** The truncation was intended to access the former local road 'Cobb Street, however this road was formally closed in 2003.

# Area 6 - Leach Highway



Location: Leach Highway, Kewdale, between Miles Road and Belmont Avenue.

**Mapping Amendment Description:** Rezoning a portion of the Leach Highway road reserve in Kewdale from 'Industrial' to 'Primary Regional Roads'.

**Justification:** To appropriately align the Primary Regional Road reservation to encompasses the Leach Highway road reserve.

### Area 7 – Tonkin Highway



70) and 3 (No 72) Stanton Road, Redcliffe

Mapping Amendment Description: Rezoning Lots 471 (No.7), 472 (No.9), 67 (No.11) Ryans Court, Redcliffe and Lots 5886 (No. 70) and 3 (No 72) Stanton Road, Redcliffe, from 'Primary Regional Roads' to 'Urban'.

Justification: This land has been on-sold by Main Roads WA.

### **Area 8 – Great Eastern Highway**



**Location:** Great Eastern Highway in Ascot and Redcliffe, between Moreing Street and Aurum Street.

**Mapping Amendment Description:** Rezoning a portion of the Great Eastern Highway road reserve in Ascot and Redcliffe from 'Urban' to 'Primary Regional Roads'.

**Justification:** To appropriately align the Primary Regional Roads reservation to encompass the Great Eastern Highway road reserve.

### **Area 9 – Great Eastern Highway**

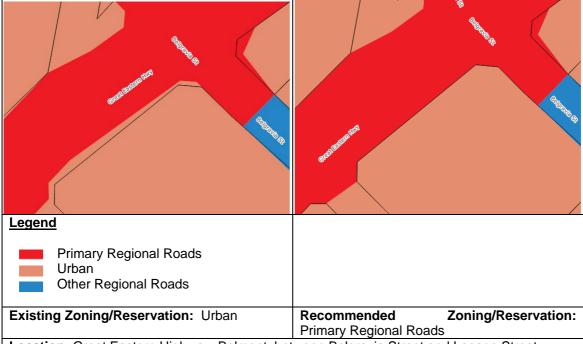


Location: Great Eastern Highway, Ascot, between Kalgoorlie Street and Resolution Drive.

**Mapping Amendment Description:** Rezoning a portion of the Great Eastern Highway road reserve in Ascot from 'Urban' to 'Primary Regional Roads'.

**Justification:** To appropriately align the Primary Regional Roads reservation to encompass the Great Eastern Highway road reserve.

Area 10 - Great Eastern Highway



Location: Great Eastern Highway, Belmont, between Belgravia Street and Lapage Street.

**Mapping Amendment Description:** Rezoning a portion of the Great Eastern Highway road reserve in Belmont from 'Urban' to 'Primary Regional Roads'.

**Justification:** To appropriately align the Primary Regional Roads reservation to encompass the Great Eastern Highway road reserve.



# Ordinary Council Meeting 26/02/19

Item 12.5 refers

**Attachment 14** 

Revised Local Planning Policy No.12 – Advertisement Signs



# **CITY OF BELMONT**

# **Advertisement Signs**

Date of Publication: Draft December 2018

## **Local Planning Policy No. 12**

Pursuant to Schedule 2, Part 2, Clause 4 (Deemed Provisions) of the Planning and Development (Local Planning Schemes) Regulations 2015.

### 1. Policy Basis

This Local Planning Policy provides guidance on the assessment of advertisement signs within the City of Belmont. This Policy outlines when advertisement signs require development approval, and the objectives and standards against which the City will assess applications for advertisement signs.

### 2. Policy Application

- 2.1 This Policy applies to all advertisement signs visible from the public realm within the City of Belmont Local Government Area.
- 2.2 This Policy exempts advertisement signs from requiring development approval where the advertisement sign complies with the criteria listed in Column A of Table 1 and the Development Requirements listed in Part 6 of this Policy.<sup>1</sup>
- 2.3 Advertisement signs that fully comply with the criteria listed in Column B of Table 1 and the Development Requirements listed in Part 6 of this Policy are deemed acceptable and cannot be refused on that basis.
- 2.4 Advertisement signs that do not comply with the criteria listed in Column B of Table 1, the Development Requirements listed in Part 6 of this Policy, and the objectives outlined in Part 3 of this Policy are deemed unacceptable and will not be supported unless exceptional circumstances exist.
- 2.5 The exemption afford by Clause 2.2 of this Policy does not apply where the advertisement sign:
  - (a) is located on land reserved under the Metropolitan Region Scheme or on a lot abutting land reserved under the Metropolitan Region Scheme;
  - (b) does not comply with the Development Requirements listed in Part 6 of this Policy;
  - (c) is illuminated or contains an electronic display screen and/or panel components;
  - (d) is inconsistent with any relevant Design Guidelines or other Local Planning Policy; and/or

The exemptions afforded by Clause 2.2 of this Policy are to be read in conjunction with Schedule 5 of the City of Belmont Local Planning Scheme No. 15.



215 Wright Street, Cloverdale 6105 (Locked Bag 379, Cloverdale 6985)

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- (e) is located in a place that is included on a Heritage List or on land located within a Heritage Area designated under the Scheme.
- 2.6 If a particular advertisement sign is not listed or defined, it shall be assessed on its individual merits in accordance with the objectives outlined in Part 3 of this Policy and the City of Belmont Local Planning Scheme No. 15.

### 3. Policy Objectives

The objectives of this Policy are to:

- 3.1 Ensure that advertisement signs are appropriate for their location, relate to the land and/or buildings for which they are placed, and do not adversely impact on the amenity of the surrounding area.
- 3.2 Ensure advertising signage is of a scale appropriate to buildings, lot size and lot frontage/s of the site relevant to the application.
- 3.3 Ensure that advertisement signs only advertise services offered and/or products produced, sold, and/or manufactured on the land or building/s related to the approved use/s taking place.
- 3.4 To ensure advertisement signs do not pose an unnecessary risk to the safety of people and vehicles by virtue of their location, design, use and function.
- 3.5 To ensure that advertising signs are simple, clear, easy to read and maintained to a high standard.
- 3.6 Protect the cultural heritage significance of particular places and/or areas.
- 3.7 To provide for the rationalisation of signage on properties with multiple advertising requirements.

### 4. Policy Definitions

The definitions of various advertisement signs is contained within Table 1.

The meaning of specific words and expressions used in this Policy are given below:

<u>Advertisement Sign</u>: Has the same meaning as an 'Advertisement' as defined in the *Planning and Development (Local Planning Schemes) Regulations 2015* – Schedule 2 – Deemed Provisions.

<u>Electronic Display Screen/Panel:</u> Means an advertisement sign or a portion of an advertisement sign that displays an electronic image or video, which may or may not include text.

<u>Façade:</u> Has the same meaning as 'Façade' as defined in the City of Belmont Local Planning Scheme No. 15.

<u>Heritage Area</u>: Has the same meaning as 'Heritage Area' as defined in the *Planning and Development (Local Planning Schemes) Regulations 2015* – Schedule 2 – Deemed Provisions.

<u>Heritage Place</u>: Has the same meaning as 'Heritage Place' as defined in section 3(1) of the Heritage of Western Australia Act 1990.

<u>Third Party Signage:</u> Means any advertisement sign advertising services and products unrelated to the subject site.

### 5. Accompanying Information

An application for development approval for an advertisement sign must be accompanied by the required information listed by Clauses 62(1), 63(1) and 63(3) of the *Planning and Development (Local Planning Schemes) Regulations 2015* – Schedule 2 – Deemed Provisions.

Applications for development approval which are not accompanied by the necessary accompanying information will <u>not</u> be accepted for assessment and processing.

All applications for Electronic Display Screen/Panel Signs are required to be accompanied with an electronic display screen/panel lighting impact report from a qualified lighting engineer that specifies the maximum daytime, dawn/dusk, and night time brightness relative to the surrounding development and land uses and the dwell/transition time.

All applications for advertisement sings within a Heritage Area/Place are required to be accompanied with a heritage assessment prepared by a qualified practitioner that specifies the impact of the sign on the heritage of the area.

### 6. <u>Development Requirements</u>

### 6.1 General Requirements

- 6.1.1 Advertisement signs shall only advertise services and products available on the premises to which it relates. Third party advertising is not permitted, except where it complies with Clause 6.5.1 of this Policy.
- 6.1.2 Advertisement signs must be contained wholly within the property boundaries of a lot and shall not overhang or encroach into any reserve (including road reserve).
- 6.1.3 Advertisement signs that will or are likely to, cause interference with or be hazardous to vehicular traffic and pedestrians, are prohibited.
- 6.1.4 Advertisement signs which contain offensive or misleading material are prohibited.
- 6.1.5 Street numbering is to be incorporated into advertisement signage, which is clearly visible from the street.
- 6.1.6 Development approval is not required in the event that the content of an approved advertisement sign is proposed to be changed, subject to the dimensions, location and structure remaining unchanged.
- 6.1.7 Development within the Mixed Business Zone (Belmont Business Park) is encouraged to incorporate the 'Belmont Business Park' branding.

### 6.2 Illuminated and Electronic Display Screen/Panel Signs

- 6.2.1 All Electronic Display Screen/Panel signs shall:
  - (a) be restricted to static images that are:
    - (i) externally illuminated during hours of darkness; or
    - (ii) displayed electronically, i.e. in a digital format.
  - (b) have a minimum dwell time duration of 10 seconds;
  - (c) not contain a transition time from one display to another that exceeds 0.1 seconds; and
  - (d) No display transitional effects such as fly-in, fade-out and scrolling.
- 6.2.2 Illuminated advertising signs, including electronic display screens/panels, abutting any road must:
  - (a) use a low level of illumination, not exceeding 300cd/<sup>m2</sup>, not flash, pulsate, or chase, and not cause a nuisance, by way of light spillage, to light received to the abutting sites to a maximum of 50 lux;
  - (b) not contain fluorescent, reflective or retro reflective colours or materials:
  - (c) not interfere with or be likely to be confused with traffic control signals;
  - (d) screen any electrical cables from the public realm and be maintained in a tidy manner; and
  - (e) not be located in Residential areas.

### 6.3 Signage Strategy

- 6.3.1 A Signage Strategy outlining the type, size, and location of all existing and proposed advertisement signs will be required for developments with multiple tenancies on a site.
- 6.3.2 A Signage Strategy (example illustrated in Appendix 1) required by Clause 6.3.1 of this Policy shall include the following information:
  - (a) a scaled site plan, illustrating the location of any existing and proposed signage;
  - (b) elevation details illustrating the location and dimensions of any existing and proposed/future signage;
  - (c) details of any illumination if signage is proposed to be illuminated;
  - (d) evidence that all tenancies have equitable access to signage locations and

- (e) other plans and information that the City may reasonably require to enable the signage strategy to be assessed and determined.<sup>2</sup>
- 6.3.3 All advertisement signs shall comply with the approved Signage Strategy, unless otherwise approved by the City under a separate development approval.
- 6.3.4 Further development approval is not required where an advertisement sign complies with an approved signage strategy.

### 6.4 Heritage Places

- 6.4.1 Any original and early signage (including remnants) that contribute to the cultural heritage significance of a heritage place shall be preserved.
- 6.4.2 The size, scale, location, materials and colours of any proposed advertisement sign on a heritage place must respect the heritage value of the site.
- 6.4.3 Advertisement signs shall not impact on the heritage value/significance of an area.

### 6.5 Third Party Advertisement Signs

- 6.5.1 Council may allow third party advertisement signs, including signage associated with a special event, to be displayed on a local reserve (excluding road reserve) where:
  - (a) The signage is only displayed for a maximum of four weeks leading up to a special event and is removed following the final day of the event;
  - (b) The signage is incidental to the special event and/or sporting/community activity.
  - (c) The Applicant and Landowner obtain development approval for the proposed advertisement signage from the City of Belmont.

Third party advertising from sponsors associated with sporting and/or community activities on site will be considered on a case-by-case basis.

### 6.6 Advertisement Signs required by City of Belmont and Public Authorities

6.6.1 Advertisement signs required by the City of Belmont and/or a public authority are exempt from requiring development approval.

 $<sup>^{2}</sup>$  All Signage Strategies shall reflect the information outlined in Appendix 1.

# TABLE 1 – ADVERTISEMENT SIGN REQUIREMENTS

SIGN TYPE	COLUMN A - EXEMPTED ADVERTISEMENTS	COLUMN B - DEEMED TO COMPLY
Signs on Buildings		
i. Awning Signs (below)	N/A - All Awning Signs require development approval.	Awning Signs (below) shall:
An advertisement sign fixed to or suspended from the underside of a verandah, balcony or awning.		a) achieve a minimum height clearance of 2.7m from the natural ground level;
		b) not exceed a depth of 500mm;
		c) not exceed an area of 4m <sup>2</sup> ;
		d) not be within 3m of another such sign attached to the underside of the same verandah;
		e) not project or overhang beyond the boundaries of the lot;
		f) consist of one sign per tenancy; and
		g) only be located near the entrance to a tenancy.
ii. Awning Signs (above)	N/A – All Awning Signs require development	Awning Signs (above) shall:
An advertisement sign located above the outer fascia of a		a) not exceed 400mm in height;
verandan, balcony or awning.		b) not exceed 500mm in depth;
		c) not project beyond the outer metal frame or the surround of the fascia of the building to which it is attached;
		d) not project or overhang beyond the boundaries of the lot;

SIGN TYPE	COLUMN A – EXEMPTED ADVERTISEMENTS	COLUMN B - DEEMED TO COMPLY
		e) consist of one sign per tenancy; and
		f) only be located near the entrance to a tenancy.
iii. Projecting Signs	N/A – All Projecting Signs require	All Projecting Signs shall:
An advertisement sign which projects more than 300mm out from a wall of a building, below the eaves or ceiling height.	developinent approval.	a) be limited to a maximum of one sign per tenancy;
		b) have a minimum clearance of 2.7m from the natural ground level;
		c) not project out more than 1m from the wall;
		d) not exceed 4m <sup>2</sup> in area;
		e) not be placed within 2m of either end of the wall to which they are attached; and
		f) not project above the top of the wall to which they are attached.
iv. Wall Signs	N/A - All Wall Signs require development	Wall Signs shall:
An advertisement sign which is affixed to the external part of a wall of a building, a gable end, or a building façade and no part of which is above the roofline of the building.		<ul> <li>a) not extend laterally beyond either end</li> <li>of the wall; and</li> </ul>

SIGN TYPE	COLUMN A – EXEMPTED ADVERTISEMENTS	COLUMN B - DEEMED TO COMPLY
CHIROPRACTIC		b) not cover more than 10% of the façade for each tenancy within a building and/or development site per street frontage; or c) where there is an approved signage strategy, not cover more than 25% per property frontage.
v. Window Signs	Window Signs that:	Window Signs shall:
An advertisement sign which is painted or affixed to either the interior or exterior surface of the glazed area of the window.	<ul> <li>a) do not cover more than 15% of the total window area per tenancy;</li> </ul>	a) not cover more than 50% of the total window area per tenancy;
*Note: This includes all areas of non-advertising window coverings which block views into or out of the window of the building to which they are affixed.	<ul><li>b) not obstruct views onto the public realm; and</li><li>c) not detract from the streetscape.</li></ul>	b) not obstruct views onto the public realm; and c) not detract from the streetscape.

COLUMN B – DEEMED TO COMPLY		Roof Signs are not permitted.
COLUMN A – EXEMPTED ADVERTISEMENTS		Roof Signs are not permitted.
SIGN TYPE	SELMERIANE . SACH KARE . BACKER . BACKE	An advertisement sign erected on the roof of a building.  Roof Sign

SIGN TYPE	COLUMN A – EXEMPTED ADVERTISEMENTS	COLUMN B – DEEMED TO COMPLY
vii. Balloon Sign	N/A – All Balloon Signs require development	Balloon Signs shall:
An inflatable advertisement sign.	appiovai.	a) not exceed 7m in diameter;
		b) not exceed 9m in height;
		c) not be displayed for more than 14 days in aggregate for any one calendar year; and
		d) be securely attached.
Freestanding Signs		
viii. Billboards	Billboard Signs are not permitted.	Billboard Signs are not permitted.
An advertisement sign fixed to a free-standing structure or building which may include LED components, however is not classified as a pylon or wall sign.		

SIGN TYPE	COLUMN A – EXEMPTED ADVERTISEMENTS	COLUMN B - DEEMED TO COMPLY
ix. Property Transaction, Display Home, and Building Construction Signs	Property Transaction, Display Home, and Building Construction Signs pertaining to Single Houses. Grouped and Multiple	Property Transaction, Display Home, and Building Construction Signs shall:
An advertisement sign advertising the display, sale, construction	that:	a) be a maximum 1m <sup>2</sup> in area per 3m of
or lease of buildings, land and/or development.	a) do not exceed an area of 2m <sup>2</sup> ;	4m <sup>2</sup> ;
	b) are limited to a maximum of one sign per street frontage for each property or tenancy; and	b) not exceed a maximum height of 2m, inclusive of supporting posts/structures; and
FOR	c) do not exceed a maximum height of 1.5m, inclusive of supporting posts/structures.	c) only be approved for a period not exceeding twelve months at any one time.
	Property Transaction and Building Construction Signs for shopping centres and commercial developments that:	d) be limited to one sign per street frontage for each property or tenancy.
	a) do not exceed an area of 5m <sup>2</sup> ;	
LOTS	b) are limited to a maximum of one sign per street frontage for each property or tenancy; and	
The state of the s	c) do not exceed a maximum height of 3m, inclusive of supporting posts/structures.	

SIGN TYPE	COLUMN A – EXEMPTED ADVERTISEMENTS	COLUMN B – DEEMED TO COMPLY
x. Flag Pole	Flag Poles that:	Flag Poles shall:
An advertisement sign consisting of a banner that is made from a piece of material which is attached to a pole.	<ul> <li>are limited to a maximum of one per property;</li> <li>b) do not exceed a maximum height of emerged</li> </ul>	a) be limited to a maximum of three per property, with the exception of residential properties which are limited to a maximum of one per property.
	c) the area of the flag does not exceed $2m^2$ .	b) do not exceed a maximum height of 6m; and
		c) not exceed 4m <sup>2</sup> in area per flag.
xi. Pylon Signs	N/A – All Pylon Signs require development	Pylon Signs shall:
An advertisement sign which is affixed to the ground having one or more supports where the overall height (inclusive of any		a) have a minimum clearance of 2.7m from the natural ground level;
supports) is greater than the sign's nortzontal dimension.		b) be restricted to a maximum of one Pylon Sign per street frontage, where the property has more than one street frontage;
		c) not exceed 2.5m measured horizontally across the face of the sign;

SIGN TYPE	COLUMN A – EXEMPTED ADVERTISEMENTS	COLUMN B – DEEMED TO COMPLY
		d) for all advertisement signage to be incorporated into one Pylon Sign when proposed to be erected on a lot with multiple tenancies/units. Therefore, each unit/tenancy on a lot shall be provided with one infill panel;
		e) not be permitted if a Monolith Sign has been approved or erected on a property; and
٥٥		f) not be more than 6m above natural ground level (including supporting structures), except where:
		g) multiple tenancies/units are located on a lot, then the height can be increased to 7m above natural ground level (including supporting structures).
		h) properties are immediately abutting Great Eastern Highway, then the height can be increased to 8m above natural ground level (including supporting structures).
xii. Monolith Signs	N/A – All Monolith Signs require development approval.	Monolith Signs shall:
An advertisement sign comprising of a solid, ground mounted structure, where the vertical dimension exceeds the horizontal dimension, but does not include a Pylon Sign or a Directional		a) not be more than 6m above natural ground level (including supporting structures), except where:
Sign.		i. multiple tenancies/units are located on a lot, then the height can be increased to 7m above natural ground

SIGN TYPE	COLUMN A – EXEMPTED ADVERTISEMENTS	COLUMN B – DEEMED TO COMPLY
		level.
		ii. properties are immediately abutting Great Eastern Highway, then the height can be increased to 8m above natural ground level.
100		b) have a maximum area of 15m² for a 6m high sign, 18m² for a 7m high sign and/or 20m² for a 8m high sign;
		c) be restricted to a maximum of one per street frontage, where the property has more than one street frontage;
		allow for all advertisement signage to be incorporated into one Monolith Sign when proposed to be erected on a lot with multiple tenancies/units. Therefore, each unit/tenancy on a lot shall be provided with one infill panel; and
		e) not be permitted where a Pylon Sign has been approved or erected on a property.
Portable / Temporary Signage		
xiii. Ground Based Signs	Ground Based Signs that:	Ground Based Signs shall:
An advertisement sign not permanently attached to the ground including, but not limited to a sandwich board sign which consists	a) contain a maximum area of 0.5m² each side;	a) provide a clearance of a minimum of 1.8m to a footpath or pedestrian walkway.
by hinges or other means, which is no higher than 1.2m above natural ground level.	b) are only displayed during the operating hours of the business to	b) be secured to the satisfaction of the

	COLUMN A - EXEMPTED ADVERTISEMENTS	COLUMN B – DEEMED TO COMPLY
()	which the sign relates;	City to ensure they do not move/blow over:
	c) provide a clearance of a minimum of 1.8m to a footpath or pedestrian walkway;	c) be placed a maximum distance of 10m from the building to which the sign relates:
ios phospin	d) are limited to a maximum of one sign per tenancy on a lot;	d) only be displayed during the
7_	e) are not located more than 5m from the building to which the sign relates;	
	f) do not exceed a maximum height of	e) be ilmited to a maximum of one sign per tenancy on a lot;
	level (including sures).	f) be a maximum of $1m^2$ in area per side; and
		g) do not exceed a maximum height of 1.2m measured from natural ground level (including supporting structures).
xiv. Bunting/Tethered/Flag Signs	Bunting/Tethered/Flag Signs are not	Bunting/Tethered/Flag Signs are not
A single or group of flags or material that may be free standing or attached to a rope or line stretched between two or more points. The term includes inflatables (excluding balloons), bunting, banners and kites.	בר היים היים היים היים היים היים היים היי	

SIGN TYPE	COLUMN A – EXEMPTED ADVERTISEMENTS	COLUMN B - DEEMED TO COMPLY
xv. Trailer Mounted Signs	Trailer Mounted Signs are not permitted,	Trailer Mounted Signs are not permitted,
An object which is displayed for the purposes of advertisement (including a variable message sign); or an advertisement sign which is attached to or placed on a vehicle (car, truck, boat, trailer, caravan, machinery, whether moveable or not).		except where used by public authorities.

COLUMN B – DEEMED TO COMPLY	
COLUMN A – EXEMPTED ADVERTISEMENTS	
SIGN TYPE	BUY IT PARKIT

SIGN TYPE	COLUMN A – EXEMPTED ADVERTISEMENTS	COLUMN B - DEEMED TO COMPLY
xvi. Fence Sign	All Fence Signs require development	Fence Signs shall:
An advertisement sign attached to a fence.	/	a) be limited to a maximum of one sign per street frontage on any one lot:
DISTRIBUTE STRING OF		b) be securely attached;
* Taphare * Taphare * Taphare * Truch Valves Wirth Wirth Wir command		<ul><li>c) sit flush on the fence to which it is attached;</li></ul>
		d) not contain any sharp or pointed edges below a height of 2.7m;
		d) not cover more than 30% of the total area of the fence; and
		e) not impact on views to the street and/ or building.
xvii. Tower Sign	Tower Signs are not permitted.	Tower Signs are not permitted.
A sign affixed to, or placed on an open structural mast or tower.		

ENTS COLUMN B - DEEMED TO COMPLY	
COLUMN A – EXEMPTED ADVERTISEMENTS	
SIGN TYPE	

Scale 1:300 **Building Elevations** 37.7m 23.3m 59m E1 - Elevation 1 E2 - Elevation 2 E5 - Elevation 5 E4 - Elevation 4 Wall Sign Dimensions E3 - Elevation 3 9.8m 5.2 m Location of Pylon Sign 4.6 m 3.5 m ы m 7.2 Street Facing Elevation of Pylon Sign 2.4m 8m E2 3.3m Site Plan z<del>(</del> 0.6m

Appendix 1 – Signage Strategy Example

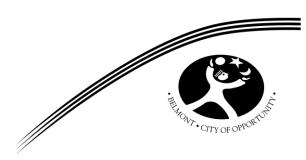


# Ordinary Council Meeting 26/02/19

Item 12.5 refers

**Attachment 15** 

# Key Assessment Provision Changes from Existing Policy



### ATTACHMENT 2 - KEY CHANGES TO DEVELOPMENT ASSESSMENT CRITERIA

Sign Type	Key Changes Proposed	Justification
Awning Signs (below)	<ul> <li>New advertisement sign classification.</li> <li>New assessment provisions pertaining to:         <ul> <li>ground clearance;</li> <li>depth;</li> </ul> </li> </ul>	Awning Signs (below) are not currently outlined in LPP 12 and their inclusion is considered necessary. The new assessment provisions are supported for the following reasons:  The ground clearance of a minimum of 2.7m was deemed
	o area; o number of signs; and o location requirements.	necessary for safety reasons, to ensure that signs do not conflict with pedestrian maneuverability.  - Limiting the number of signs was required to avoid proliferation of signage.  - Locational criteria have been introduced to ensure that signs are located in a prominent location near the entrance to tenancies.  - Restrictions on the size of Awning Signs have been introduced to ensure that they do not become excessive and impact on the amenity of the locality.
Awning (above) Signs	- New advertisement sign classification New assessment provisions pertaining to:  o height; o depth; o number of signs; and o location requirements.	Awning Signs (above) are not currently outlined in LPP 12 and their inclusion is considered necessary. The new assessment provisions are supported for the following reasons:  The height and depth of the sign was required to be restricted to ensure the signage did not dominate the overall façade of the building.  Limiting the number of signs was required to avoid the proliferation of signage.  Locational requirements were implemented to ensure signs are located in a prominent location near the entrance to tenancies. In addition, the locational requirements ensure signs do not project beyond the frame of the fascia of the building to which it is attached.
Projecting Signs	- Remove provisions for Projecting Signs attached to the fascia of a verandah and Projecting Signs attached to the underside of a verandah.	Provisions for Projecting Signs attached to the fascia and the underside of a verandah are proposed to be removed from the revised LPP 12, as these signs are now classified as Awning Signs.
Wall Signs	- Removing provision limiting wall signs to a maximum area of 8m <sup>2</sup> .	The existing provision restricting Wall Signs to a maximum area of 8m² per wall

Sign Type	Key Changes Proposed	Justification
	<ul> <li>Replacing area provision with "wall signs shall not cover more than 10% of the façade for each tenancy or 25% per property frontage where there is an approved signage strategy."</li> <li>Removing door clearance from ground level provision.</li> </ul>	face is proposed to be replaced with a standard based on maximum percentage of the façade of each tenancy. The reason for this is that the existing provision is often varied to allow for buildings to have a level of Wall Signage that is consistent with the scale of the building façade. It is proposed that the revised provision will allow for Wall Signs that are a proportionate size to the façade of the building.  It was also considered necessary to remove the 2.7m clearance requirement for Wall signs located directly over door openings. If a sign is located above a door opening, it is located sufficiently high enough to not pose a safety risk.
Window Signs	<ul> <li>Removing existing 10m² in area provision.</li> <li>Retain provision outlining that window signage shall not cover more than 50% of the total window area per tenancy.</li> <li>Exempting Window Signs from requiring Development Approval where they do not cover more than 15% of the total window area, per tenancy.</li> <li>New assessment provisions pertaining to:         <ul> <li>impact on streetscape; and</li> <li>obstruction of views.</li> </ul> </li> </ul>	The existing provision restricting Window Signs to a maximum area of 10m² is proposed to be removed and replaced with a standard based on maximum percentage of window area. This change will ensure that signage is an appropriate level in proportion to the size of the window of the building.  It was considered appropriate to exempt a small amount of Window Signs (i.e. 15%) from requiring development approval. The reason for this is that Window Signs are often interchangeable and it would be onerous to require approval for such signage.  The revised Policy includes provisions stipulating that Window Signs must not obstruct views to the public realm or detract from the streetscape. The purpose of this is to ensure that they do not detract from the streetscape or negatively impact on the amenity of the locality, and that they still allow adequate passive surveillance to the street and the building.
Roof Signs	- No change, Roof Signs are not permitted.	Roof Signs are not currently permitted in LPP 12 and LPS 15. Given that Roof Signs have the potential to detrimentally impact on the amenity of the locality, this restriction remains unchanged under the revised Policy.
Balloon Signs	- Revising the provision requiring an applicant to submit evidence that the balloon sign is of a structurally sound design with a provision simply stating that the Balloon Sign should be securely attached.	The structural integrity of any Balloon Sign will be assessed through a Building Permit application, and therefore it is not appropriate or necessary to require this technical assessment through the development approval process.
Billboards	<ul><li>New advertisement sign classification.</li><li>Billboard Signs are not permitted.</li></ul>	Billboards are not currently included in LPP 12 and their inclusion is considered necessary.
		The revised LPP 12 does not permit

Sign Type	Key Changes Proposed	Justification
		Billboards on the basis that they have the potential to detrimentally impact on the amenity of the locality due to their size and bulk.
Property Transaction, Display Home and Building Construction Signs	<ul> <li>New advertisement sign classifications (Display Home and Building Construction Signs).</li> <li>New assessment provisions pertaining to:         <ul> <li>area;</li> <li>height; and</li> <li>maximum development approval period.</li> </ul> </li> <li>Exempting Property Transaction, Display Home and Building Construction Signs pertaining to Single Houses, Grouped and Multiple Dwellings that:         <ul> <li>do not exceed an area of 2m²;</li> <li>are limited to a maximum of one sign per street frontage for each property or tenancy; and</li> <li>do not exceed a maximum height of 1.5m, inclusive of supporting posts/structures.</li> </ul> </li> <li>Exempting Property Transaction and Building Construction Signs for shopping centres and commercial developments that:         <ul> <li>do not exceed 5m² in area;</li> <li>are limited to a maximum of one sign per street frontage for each property or tenancy; and</li> </ul> </li> <li>do not exceed a maximum height of 3m, inclusive of supporting posts/structures.</li> </ul>	LPS 15 currently exempts Property Transaction, Display Home and Building Construction Signs from requiring Development Approval where certain criteria are not exceeded. The revised LPP 12 seeks to maintain this exemption, but also include assessment criteria for signs that fall outside of the exemption criteria. Both the exemption and assessment criteria are premised on the maximum area, height and quantity of signs.
Flag Poles	<ul> <li>Increase maximum number of Flag Poles permitted per property to three with the exception of residential properties.</li> <li>Removing provision not permitting Flag Poles to be located within 5m of any sign located on the same lot.</li> <li>Exempting Flag Poles from requiring Development Approval that:         <ul> <li>are limited to a maximum of one per property;</li> </ul> </li> </ul>	In non-residential areas, it was considered appropriate to increase the maximum number of Flag Poles permitted per property from two to three. This would allow for businesses to display a combination of the following:  - Australian Flag;  - Indigenous Australian Flag;  - Western Australian Flag; or  - A company flag.

Sign Type	Key Changes Proposed	Justification
	<ul> <li>do not exceed a maximum height of 6m; and</li> <li>the area of the flag does not exceed 2m².</li> </ul>	In terms of Flag Poles in residential areas, it is considered appropriate to reduce the permitted amount of Flag Poles from two to one. This would be better in-keeping with a residential context and it is considered that anything greater would be excessive.
		It was also considered appropriate to exempt Flag Poles from requiring Development Approval where only one Flag Pole is proposed, providing it does not exceed the size requirements.
Pylon Signs	<ul> <li>Removing provisions relating to where a Monolith Sign has been approved, a Pylon Sign may also be considered.</li> <li>New and amended provisions relating to:         <ul> <li>maximum height and width of sign;</li> <li>requiring advertisement signage to be incorporated into one sign when proposed to be located on a lot with multiple tenancies/units.</li> <li>maximum number of signs; and</li> <li>not being permitted where a monolith sign has been approved or erected.</li> </ul> </li> </ul>	It is considered that permitting both a Pylon Sign and Monolith Sign on a property would be excessive, and therefore it is proposed to no longer be permitted by the revised Policy.  The introduction of new, or the amendment of existing provisions relating to the design of Pylon Signs have been included/undertaken for the following reasons:  - Including height and width provisions ensure that the scale of any Pylon Signs proposed do not detrimentally impact on the amenity of the locality.  - Greater flexibility is required in relation to the height of Pylon Signs that serve multiple tenancies/uses on a lot and/or are located on higher order roads
		where greater exposure is necessary to attract passing vehicles.  - Greater flexibility is required in relation to the number of Pylon Signs permitted for properties with multiple street frontages (i.e. one per street frontage, as opposed to one per property).
Monolith Signs	<ul> <li>Removing provisions relating to where a Pylon Sign has been approved, a Monolith Sign may also be considered.</li> <li>New and amended provisions relating to:</li> </ul>	For the same reasons mentioned above, it is considered that permitting both a Pylon Sign and Monolith Sign on a property would be excessive, and therefore it is proposed to no longer be permitted by the revised Policy.
	<ul> <li>maximum height of sign;</li> <li>maximum area of sign;</li> <li>number of signs;</li> <li>requiring advertisement signage to be incorporated into one sign</li> </ul>	The introduction of new, or the amendment of existing provisions relating to the design of Monolith Signs have been included/undertaken for the following reasons:  Greater flexibility is required in relation to the height of Monolith
	when proposed to be erected on a lot with multiple tenancies/units.	Signs that serve multiple tenancies/uses on a lot and/or

Sign Type	Key Changes Proposed	Justification
	o not being permitted where a Pylon Sign has been approved or erected on a lot.	are located on higher order roads where greater exposure is necessary to attract passing vehicles.
		- The maximum allowable area for a Monolith Sign has been increased from 6m² to better align with the maximum height permitted by the Policy i.e. 15m² for a standard 6m high sign, 18m² for a 7m high sign (multiple tenancies) and 20m² for an 8m high sign (Great Eastern Highway).
		- Greater flexibility is required in relation to the number of Monolith Signs permitted for properties with multiple street frontages (i.e. one per street frontage, as opposed to one per property).
Ground Based Signs	Based Signs to have no moving parts once in place.	The revised provisions for Ground Based Signs seek to address minimum clearance from a footpath, secureness of the sign, maximum distance from a building and
	- Remove provisions pertaining to Ground Based Signs which, in the opinion of the Council are not portable.	overall height. It is anticipated that these provisions will ensure that Ground Based Signs can be sufficiently visible whilst also not impacting on the amenity of the
	- New provisions relating to:	locality through cluttering.
	o minimum clearance from footpaths;	The revised Policy seeks to exempt small- scale Ground Based Signs from requiring development approval as they are unlikely
	o secureness of the sign;	to detrimentally impact on the amenity of the locality.
	<ul> <li>maximum distance sign can be located from associated building;</li> </ul>	
	o allowing a maximum area of 1m <sup>2</sup> per side; and	
	o maximum height of 1.2m.	
	- Exempting Ground Based Signs from requiring development approval that:	
	o contain a maximum area of 0.5m <sup>2</sup> each side;	
	<ul> <li>are only displayed during the operating hours of the business to which the sign relates;</li> </ul>	
	o provide a minimum clearance of 1.8m to a footpath or pedestrian walkway;	
	o are limited to a maximum of one sign per tenancy on a lot;	
	o are not located more than 5m	

Sign Type	Key Changes Proposed	Justification
	from the building to which the sign relates; and  o do not exceed a maximum height of 0.75m measured from natural ground level (including supporting structures).	
Bunting/Tethered/ Flag Signs	- No changes, Bunting/ Tethered/ Flag Signs are not permitted.	Bunting/Tethered/Flag Signs are not currently permitted under the existing LPP 12. Given that Bunting/Tethered/Flag Signs have the potential to detrimentally impact on the amenity of the locality, this restriction remains unchanged under the revised Policy.
Trailer Mounted Signs	Trailer Mounted Signs are not permitted, except where used by public authorities.	Trailer Mounted Signs are not currently included in LPP 12. Due to Trailer Mounted Signs having the potential to detrimentally impact on the amenity of the locality, cause nuisance, and distract drivers, they will not be permitted. Notwithstanding, due to their ability to draw attention, it is considered appropriate to permit their use by a public authority to advise the community of important messages.
Fence Signs	<ul> <li>New advertisement sign classification.</li> <li>New assessment provisions pertaining to: <ul> <li>number of signs;</li> <li>obstruction of views;</li> <li>area;</li> <li>material; and</li> <li>attachment to fence.</li> </ul> </li> <li>Exempting Fence Signs from requiring development approval that are advising of a potential safety hazard or are required by legislation.</li> </ul>	Fence Signs are not currently included in LPP 12 and their inclusion is considered necessary. The new assessment provisions are supported for the following reasons:  - Limiting the number and area of signs will ensure that they do not have the potential to detrimentally impact on the amenity of the locality.  - Fence Signs will not be permitted to obstruct views to the street and or into the building to ensure passive surveillance is able to occur.  - Provisions requiring Fence Signs to be securely attached flush to the fence, and without sharp or pointed edges aim to protect public safety.  The exemption of Fence Signs that advise of a potential safety hazard or that are required by legislation from requiring development approval will expedite their assembly and erection.
Tower Signs	<ul> <li>New advertisement sign classification.</li> <li>Tower Signs are not permitted.</li> </ul>	Tower Signs are not currently included in LPP 12. Due to Tower Signs having the potential to detrimentally impact on the amenity of the locality by virtue of their size and scale, they will not be permitted.

Sign Type	Key Changes Proposed	Justification
Illuminated and Electronic Display Screen/Panel Signs	<ul> <li>New advertisement sign classification.</li> <li>Outlines assessment provisions pertaining to:         <ul> <li>dwell time;</li> </ul> </li> </ul>	Illuminated and Electronic Display Screen/Panel Signs are not currently outlined in LPP 12 and their inclusion is considered necessary. The new assessment provisions are supported for the following reasons:
	<ul> <li>transitional effects;</li> <li>location;</li> <li>illumination levels; and</li> <li>materials.</li> </ul>	<ul> <li>The minimum dwell time and transition time ensures that signage is not changed too frequently and is therefore does not unreasonably distract passing vehicle traffic.</li> <li>Transitional effects will not be permitted to ensure there is no distraction to passing vehicle traffic.</li> </ul>
		- Limitations on illumination levels ensures that signs are unable to be confused with traffic control signals and are not a distraction or nuisance to passing vehicle traffic.
		<ul> <li>A restriction on Illuminated and Electronic Display Screen/Panel Signs in Residential ensures that the amenity of residents is not impacted upon.</li> </ul>

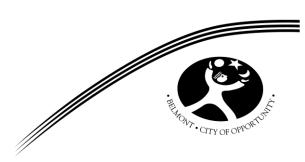


#### Ordinary Council Meeting 26/02/19

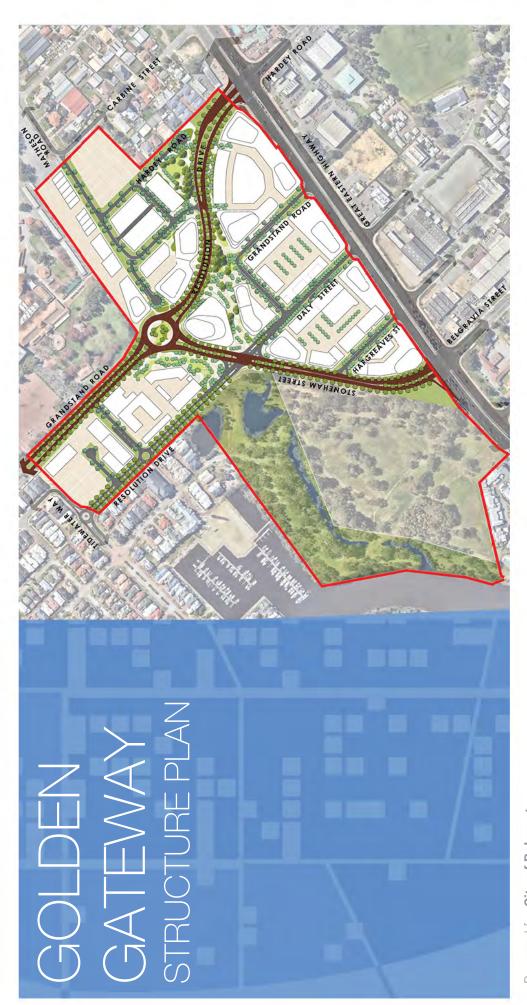
Item 12.6 refers

**Attachment 16** 

Draft Golden Gateway Local Structure Plan Report (Excluding Attachments)







Prepared for City of Belmont
Prepared by Taylor Burnell Barnett

# DOCUMENT HISTORY AND STATUS



## ENDORSEMENT

This Structure Plan is prepared under the provision of the City of Belmont Local Planning Scheme No. 15

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Signed for and on behalf of the Western Australian Planning Commission

an officer of the Commission duly authorised by the Commission pursuant to section 16 of the Planning and Development Act 2005 for that purpose, in the

presence of:

Date

...... Date of Expiry

### Golden Gateway | Structure Plan

by WAPC			
Date Approved by WAPC			
Amendment Type			
Summary of the Amendment			
Amendment No.			

TABLE OF AMENDMENTS

# EXECUTIVE SUMMARY

This Structure Plan is prepared to guide the subdivision and development of land contained within the inner edge of the line denoting the Structure Plan boundary on the Structure Plan map (hereafter referred to as 'Golden Gateway' or 'subject land').

The subject land is located:

- Within the municipality of the City of Belmont;
- Approximately 5 kilometres (km) north-east of the Perth Central Business District (CBD) 3km north of Belmont Forum and mixed business area and 5km north-east of Victoria Park entertainment precinct; and
  - Approximately 2.5km east of Graham Farmer Freeway and 2km west of Tonkin Highway.

The subject land encompasses a mix of uses comprising mixed business, retail (food and beverage), public uses associated with the Western Australian Turf Club (WATC), Ascot Racecourse and Ascot Kilns, Belmont Trust Land and Swan River environs. The remainder of the subject land is largely undeveloped and devoid of vegetation.

The development of the Belmont Trust Land and Ascot Kilns sites are subject to separate planning processes.

The Ascot Kilns site is owned by the Western Australian Planning Commission (WAPC) and is the subject of a draft Local Development Plan (LDP) and draft Local Planning Policy (LPP) that was considered for final approval by Council at its meeting of 12 December 2017. The draft LDP and LPP details the intended future planning vision for this site with regards to proposed land uses, built form, development standards and the retention of the majority of the heritage listed kilns and chimney structures.

The Belmont Trust Land is owned by the City of Belmont and managed by the 'Belmont Trust'. This land is not subject to any formal statutory planning processes at this stage and nor is there a specific timeline for the future planning of this land. The future consideration for this land is dependent upon the 'Belmont Trust'.

The Structure Plan proposes development of land for:

- Commercial uses;
- Retail uses;
- Residential purposes comprising a mix of low, medium and high residential densities;
- Public Open Space (POS) including foreshore reserve; and
- Access streets.

Item	Data	
Total area covered by the Structure Plan	23.9871 ha	1.2.3
Area of each land use proposed:  Residential  Mixed Use	<b>Hectares</b> 3.6961 ha 9.4742 ha	င်း
Estimated No. of Dwellings	3,000	3.3.1.1
Estimated Residential Site Density	283 Dwellings per site/ha	4.1.2.1
Estimated Population	5,400 persons	3.3.1.1
No. of High Schools	N/A	3.6
No. of Primary Schools	N/A	3.6
Estimated Commercial Floor Space	5,920m² nett lettable area	3.3.2.1
Estimated Retail Floor Space	1,200m² nett lettable area	3.3.2.2
Estimated area and percentage of Public Open Space given over to:  Local Parks	3.47% 0.6974 ha 2 parks	3.3.7
Estimated area of natural area (existing Parks and Recreation Reservation)	4.5556 ha	3.3.7

# TABLE OF CONTENTS

NO SON MADE ON THE NEW YORK ON			MOVEME	3.4.1 ROAD NETWORK AND TRAFFIC MANAGEMENT DEVICES		PEDESTRIAN AND CYCLING NETWORK	3.4.4 PUBLIC TRANSPORT	EK MANAGEMEN I	3.5.1 LOCAL WATER MANAGEMENT STRATEGY 67	GROUNDWATER MANAGEMENT	3.6 EDUCATION FACILITIES 68	3.7 EMPLOYMENT 68	3.8 INFRASTRUCTURE COORDINATION, SERVICING AND STAGING 68	WATER SUPPLY	3.8.2 WASTEWATER 69	FOWER SUPPLY GAS SUPPLY	TELECOMMUNICATIONS	3.8.6 WATER CORPORATION MAIN DRAIN	EMENTATION	3.9.1 LOCAL PLANNING POLICY 3.9.2 SCHEME AMENINMENT TO FACILITATE	4	3.9.3 SCHEME AMENDMENT TO 'NORMALISE'	4				TECHNICAL APPENDICES		APPENDIX A BUSHFIRE MANAGEMENT PLAN	APPENDIX B ENVIRONMENTAL ASSESSMENT REPORT	APPENDIX C MOVEMENT AND ACCESS STRATEGY APPENDIX D LOCAL WATER MANAGEMENT STRATEGY	APPENDIX E INFRASTRUCTURE ASSESSMENT REPORT	APPENDIX F PUBLIC REALM STRATEGY	
20	67	29	29	30	00000	0 0	30	30	3 2	33	31	31	31	31	33 8	33	33	33	33	8 8 4 4	34	35	35	35	35	32	36	42	44 44	) (	48	49	20 8	51 52
213 EALINA	۷ .	FOR		2.2.2 ACID SULFATE SOILS	2		2.3.1 GROUNDWATER 2.3.2 SURFACE WATER	2.4 BUSHFIRF HAZABD				2.6 EXISTING MOVEMENT NETWORK		2.6.2 INTERNAL ROADS		2.7 ROAD TRAFFIC NOISE	2.8 EXISTING INFRASTRUCTURE AND SERVICING	2.8.1 WATER SUPPLY		2.8.3 POWER SUPPLY 2.8.4 GAS SUPPLY		STRUCTURE PLAN	3.1 VISION AND OBJECTIVES			3.2 DESIGN PREPARATION	3.2.1 OPPORTUNITIES		HSI I	ולאולים יי	3.3.1 RESIDENTIAL 3.3.2 OTHER LAND USES	3.3.3 UNACCEPTABLE LAND USES 3.3.4 BUILDING HEIGHT		3.3.6 CAR PARKING 3.3.7 PUBLIC OPEN SPACE
		2	2	2	က	m	n	4	4 ro	LC.	) LC	000	10	0	0	0	_	_	_	_	2	8	7	7	7	_	0 -	- 4	4	9	V 00	0	0	തെ
PART ONE IMPRINATION	TANI ONE IMPLEMENTATION		2 OPERATION 2	3 STAGING	4 SUBDIVISION AND DEVELOPMENT REQUIREMENTS	4.1 LAND USE ZONES/RESERVES	MIXED USE (R-AC0)	RESIDENTIAL	4.1.3 PARKS AND RECREATION 4.1.4 LOCAL ROADS	OPMENT BEOUREMENTS	4.2.1 PRECINCT DEVELOPMENT REQUIREMENTS	GENERAL DEVELOPMENT REQUIREMENTS	5 OTHER REQUIREMENTS	5.1 SCHEME AMENDMENT 10	5.2 LOCAL PLANNING POLICY 10	5.2.1 CONTENT OF LOCAL PLANNING POLICY 10	5.3 INFRASTRUCTURE FUNDING STRATEGY	5.4 BUSHFIRE MANAGEMENT	5.5 ABORIGINAL HERITAGE	5.6 NOISE ATTENUATION	6 ADDITIONAL INFORMATION 12	PART TWO EXPLANATORY INFORMATION	1 PLANNING BACKGROUND 17	1.1 INTRODUCTION AND PURPOSE	1.2 LAND DESCRIPTION 17	1.2.1 LOCATION 17	LAND USE	NING FRAMEWORK	ZONING AND RESERVATIONS	PLANNING STRATEGIES	1.3.3 PLANNING POLICIES 27 1.3.4 PRE LODGEMENT CONSULTATION 28	2 SITE CONDITIONS AND CONSTRAINTS 29	21 ENVIRONMENTAL ASSETS AND CONSTRANTS 29	2.1.1 VEGETATION 2.1.2 FLORA

## PART ONE IMPLEMENTATION

# STRUCTURE PLAN AREA

This Structure Plan shall apply to the Golden Gateway Precinct, being the land contained within the inner edge of the line denoting the Structure Plan boundary on the Structure Plan map (**Plan 1**).

## OPERATION

This Structure Plan commences operation on the date it is approved by the Western Australian Planning Commission (WAPC).

## STAGING

The staging of subdivision and development will be influenced by the timing of major road realignments and upgrades, and land rationalisation. As most of the developable land is fragmented and privately owned, the actual timing and sequence of development will be subject to market demand and individual development intentions. Land within the northern section of the subject land is less constrained by land ownership, with the WA Turf Club (WATC) and WAPC owning the majority of this land and will be more conducive to early development.

Servicing infrastructure required to support future development of the subject land is either in place or can be relocated/provided to service the subject land and as such is not regarded as an impediment to staging.

Changes to the road network will be a key trigger to enable development throughout most of the precinct. In particular, works necessary prior to development commencement include modifications to Stoneham Street, Resolution Drive, Daly Street, Raconteur Drive and Matheson Road.

The delivery timeframe of the local centre will be largely determined by economic factors and the rate of population growth.

**Table 1** below provides an outline of the key triggers for enabling development within various parts of the Structure Plan area.

TABLE 1: STAGING TRIGGERS

	DEVELOPMENT AREAS COMMENT	Kilns site and No subdivision or development to adjoining WATC be approved until the planning Admin site framework is in effect.  Great Eastern Highway land	All land referred to establish new road alignments and 4 below and rationalise cadastral boundaries, prior to development of any land requiring access from those roads either directly or indirectly, or impacted by land assembly requirements.	Land north of Resolution Dve	Land fronting the Development may be permitted to occur prior to upgrades subject to contribution towards upgrade
1. Planning Framewol implemen Scheme R Structure approval 2. Modificati upgrades Grandstan Resolutiol and Stone and Stone Resolution roundabo Create lin reserve. 3. Extension Matheson connect te	STAGING TRIGGERS D	ork ntation - Rezoning,	ss of and Rd, on Dve neham St neham St / on Dve oout.	Extension of Matheson Rd to connect to Resolution Dve.	Realignment and upgrades of Hargreaves, Daly

# SUBDIVISION AND DEVELOPMENT REQUIREMENTS

This Structure Plan comprises the plans outlined below:

#### Plan 1 - Structure Plan Map

Outlines the zones, reserves and residential densities applicable within the Structure Plan area.

#### Plan 2 – Precinct Plan

Identifies development precincts within the Structure Plan area, for the purpose of defining specific development criteria.

### Plan 3 – Building Height Plan

Depicts the intended building heights within the Structure Plan area. All development should demonstrate compliance with the Building Height Plan.

# 1 LAND USE ZONES/RESERVES

The Structure Plan Map (**Plan 1**) outlines the following zones and reserves applicable within the Structure Plan area:

- Mixed use.
- Residential.
- Local roads.
- Parks and Recreation.

Land use permissibility within the Structure Plan area shall accord with the land use permissibility of the corresponding zone/reserve listed above, as specified in Table 1 of the City of Belmont Local Planning Scheme No. 15 (LPS 15) to the extent that the zoning of the land under the Scheme permits. The Responsible Authority should also have due regard for the uses listed as "Unacceptable" under the following zoning statements.

## 4.1.1 MIXED USE (R-AC0)

The Mixed Use zone is intended to facilitate the development of a mix of varied, but compatible, land uses including residential, offices, retail, commercial, civic and entertainment uses, in a highly integrated built form environment.

The objectives of the Mixed Use area are to:

- Provide a diversity of land uses and housing types.
- Provide for development that contributes to the creation of a high quality public realm and creates a sense of identity and character.
- Provide local retail/commercial facilities to the Structure Plan Area as well as the broader locality.

With the exception of the Main Street Precinct, it is envisaged the Mixed Use zone will accommodate residential development consisting primarily of multiple dwellings. Non-residential development is encouraged at ground level, however the 'mixed use' designation provides the flexibility for land uses to change and evolve over time in response to market conditions.

In relation to Precinct 3 Main Street, as defined in Section 4.2, the primary objective is to promote development of a local centre, where retail and other local commercial services should dominate the ground level.

## 4.1.1.1 LAND USE PERMISSIBILITY

Land use permissibility shall generally be in accordance with the corresponding zone in the Zoning Table in LPS 15. However, having regard for the amenity for future residents the following uses are considered to be Unacceptable in the Structure Plan Area and should not be approved:

- Auction Mart
- Caretakers Dwelling
- Fast Food Outlet / Lunch Bar
- Home Store
- Garden Centre
- Industry Light
- Motor Vehicle Repair
- Night Club
- Radio or TV Installation
- Restricted Premises
- Service Station
- Single House (with the exception of Precinct 7)
- Vet Hospital
- Warehouse

Retail floorspace shall be predominantly located within the Main Street Precinct as shown on the Precinct Plan (Plan 2). Retail uses may also be permitted along the Great Eastern Highway frontage.

Residential development within the Mixed Use zone shall be in accordance with the 'R-ACO' code and associated standards as set out in **Table 2**: Precinct Development Table for the relevant Precinct.

#### 4.1.1.2 PLOT RATIO

There is no maximum plot ratio applicable within the Mixed Use zone.

#### 4.1.2 RESIDENTIAL

The Residential zone is intended to encourage the delivery of a diverse range of living choices, while respecting the interface with various existing residential areas.

The objectives of the Residential zone are to:

- a) Maximise the urban infill opportunity.
- b) Ensure a high quality of urban amenity through high standards of design in both the public and private realms.
- c) Seek to enhance the amenity of surrounding existing residential environments.

## 4.1.2.1 DWELLING TARGET

A variety of dwelling densities are proposed, depending on the characteristics of the location. Overall, the dwelling targets for the Residential zone is/are:

283 dwellings per gross ha.

#### 4.1.2.2 DENSITY

**Plan 1** (Structure Plan) defines the residential densities that apply to different areas within the Structure Plan.

## 4.1.3 PARKS AND RECREATION

The foreshore reserve and Belmont Trust Land are included in the Structure Plan Area for context only. No specific works or requirements are required under the Structure Plan for these areas. The Belmont Trust Land will be the subject of separate planning to be undertaken by the Belmont Trust.

Within the balance of the Structure Plan Area, Public Open Space (POS) is to be provided generally in accordance with **Plan 1** and should be vested in the Crown and managed by the Local Government. The development of land included within the Swan and Canning River Development Control Area will be subject to the approval of the Department of Biodiversity, Conservation and Attractions (DBCA).

The POS is to provide for both active and passive recreation uses. The POS areas may accommodate stormwater generated from the proposed development; however, this should be designed such that its function as local open space is not compromised.

#### 4.1.4 LOCAL ROADS

#### 4.1.4.1 EXISTING ROADS

Existing local roads are to be upgraded to reflect an inner urban street character, featuring on-street parking, high quality landscape and pedestrian facilities. The existing 20m reserve width shall be maintained to ensure that the street serves a high quality public realm function in addition to facilitating local traffic movement.

The introduction of additional roads within the Structure Plan Area may occur at the subdivision application stage and in accordance with Part 10 of the Planning & Development Act 2005. These roads are to be designed to a residential standard in accordance with the requirements of the Local Government. Road reserve widths shall be 20m, to reflect similar characteristics to the existing road system, unless an alternative design is supported by the Local Government and approved by the WAPC.

# 2 DEVELOPMENT REQUIREMENTS

# 4.2.1 PRECINCT DEVELOPMENT REQUIREMENTS

The following precincts have been established to ensure that the Structure Plan Area is developed in a comprehensive and integrated manner having regard to desired character, preferred land uses, residential density, built form and public realm design principles:

- Precinct 1: Great Eastern Highway
- Precinct 2: Stoneham Street
- Precinct 3: Main Street (Daly Street)
- Precinct 4: Resolution Drive
- Precinct 5: Ascot Kilns
- Precinct 6: Racecourse Interface
- Precinct 7: Hardey Road (East)

## 4.2.1.1 STATEMENTS OF INTENT

## Precinct 1: Great Eastern Highway

The Great Eastern Highway Precinct will present itself as a strong, unified commercial and mixed-use edge.

Commercial uses are encouraged at ground level and above with residential development to occupy at upper storeys.

The visual prominence of the Great Eastern Highway frontage will require sensitive architectural treatment to ensure that the built form contributes positively to the aesthetic quality of the area.

#### Precinct 2: Stoneham Street

The precinct, whilst still remote from the river front, will be the primary interface between the Golden Gateway development and the river.

Understanding that planning for Belmont Trust Land is yet to be undertaken, it is recommended that any future planning should maintain strong physical links between the river and the future Golden Gateway population and workforce.

Development addressing Stoneham Street is to provide an appropriate interface to the Belmont Trust Land to ensure a high standard of visual amenity and surveillance within a mixed use environment. The aspect towards the river may be attractive for food and beverage uses, which should be accommodated.

A tree-lined promenade along Hargreaves Street will create a unique vista with the Belmont Trust Land and the Swan River.

## Precinct 3: Main Street (Daly Street)

Daly Street will become the community nucleus for the development and the surrounding community. The precinct is intended to perform a Local Centre function, anchored by a small supermarket and supported with local specialty shops, and restaurant/cafes. Retail development must present a 'Main Street' character, with active edges to the street.

A leafy boulevard will provide a shared vehicle pedestrian space, providing a pleasant public realm against an active street frontage and a strong physical link for pedestrians between the Main Street and the river.

#### **Precinct 4: Resolution Drive**

This precinct will be characterised by medium-high density residential uses with the potential for commercial uses at ground level.

Buildings at the junctions of Stoneham Street and Resolution Drive should feature a distinctive and iconic built form which marks an 'arrival' point to Golden Gateway. The Stoneham Street junction is considered to present an opportunity for a high standard short stay/hotel development. Additional building height will be encouraged in these programs.

Ground level development on the south side may be residential or commercial but should be designed to accommodate non-residential use.

Trees will line either side of the southern portion of Grandstand Road (between Great Eastern Highway and Resolution Drive) to create an attractive pedestrian environment connecting with the central open space area.

#### Precinct 5: Ascot Kilns

This precinct is characterised by the historic kilns and landmark chimney stacks that are of considerable State heritage significance. Development will therefore have a strong heritage and landscape focus, using built form to celebrate and frame the historic structures, and to secure their ongoing preservation.

This precinct is the subject of separate Local Planning Policy (LPP) and Local Development Plan (LDP).

## Precinct 6: Racecourse Interface

The Ascot Racecourse holds several large events during the year that have an impact on its surrounds in terms of traffic, parking and pedestrian movement. Proposed modifications to the road and movement systems are expected to improve amenity in the interface precinct and consequently open up this area for potential redevelopment.

Responding to this opportunity, the Racecourse Interface Precinct will be a transitional area comprising primarily 'townhouse style' and low-scale apartment development. The precinct will be designed to ensure an adequate interface from a higher density mixed use environment to the more moderately scaled existing residential development, having regard to its location adjacent to the Racecourse and Ascot Kilns.

## Precinct 7: Hardey Road (East)

A distinctive low density residential precinct comprising predominately single residential development to act as a transitional area to low-scale existing residential development within the stables area.

# 4.2.1.2 PRECINCT STANDARDS AND REQUIREMENTS

Table 2: Precinct Development Table outlines the standards and requirements for subdivision and development in the corresponding precincts designated on Plan 2 Precinct Plan. Building height requirements should be read in conjunction with Plan 3 Building Height Plan.

In addition to the Precinct Development Table, Design Guidelines shall be adopted to provide further guidance for subdivision and development of the precinct pursuant to Section 5.1.

In relation to Precinct 5 Ascot Kilns, development standards and requirements in this Structure Plan should be read in conjunction with the Ascot Kilns Design Guidelines and

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	Precinct	R-Code	Min. height	Max. height	Min. side setback	Min. front setback	Max. front setback
<del></del>	Great Eastern Highway	R-AC0	Podium: 2 storeys Tower: 7 storeys	Podium: 5 storeys Tower: 15 storeys	Podium: Nil Tower: as per Apartment Design Guidelines	Podium: Nil Tower: 3m	Podium: Nil Tower: N/A
2	Stoneham Street	R-AC0	Podium: 2 storeys Tower: 5 storeys	Podium: 3 storeys Tower: 10 storeys	Podium: Nil Tower: as per Apartment Design Guidelines	Podium: Nil Tower: 3m	Podium: Nil Tower: N/A
m	Main Street (Daly Street)	R-AC0	Podium: 2 storeys Tower: 5 storeys	Podium: 3 storeys Tower: 10 storeys	Podium: Nil Tower: as per Apartment Design Guidelines	Podium: Nil Tower: 3m	Podium: Nil Tower: N/A
4	Resolution Drive	R-AC0	Podium: 2 storeys Tower: 5 storeys	Podium: 3 storeys Tower: 10 storeys	Podium: Nil Tower: as per Apartment Design Guidelines	Podium: Nil Tower: 3m	Podium: Nil Tower: N/A
വ	Ascot Kiins	R-AC0	Refer to Ascot Kilns Design Guidelines and Local Development Plan	Refer to Ascot Kins Design Guidelines and Local Development Plan	Refer to Ascot Kilns Design Guidelines and Local Development Plan	Refer to Ascot Kilns Design Guidelines and Local Development Plan	Refer to Ascot Kilns Design Guidelines and Local Development Plan
9	Racecourse Interface	R40 & R100	2 storeys	6 storeys	As per Residential Design Codes	As per Residential Design Codes	N/A
7	Hardey Road (East)	R20	A/N	2 storeys	As per Residential Design Codes	As per Residential Design Codes	N/A

#### Notes:

- Minimum and maximum building heights specified for tower components are inclusive of podium levels. 7
- Landmark sites Tower elements shall be permitted up to 5 storeys above specified maximum height, subject to the criteria set out in Section 4.2.2. Podium heights are permitted to range between 3-5 storeys. ς;
- 3. This table is to be read in conjunction with the more detailed provisions of the LPP.
- In relation to Precinct 5 Ascot Kilns, this table is to be read in conjunction with the Ascot Kilns Design Guidelines and LDP. 4.

# 4.2.2 GENERAL DEVELOPMENT REQUIREMENTS

Development within the subject land shall be generally in accordance with the standards and requirements of LPS 15 and the relevant LPP, having due regard to the provisions contained within this LSP.

Proposed variations to the standards and requirements of LPS 15 or the provisions of this LSP are to be outlined within a development application and will be considered by the Responsible Authority with due regard to the intent and purpose of the standards.

#### 4.2.2.1 PARKING

Car parking should be provided in accordance with LPS 15, subject to the following variations:

1. The Local Government wishes to encourage innovative approaches to car parking provision, such as reciprocity, car-pooling programs or other innovations, that may result in reduced parking provision where appropriate.

The Responsible Authority will consider approving a reduced parking provision where it can be demonstrated that an alternative parking proposal is sound and will result in a reduction in parking demand. Any proposed variation should be supported by a parking demand assessment undertaken by a suitably qualified professional.

- 2. The following specific requirements apply:
- a) For Mixed Use development, all residential parking in excess of 1 bay per dwelling, and at least 50% of the minimum required parking for non-residential uses shall be made available for general use of either residential or nonresidential uses (these bays represent unallocated communal parking bays).
- b) Mixed Use development that proposes parking as outlined in 2a) above should be required, as a condition of Development Approval, to prepare a Car Parking Strategy that addresses the management of the unallocated communal parking provision, including:

- The hours during which parking bays shall be made available for general public access.
- Location, signage and monitoring of usage of the unallocated communal parking bays.
- c) For multiple dwelling residential development, parking requirements shall be as follows:
- Minimum parking: in accordance with **Table 3**.
- ii. Maximum parking: not to exceed double the minimums specified in **Table 3**.

TABLE 3: MULTIPLE DWELLING RESIDENTIAL PARKING

Parking Types	Location A	Location B
1 bedroom dwellings	0.75 bay per dwelling	1 bay per dwelling
2+ bedroom dwellings	1 bay per dwelling	1.25 bays per dwelling
Visitor Parking	1 bay per 4 dwellings up to 12 dwellings 1 bay per 8 dwellings for the 13th dwelling above	

#### Definitions:

**Location A**: Within 250m of a high frequency bus route, measured in a straight line from along any part of the route to any part of the lot.

**Location B:** Not within Location A.

d) The provision of car parking that is in excess of the minimum required for the site will only be approved where it is designed to be adaptable for future conversion into habitable floor space, or other useable space for communal or private usage. In order for parking to be considered adaptable, it must be shown as located in a position that is suitable for an alternative use, not included in individual strata titles and constructed to comply with habitable floorspace standards.

This requirement may be waived if it can be demonstrated that complying with the requirement would not be practical or would result in a less desirable outcome.

#### **BUILDING HEIGHT** 4.2.2.2

Minimum and maximum building heights within the Structure Plan Area are to be in accordance with the ranges identified in Table 2 and on Plan 3.

Notwithstanding the provisions of Table 2 and Plan 3, maximum building heights are subject to compliance with the Airports (Protection of Airspace) Regulations 1996

https://www.perthairport.com.au/Home/corporate/planning-and-projects/airspace-Information on Obstacle Limitations Surfaces is available at protection

#### 4.2.2.3 LANDMARK SITES

Landmark site locations have been identified on Plan 3. These sites have been located in response to priority view lines and public vistas. They define local character and maximise legibility through high quality pedestrian scale experience and are generally expected to be of a greater building height (3-5 storeys for podium element and an additional 5 storeys above the specified maximum for tower element) than surrounding development.

specified in the adopted LPP.

In some instances, the podium element may not be required, if it can be demonstrated

The Responsible Authority will only approve the additional height potential for a landmark site if it is satisfied that the proposal demonstrates design excellence in the form of articulation of the building, proportion, quality, scale, massing and detailing as

that:

1. The landmark character of the site and/or building is more effectively achieved; and

2. The design will offer publicly accessible open space contiguous with adjacent public

space (including street verge)

# 5 OTHER REQUIREMENTS

## 5.1 SCHEME AMENDMENT

The existing zoning arrangement within the Structure Plan Area does not currently reflect the proposed zoning as outlined within **Plan 1**. Consequently, a Scheme Amendment will be required to address this inconsistency during the implementation of the Structure Plan.

The Structure Plan Area should be rezoned to 'Special Development Precinct' to provide the appropriate base zoning to facilitate integrated development for the Precinct. In addition, in order to achieve the required planning framework for the Structure Plan Area, it will be necessary to designate a 'Development Area' over the subject land under Part 6 of LPS 15. In accordance with Clause 6.2.4 of LPS 15, the preparation and approval of a Structure Plan is required to guide subdivision and development. This process can occur concurrently with the rezoning of the Structure Plan Area to 'Special Development Precinct' as described above.

As a pre-requisite to the finalisation of this Scheme Amendment process, the City will require the preparation and adoption of a LPP.

# 5.2 LOCAL PLANNING POLICY

No development or subdivision should be approved within the Structure Plan Area, until a LPP is adopted by the Local Government containing design guidelines and development standards, unless otherwise agreed by the Local Government.

Upon adoption, the LPP will provide a comprehensive development framework for the Golden Gateway Precinct, with the exception of the Ascot Kilns Precinct which is proposed to be subject to a separate LDP and LPP. Accordingly, subject to the discretion of the City of Belmont and WAPC, additional LDPs may not be required.

All subdivision and development shall be generally in accordance with the LPP.

# 5.2.1 CONTENT OF LOCAL PLANNING POLICY

The purpose of the LPP is to guide development within the Golden Gateway Precinct, providing sufficient detail to ensure comprehensive control over development (via a performance-based approach) to achieve high quality built form outcomes.

The document will define the proposed character of the various streetscapes and open spaces and will illustrate how the interface between the built form and the street should be treated in order to create high quality public spaces.

The following matters should be considered in the preparation of the LPP:

- Land use
- Minimum number of dwellings and dwelling diversity
- Building height, form and typology
- Building envelopes
- Setbacks
- Activation
- Form and mass of buildings
- Relationship between buildings and the public realm
- Landmark buildings
- Public art
- Climate and wind
- Overshadowing, overlooking and privacy
- Vehicular access, parking and service areas

Development applications received, either as a singular land use or as a component of an integrated mixed use development, are to be assessed by the Responsible Authority against the provisions of this Structure Plan and LPP.

In relation to residential development, in the event of there being any variations or conflict between the provisions, standards or requirements of the R-Codes and the provisions, standards or requirements of this Structure Plan, then the provisions, standards or requirements of the R-Codes shall prevail, except where the provisions, standards or requirements are stated in the LPP.

# 3 INFRASTRUCTURE FUNDING STRATEGY

The Local Government will establish an appropriate funding strategy for the provision of infrastructure within the Structure Plan Area. The strategy may include the introduction of a Development Contribution Area (DCA) through LPS 15, under which a Development Contribution Plan (DCP) can be implemented to contribute to the funding of public infrastructure necessary to facilitate development in the Structure Plan Area.

Infrastructure items that would be eligible to be funded under a DCP should be in accordance with State Planning Policy 3.6 Development Contributions for Infrastructure

# 5.4 BUSHFIRE MANAGEMENT

This Structure Plan is supported by a Bushfire Management Plan (BMP), which is contained at **Appendix A**.

Where appropriate, development will have regard to the Bushfire Attack Level (BAL) Assessment contained in this Report and be determined in accordance with Schedule 2, Part 10A of the Planning and Development (Local Planning Schemes) Regulations 2015 and section 6.3 of SPP 3.7 Planning in Bushfire Prone Areas (SPP 3.7).

An LDP is required to be prepared for all lots with a BAL of 12.5 or greater.

Where a subdivision application includes land with a BAL of 12.5 or greater, the Local Government shall recommend to the WAPC that a condition be imposed on the grant of subdivision approval for a notification to be placed on the Certificate of Title to suitably respond to the following:

"That a lot with a bushfire attack level BAL rating of 12.5 or higher is subject to a BMP."

## 5.5 ABORIGINAL HERITAGE

A search of the Department of Planning, Lands & Heritage (DPLH) aboriginal heritage enquiry system identifies one site occurring within the northern/western portion of the subject land (Site ID 3753).

Should the aboriginal heritage site identified as meeting the requirements of section 5 of the Aboriginal Heritage Act 1972 (AHA) be proposed to be disturbed in any way, an application must first be made and consent granted under section 18 of the AHA.

Furthermore, where applicable, an Aboriginal Heritage Management Plan shall be prepared and implemented prior to subdivision of any land affecting the identified site.

## 5.6 NOISE ATTENUATION

For subdivision and development located adjacent to Great Eastern Highway, Resolution Drive and Grandstand Road, an acoustic assessment shall be undertaken and included as part of any application to demonstrate that the proposed design will meet the internal noise level requirements of State Planning Policy 5.4 Road and Rail Transport Noise and Freight Considerations in Land Use Planning (SPP 5.4).

In accordance with SPP 5.4 a notification shall be required to be placed on the Certificate of Title for lots where dwellings are exposed to traffic noise that exceeds the outdoor "Noise Target" as defined in the Policy.

# 6 ADDITIONAL INFORMATION

Table 4 below outlines additional information that will be required at future approval stages. Additional information requirements may not be limited to those listed; the City or WAPC may require other information in relation to particular proposals.

TABLE 4: MANAGEMENT PLANS, REPORTS AND STRATEGIES

	Approval stage	Approving Authority		
Water Management			Management Plan	
Local Water Management	Documented in Structure Plan and to be	WAPC, City,	Engineering	
Strategy (LWMS)	considered as part of Structure Plan process. Implementation as part of UWMP	DWER	Servicing Report	0 0
Urban Water Management Plan (UWMP)	Condition of subdivision	WAPC, City, DWER		5
Environment			Geotechnical	C
Environmental Assessment Report	Documented in Structure Plan Implementation via Subdivision	WAPC, City, OEPA,	Other	
Fire Management Plan	Condition of subdivision	WAPC, City	Local Development Plan(s)	O eu
Foreshore Management Plan	Condition of Subdivision	WAPC, City, DBCA		
Landscape Management Plan	Condition of subdivision	City		
Aboriginal Heritage Management Plan	Condition of subdivision	DPLH		
Acoustic Report (Noise Attenuation)	Condition of planning approval	City		
Acid Sulphate Soils	Condition of Subdivision	DWER		
Investigation for soil and groundwater contamination	Condition of Subdivision	WAPC, City		
Identification and protection of vegetation worthy of protection	Condition of Subdivision	WAPC, City		
Erosion and Sediment	Condition of Subdivision	WAPC, City		

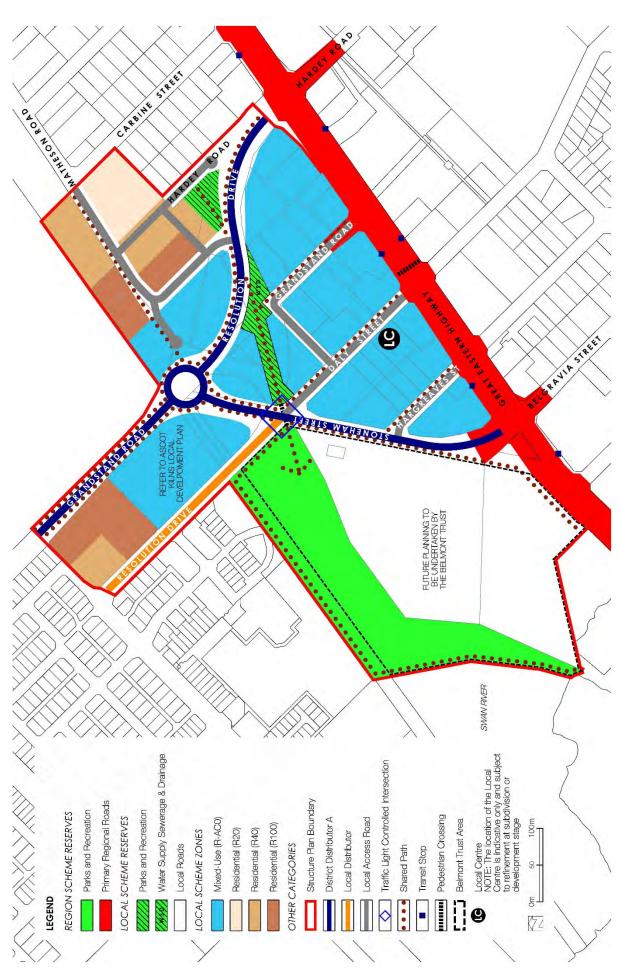
Approval stage			Documented in Structure Plan		nondition of Subdivision			Plan(s) Condition of subdivision if deemed necessary by City								
Approving Additional information	Management Plan	WAPC, City,	WER Servicing Report	WAPC, City, DWER	Leothorical Leothorical		OEPA, Other	WAPC, City Local Development Plan(s)	WAPC, City, DBCA	City	рргн	City	DWER	WAPC, City	WAPC, City	WAPC, City
Approval stage		o pe	considered as part of Structure Plan D process. Implementation as part of UWMP	Condition of subdivision		Documented in Structure Plan	via Subdivision		Condition of Subdivision D	Condition of subdivision	Condition of subdivision	Condition of planning approval	Condition of Subdivision	Condition of Subdivision	Condition of Subdivision	Condition of Subdivision
		nent		ment Plan		sment		_	nt Plan	ent Plan	anagement	Φ		nd nation	ection of protection	

City, Water Corp, Western Power, ATCO Gas

City

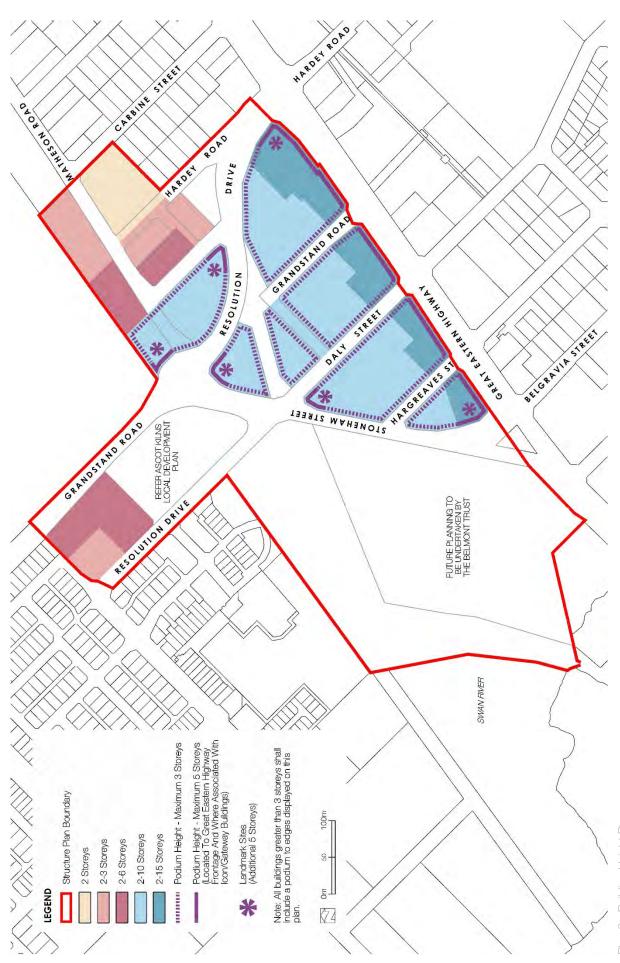
City

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Plan 1- Structure Plan





Plan 3 - Building Height Plan

# PART TWO EXPLANATORY INFORMATION

# PLANNING BACKGROUND

# 1 INTRODUCTION AND PURPOSE

This report has been prepared to provide a technical explanation for the provisions contained in Part 1-Implementation of the Golden Gateway Structure Plan.

The Structure Plan outlines the development vision for the ultimate development of the Golden Gateway Precinct (the subject land) and establishes key requirements. The Structure Plan also includes information regarding the development of the public realm and assesses the proposed development in context with the surrounding physical and natural environment.

The Project Team, responsible for preparing the information contained within this report, (in consultation with the City of Belmont and relevant Service Authorities) include those detailed in **Table 1.** 

## TABLE 1: PROJECT TEAM RESPONSIBILITIES

Consultant	Taylor Burrell Barnett	Taylor Robinson	Cardno	Urbaqua	Flyt	EPCAD	Place Match	Urbaqua
Project Role	Town Planning and Urban Design	Architectural	Civil Engineering	Environment Management and Hydrology	Traffic and Transport	Landscape	Community Engagement	Bush Fire Management

## .. LAND DESCRIPTION

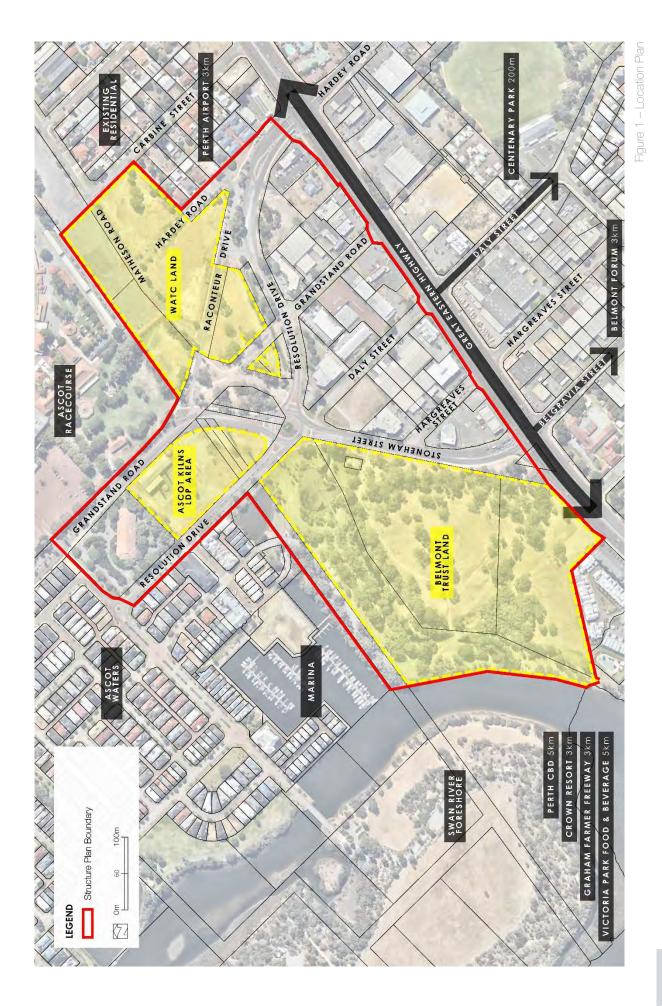
#### 1.2.1 LOCATION

The location and extent of the subject land is outlined in **Figure 1.** The subject land is located at the axis of the key movement corridors of Great Eastern Highway, Stoneham Street, Grandstand Road and Resolution Drive and includes key strategic sites such as Belmont Trust Land, Ascot Kilns and Western Australian Turf Club (WATC) headquarters and associated land.

**Figure 2** shows the subject land's district context. The land is located approximately 5 kilometres (km) north-east of the Perth Central Business District (CBD), 3km north of Belmont Forum and mixed business area, and 5km north-east of Victoria Park entertainment precinct. Within its immediate context, the subject land is located adjacent the Swan River and Ascot Racecourse.

It is also well connected to regional movement networks such as the Graham Farmer Freeway and Tonkin Highway. The Garratt Road Bridge also provides a key connection to the north across the Swan River.

Within the local context, the subject land can be regarded as lacking in basic convenience shopping facilities. The BP Service Station located on the corner of Great Eastern Highway and Resolution Drive and delicatessen located at Epsom Avenue approximately 2km south-east of the subject land provide the nearest local conveniences. However, the nearest neighbourhood centres (supermarkets) are Eastgate Commercial Centre, Kooyong Road, approximately 2.5km to the south-west, or Belvidere Street approximately 2.5km to the south. Additional services are located approximately 3km to the north-west of the subject land at Maylands Shopping Centre (neighbourhood centre) or 3km to the south at Belmont Forum (Secondary Centre).



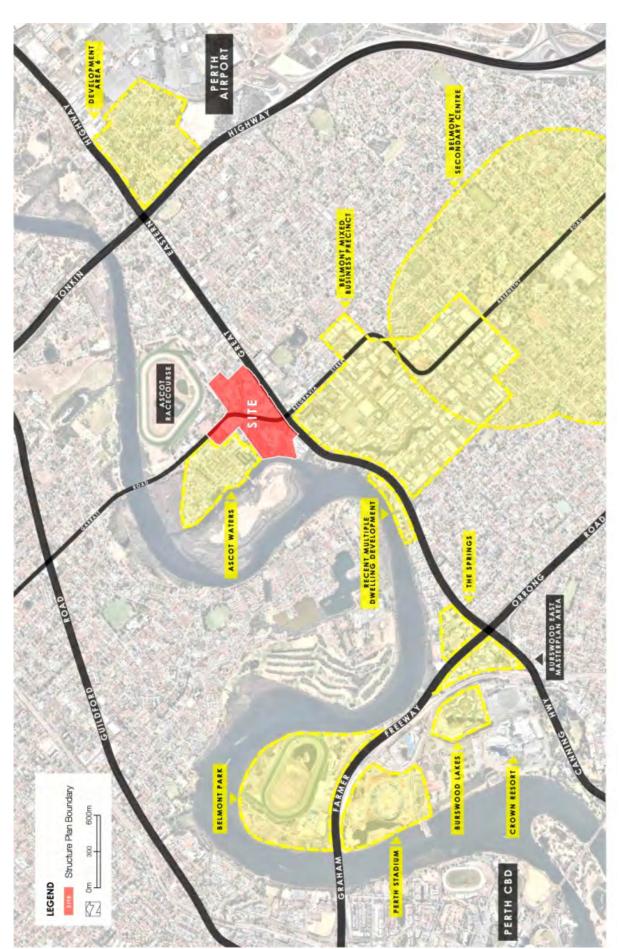


Figure 2 - District Context Plan

#### **1.2.2 LAND USE**

The subject land can be divided into four areas based on existing uses (refer Figure 3):

- The area bounded by Great Eastern Highway, Stoneham Street and Resolution Drive is characterised by predominately mixed business development and small pockets of retail (food and beverage) uses along Great Eastern Highway;
- The western portion of the subject land encompassing the Belmont Trust Land is largely cleared within the central portion with mature vegetation around the periphery. The site was historically used as a baseball field;
- The northern portion of the subject land is partially developed with the WATC Headquarters and Ascot kilns and chimney stacks; and
- The remainder of the subject land within the north-eastern corner is largely undeveloped and comprises a number of existing road reserves and WATCowned land used for overflow parking on racing event days.

The development of the Belmont Trust Land and Ascot Kilns sites are subject to separate planning processes.

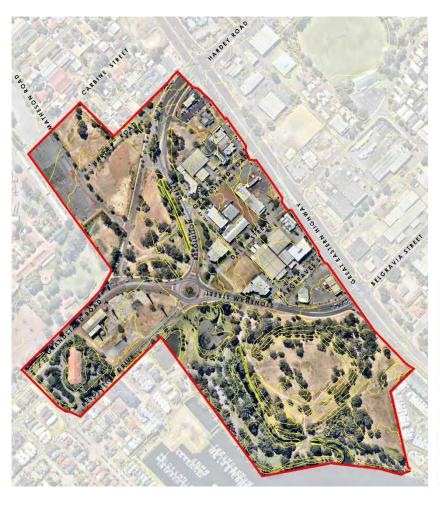




Figure 3 – Site Plan

# 1.2.3 LEGAL DESCRIPTION AND OWNERSHIP

The subject land is approximately 23.9871 hectares (ha) in area comprising the land identified in Table 2 and Figure 4.

**TABLE 2: LAND TENURE** 

Lot/Reserve	Landowner	Plan Number	Volume/Folio	
1 Resolution Drive	City of Belmont	P76257	2835/27	0.3642
5 Resolution Drive	City of Belmont	D64041	1776/785	4.1919
642 Great Eastern Highway	City of Belmont	P66341	2763/431	2.6481
950 Marina Drive / R52200	State of WA (City of Belmont)	P73752	LR3165/863	0.5843
512 Marina Drive / R51911	State of WA (City of Belmont)	P39786	LR3025/38	0.7749
513 The Boardwalk / R51911	State of WA (City of Belmont)	P32861	LR3025/39	0.2621
10417 Grandstand Road / R38783	State of WA (Water Corporation)	P185797	LR3048/920	0.1059
12645 Grandstand Road / R45069	Water Corporation	P15104	LR3064/783	0.2181
3 Grandstand Road	Edward Van Heemst The Chairman For The Time Being Of The Committee For The Time Being Of The WA Turf Club	D55346	1742/278	0.0351
13 Grandstand Road	WA Turf Club	D26760	1883/670	0.7316
51Raconteur Drive	Edward Van Heemst The Chairman For The Time Being Of The Committee For The Time Being Of The WA Turf Club	P15104	1883/668	0.6940
100 Raconteur Drive	Edward Van Heemst The Chairman For The Time Being Of The Committee For The Time Being Of The WA Turf Club	P60341	2723/304	2.5726
452 Grandstand Road	WA Turf Club	P60339	2723/355	1.1441
7705 Matheson Road	Chairman of the WA Turf Club & His Successors in Office	P209359	1789/567	
1 Grandstand Road	State Planning Commission	D55346	1742/276	0.2452
197 Grandstand Road	State Planning Commission	P2635	1754/354	0.3927
236 Grandstand Road	State Planning Commission	P2635	1754/354	0.8925
237 Grandstand Road	WA Planning Commission	P2635	2117/791	0.9796
713 Grandstand Road	WA Planning Commission	D93557	2117/790	1.2806
707 Great Eastern Highway	Pedersen, AG & Pedersen, NW	P67257	2750/217	0.4767
709 Great Eastern Highway	Australian Postal Commission	P67258	1122/816	0.0551
1 Stoneham Street	5 Stoneham Road Belmont (Strata Scheme)	D41222	SP20374	0.2373
43 Hargreaves Street	Tarfield Holdings Pty Ltd	P2294	1582/988	0.1012
44 Hargreaves Street	Tarfield Holdings Pty Ltd	P2294	1582/989	0.1012

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Lot/Reserve	Landowner	Plan Number		Area (ha)
45 Hargreaves Street	Jones, ED & Moor, JR	P2294	1977/545	0.1012
1 Great Eastern Highway	Ascot Grove (Strata Scheme)	P72552	SP65435	0.1966
60 Daly Street	Qube Ascot Development Ltd	D73791	1801/608	0.3934
36 Daly Street	Motwil Pty Ltd	P2294	1582/987	0.1012
35 Daly Street	Motwil Pty Ltd	P2294	1582/986	0.1012
650 Daly Street	76, 78 Daly Street, Belmont (Strata Scheme)	D59457	SP10988	0.2024
714 Great Eastern Highway	TLC Carousel Holdings Pty Ltd	P67260	2753/447	0.2033
52 Daly Street	SMC Pneumatics Australia Pty Ltd	D68380	1839/787	0.3798
801 Daly Street	Capital Growth Holdings Pty Ltd	P403687	2907/899	0.2440
21 Daly Street	Ashguard Pty Ltd	D78708	1892/169	0.2332
22 Grandstand Road	Ashguard Pty Ltd	D78708	1892/170	0.2031
23 Grandstand Road	Starttime Pty Ltd	D78708	1892/171	0.3731
11 Grandstand Road	Command-a-com Pty Ltd	D17872	1182/103	0.1011
800 Great Eastern Highway	Selden Pty Ltd	P403687	2907/898	0.2833
100 Resolution Drive	Pindan Pty Ltd	D73202	1800/401	0.2071
101 Grandstand Road	127-129 Grandstand Street Belmont (Strata Scheme)	D73202	SP15951	0.3126
500 Grandstand Road	Kwik 'N' Kleen Pty Ltd	D90797	2076/935	0.3568
501 Great Eastern Highway	Sunlight Food Pty Ltd	D90797	2076/937	0.1063
502 Great Eastern Highway	Worldfirst Enterprises Pty Ltd	D90797	2076/938	0.1788
730 Great Eastern Highway	Novell Properties Pty Ltd	P67267	2753/474	0.3574
100 Great Eastern Highway	Selden Pty Ltd	P73087	2840/325	0.2622



Figure 4 - Land Tenure

# PLANNING FRAMEWORK

# 1.3.1 ZONING AND RESERVATIONS

## 1.3.1.1 METROPOLITAN REGION SCHEME

The subject land is predominately zoned 'Urban' under the Metropolitan Region Scheme (MRS) (refer **Figure 5)**.

Land abutting the Swan River within the subject land is reserved 'Parks and Recreation' and is situated within the 'Swan and Canning River Development Control Trust' area.

The south-eastern boundary abuts 'Primary Regional Roads' (PRR) reservation (Great Eastern Highway) directly to the south. This PRR reservation also extends north into the subject land at Stoneham Street and Grandstand Road.

The majority of the surrounding area is zoned 'Urban', whilst Ascot Racecourse is zoned 'Private Recreation'.

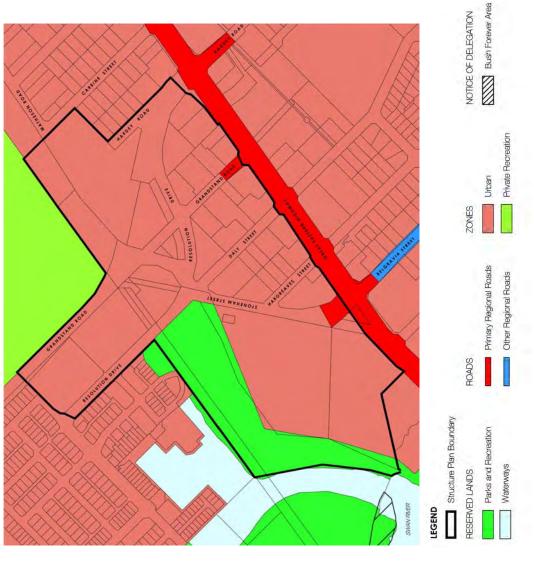


Figure 5 - MRS Zoning

# CITY OF BELMONT LOCAL PLANNING SCHEME NO. 15

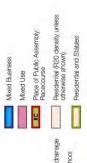
The subject land is predominantly zoned 'Mixed Use' under the City of Belmont's Local Planning Scheme No. 15 (LPS 15) (refer Figure 6).

Land within the north-eastern portion associated with Ascot Racecourse is zoned 'Place of Public Assembly – Racecourse' and identified with an 'Additional Use (A18)'. Land within the north-western portion of the subject land is also zoned 'Place of Public Assembly – Racecourse' associated with the WATC Headquarters (Lee-Steere House). Consistent with the reservations under the MRS, the western portion of land abutting the Swan River is reserved 'Parks and Recreation' and Great Eastern Highway is reserved 'Primary Regional Roads' along with connecting sections of Stoneham Street and Hargreaves Street. A stretch of land along Resolution Drive is reserved as Local Scheme Reserve - 'Parks and Recreation: Water supply sewerage and drainage'. This land contains a Water Corporation drain. Land to the south of Great Eastern Highway, within proximity to Belgravia Street is predominantly zoned 'Mixed Business' with portions also zoned 'Mixed Use'

classifications in the Structure Plan, and a Scheme Amendment will be required to bring these areas into conformity. Development within the affected areas will not be Presently the underlying zonings under LPS 15, are inconsistent with proposed permitted until such time as the land has been appropriately rezoned.









Ascot Waters Special Development Precinct

OTHER CATEGORIES

8.

Golden Gateway | Structure Plan

## 1.3.2 PLANNING STRATEGIES

## 1.3.2.1 PERTH AND PEEL @ 3.5MILLION

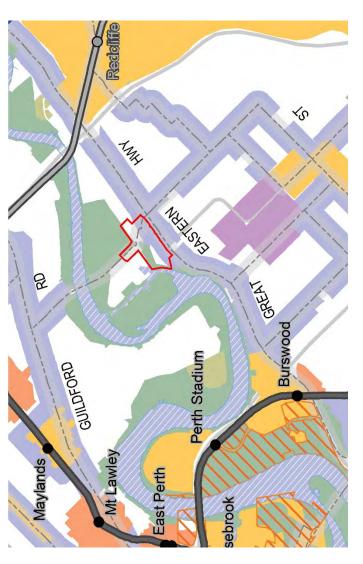
Perth and Peel@3.5million Planning Framework is a strategic suite of documents to guide future land uses through urban consolidation, integrated infrastructure and development, co-location of services and the strategic location of employment opportunities.

The subject land is located in the Central sub-region of the *Perth and Peel @3.5million Planning Framework* document.

The population in the Central sub-region is projected to grow by more than 468,000 people between 2011 and 2050 — from around 783,000 to nearly 1.2 million people. It is expected that more than 285,000 additional jobs will be accommodated in the Central subregion up to 2050.

The Central sub-region is expected to supply an additional 215,000 dwellings under the Framework, with 10,410 dwellings to be provided within the City of Belmont.

The Framework identifies Great Eastern Highway as an 'urban corridor' and Grandstand Road-Stoneham Street continuing into Hardey Road as a 'high frequency public transit' (refer **Figure 7**).



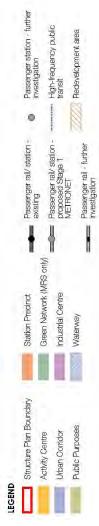


Figure 7 - Central Sub-regional Planning Framework

The Framework states that corridors should be the focus for investigating increased densities, with potential for mixed land uses where appropriate. The presence of existing or planned high-quality public transport is an important consideration in determining whether a corridor is suitable for a more-compact and diverse urban form.

## 1.3.3 PLANNING POLICIES

## 1.3.3.1 STATE PLANNING POLICIES

# SPP 5.4 Road and Rail Transport Noise and Freight Considerations in Land Use Planning

State Planning Policy 5.4 – Road and Rail Transport Noise and Freight Considerations in Land Use Planning (SPP 5.4) seeks to minimise the adverse impact of transport noise, without placing unreasonable restrictions on noise-sensitive residential development. SPP 5.4 is applied where the proposal includes:

- A proposed new noise-sensitive development in the vicinity of an existing or future major road, rail or freight handling facility.
- A proposed new major road or rail infrastructure project in the vicinity of existing or future noise sensitive and uses.
- A proposed major redevelopment of existing major road or rail infrastructure in the vicinity of existing or future noise-sensitive land uses.
- A proposed new freight handling facility.

Great Eastern Highway is identified as a 'primary freight road' under SPP 5.4. Therefore, for any subdivision or development proposed adjacent to Great Eastern Highway, an acoustic assessment should be undertaken and included as part of any application to demonstrate that the proposed design will meet the internal noise level requirements of SPP 5.4.

At present, the other key roads – Resolution Drive, Grandstand Road and Stoneham Street – do not reach the traffic threshold of 20,000 vehicles per day (vpd) to warrant consideration under the Policy; however, it is anticipated that, with the proposed changes to the road network, and future development within the Golden Gateway precinct, traffic on Resolution Drive and Grandstand Road is likely to exceed that threshold in the future. Therefore, having regard for the expected traffic volumes and the amenity for future residents, it is proposed to apply the requirements of SPP 5.4 to development adjacent to those roads.

## 1.3.3.2 LOCAL PLANNING CONTEXT

## Ascot Kilns Local Development Plan (Draft)

The draft Ascot Kilns Local Development Plan (LDP) and draft Local Planning Policy (LPP) was considered by Council for final approval at its Ordinary Council meeting of 12 December 2017.

The draft Ascot Kilns LDP and draft LPP proposes a vision to guide and coordinate future development across the 1.6ha former Bristile Kiln site. The draft LDP proposes the following outcomes:

- Creation of two development sites for residential apartments and some commercial uses within proposed building envelopes.
- Provision of an active edge component fronting onto the kilns cluster (promoting small-scale retail and hospitality).
- Development scale influenced by the surrounding lower scale residential context and the chimney stacks.
- Maintaining physical and visual access to the heritage structures from key aspects.
- Potential for integration of the heritage structures within future development sites to maximise opportunities for adaptive reuse and innovative design solutions.

# Local Planning Policy 11 Public Art Contribution Policy

The City of Belmont's Local Planning Policy No. 11 (LPP 11) outlines the requirements for the provision of public art by the developer to protect and enhance the utility, amenity and identity of the public domain.

The City of Belmont requires all development proposals within the Policy Area of a value greater than \$4.5 million to provide public art in accordance with the described method for determining public art contributions. The cost of any public art shall be no less than one percent of the value of the eligible proposal and provided in kind or alternatively, the Council may accept a cash-in-lieu payment.

A portion of the subject land falls within Precinct 4 – Great Eastern Highway Precinct of LPP 11 with the balance (excluding Ascot Kilns LDP area) situated within Precinct 8 – Ascot Racecourse and Ascot Waters Precinct of LPP 11.

# 1.3.4 PRE LODGEMENT CONSULTATION

A key component of the concept planning for the subject land has been stakeholder and community consultation and engagement. The Department of Planning, Lands & Heritage has also been a key stakeholder in the concept planning process given the presence of the Ascot Kilns site within the Golden Gateway Precinct. The WATC have also been consulted separately given its significant landholding within the precinct.

As part of the consultation and engagement strategy, three workshops were held during May 2016 and a fourth workshop in November 2016:

- City of Belmont Council Staff Workshop (6 May 2016 22 participants)
- 2. Business and Landowners Workshop (26 May 2016 5 participants)
- 3. Community and Residents Workshop (31 May 32 participants).
- Combined Business/Landowners and Community/Residents Workshop November 2016).

In addition to the above workshops, two online surveys were conducted by the City of Belmont (May and November 2016) to provide the community with the opportunity to provide additional comments. Feedback received was consistent with feedback provided at the various workshops as summarised below.

## 1.3.4.1 STAKEHOLDER WORKSHOPS

Overall, the overwhelming priority was the preservation and enhancement of POS both within Belmont Trust Land and throughout the remainder of the development. The emphasis was placed on the enhancement of active POS supported by recreational amenity and infrastructure.

Addressing the current disjointed existing road network and consequential urban form, whilst improving the experience for pedestrians and cyclists was a consistent discussion point. Great Eastern Highway poses an inevitable challenge for access, but also a major opportunity to create a sense of arrival and investment for the area. The proposed realignment of Resolution Drive and downgrading of Stoneham Street was acknowledged for its potential to improve connectivity and unlock segregated landholdings, however the traffic modelling later found that the impact of this modification on the level of traffic using the Resolution Drive-Hardey Road corridors was too significant.

Overall, residential development within the Golden Gateway Precinct was supported with varying degrees of density and height, however careful integration with existing residential (Stables area, Ascot Waters) is paramount.

Other priorities included the creation of a destination / attraction for the City of Belmont and identification of 'place' qualities that will need to be considered in subsequent planning stages.

# SITE CONDITIONS AND CONSTRAINTS

An Environmental Assessment Report was prepared by Urbaqua to support the Structure Plan. This report is included as **Appendix B**.

# 2.1 ENVIRONMENTAL ASSETS AND CONSTRAINTS

#### 2.1.1 VEGETATION

No vegetation of conservation significance is located within the subject land. Due to historic clearing, commercial and recreational activities, the vegetation within the subject land is largely degraded. The subject land does contain some mature trees and these will be retained where possible.

Bush Forever Area 313 (Swan River Salt Marshes) is located to the north and west of the subject land. Surrounding this area, the Department of Water and Environmental Regulation (DWER) has mapped an Environmentally Sensitive Area described as Temperate Saltmarsh' and listed as 'vulnerable' under the *Environment Protection and Biodiversity Conservation Act 1999* (EPBC Act). This area is an important habitat for local and migratory bird species, however is largely disconnected from the subject land.

#### 2.1.2 FLORA

A search of the EPBC Protected Matters Database was undertaken to identify flora species of conservation significance potentially occurring within a 2km radius of the subject land.

The search identified two 'endangered' species under the EPBC Act (Caladenia huegelii King Spider-orchid and Lepidossperma rostratum Beacked Lepidosperma) and one critically endangered species (Darwinia foetida Muchea Bell).

#### 2.1.3 FAUNA

A search of the EPBC Protected Matters Database was undertaken to identify fauna species of conservation significance potentially occurring within a 2km radius of the subject land.

The search identified three species of 'endangered' status under the EPBC Act and seven 'vulnerable' species.

As a result of existing uses, the subject land supports limited or no remnant vegetation with a lack of intact understorey vegetation. The subject land therefore provides little, to no, fauna habitat of significant value to native fauna. The vegetation within Belmont Trust Land may provide important habitat for local and migratory birds.

## 2.2 LANDFORM AND SOILS

# 2.2.1 LANDSCAPE AND TOPOGRAPHY

The subject land is generally flat and grades gently from 6 metres (m) Australian Height Datum (AHD) in the south-east to 3mAHD in the west. A few low points exist within the centre of the subject land at approximately 1-2mAHD.

The surface geology is described broadly as Guildford formation: Alluvial sand and clay with shallow-marine and estuarine lenses and local basal conglomerate. Two-thirds of the north-western portion of the subject land is classified as Ms2 – Sandy Silt, which has a low permeability, and eastern third as S8 – Sand.

## 2.2.2 ACID SULFATE SOILS

A review of DWER acid sulfate soils (ASS) risk mapping identifies approximately two-thirds of the subject land, predominantly the area coinciding with surface geology Ms2-Sandy Silt, as containing a Class I 'high to moderate' risk of ASS and the remainder, coinciding with S8-Sand, classified as Class II 'moderate to low' risk occurring within 3m of the natural soil surface.

Given the Class I classification, an ASS investigation will be carried out where works are proposed in these areas consistent with the DWER Guidelines. Should ASS be present within the subject land, all site works must be carried out in accordance with an ASS management plan approved by DWER.

## 2.2.3 CONTAMINATED SITES

A search of the DWER Contaminated Sites database found a portion of the subject land as 'Possibly Contaminated – Investigation Required'.

# .3 GROUNDWATER AND SURFACE WATER

### 2.3.1 GROUNDWATER

Based on the DWER Ground Water Atlas, maximum groundwater levels are within 3m of the natural surface through the northern and central portions of the subject land, with groundwater flowing in a north-westerly direction toward the Swan River.

### 2.3.2 SURFACE WATER

A Water Corporation open drain is located within the centre of the subject land. The open drain is approximately 150m in length and directs flows of runoff from the eastern urban and industrial areas to piped drainage under the Stoneham Street / Resolution Drive roundabout to a compensation basin to the west of the subject land before travelling through a further 350m of open drain to the Swan River.

The Swan River is located adjacent to the western portion of the subject land. The DWER Floodway mapping indicates that a large area in the northern portion of the subject land lies within the Swan River 100 year average reoccurrence interval (ARI) flood fringe. Protection of the Swan River's environmental attributes will require the provision of a 50m buffer to the banks of the River consistent with its designation as an environmentally protected area and conservation category wetland (CCW) is generally applied.

The subject land also abuts the Swan and Canning River Development Control Area.

## 2.4 BUSHFIRE HAZARD

A very small portion of the subject land is identified as being located within a 'Bush Fire Prone Area' adjacent the Swan River and as such, a BMP has been prepared by Urbaqua in support of the Structure Plan (refer **Appendix A**). The BMP is a strategic level plan which identifies the bushfire protection measures to be applied to development on the subject site to accommodate compliance with:

- SPP 3.7 Planning in Bushfire Prone Areas;
- Guidelines for Planning in Bushfire Prone Areas; and
- Australian Standard for the construction of buildings in bushfire-prone areas (AS3959-2009).

As part of the BMP, a Bushfire Attack Level (BAL) Contour Map has been prepared which identifies the worst case BAL in relation to the subject land. The BAL Contour Map identifies a BAL of 'Low' across the majority of the subject land and a small portion of BAL-12.5 within the Belmont Trust Land. Given the Structure Plan does not propose development within the foreshore area subject to BAL-12.5 (or wider Belmont Trust Land), it is anticipated that any bushfire hazards can be appropriately managed.

It is expected that bushfire hazard assessment will be further refined as part of future subdivision or development stages in order to accurately assess the bushfire risk posed by surrounding classified vegetation and determine specific radiant heat exposure levels (and associated BAL) for future lots created within the Structure Plan area.

#### 2.5 HERITAGE

#### 2.5.1 ABORIGINAI

A search of the Department of Planning, Lands & Heritage (DPLH) aboriginal heritage enquiry system identifies one site occurring within the northern/western portion of the subject land.

Site ID 3753 – Registered site, Name: Perth, Type: Historical, mythological, hunting place, named place, natural feature.

Prior to disturbance of the above site, an application is to be made for consent to use the land under section 18 of the AHA.

#### 2.5.2 EUROPEAN

The Ascot Kilns and chimneys were included on the State Heritage List in 2003. The Kilns were first built in 1930, manufacturing terracotta, stoneware and steel products. The Ascot Kilns LDP celebrates and enhances the site's heritage significance and maintenance.

## 2.6 EXISTING MOVEMENT NETWORK

The subject land benefits from a surrounding movement network that features access to key regional road connections, a high frequency public transport corridor and high-quality shared path cycling links.

## 2.6.1 GREAT EASTERN HIGHWAY

The subject land is bounded by Great Eastern Highway to the south which provides access to the west towards the Perth CBD, Graham Farmer Freeway and onto South Perth, Melville and Fremantle via Canning Highway. To the east, Great Eastern Highway provides access to Perth Airport, Tonkin/Roe Highway and onto Guildford, Midland and the Swan Valley.

Great Eastern Highway is classified as a 'Primary Distributor' under the Main Roads WA (MRWA) Functional Road Hierarchy and is regarded as one of the State's principal transport corridors carrying over 65,000 vpd.

Great Eastern Highway (between Kooyong Road in Rivervale to Tonkin Highway in Redcliffe) was subject to significant upgrade works between June 2011 and February 2013. These works included:

- Widening Great Eastern Highway, from four to six lanes, between Kooyong Road (Rivervale) and Tonkin Highway (Redcliffe) – a distance of 4.2 km;
- Constructing a central median for the full length of the project;
- Upgrading all major intersections to include dedicated turning movements;
- Providing U-turn facilities at key locations in order to maintain access to businesses fronting the Highway;
- Incorporating bus priority lanes into key intersections;
- Providing dedicated on-road cycling facilities;
- Constructing footpaths for pedestrians; and
- Relocating, replacing and protecting service utilities such as telecommunications, water, power and gas.

### 2.6.2 INTERNAL ROADS

The localised road network includes a network of local distributor and access roads providing access to key regional and district roads such as Great Eastern Highway and the Garret Road bridge. Grandstand Road, Resolution Drive and Stoneham Street are classified as 'District Distributor A' roads under the MRWA Functional Road Hierarchy. These are generally described as follows:

- Grandstand Road (20m road reserve) a four lane road with a central median, running north-south within the subject land, connecting the Garratt Road crossing of the Swan River with Great Eastern Highway via Stoneham Street or Resolution Drive:
- Stoneham Street (20-25m road reserve) a four lane road without a central median, running north-south within the subject land, connecting Grandstand Road/Resolution Drive with Great Eastern Highway and Belgravia Street; and
- Resolution Drive (22-47m road reserve) a four lane with a central median, running east-west within the subject land, connecting Grandstand Road/Stoneham Street with Great Eastern Highway and Hardey Road.

All of these roads are under the control of the City of Belmont. The following roads are classified as 'Local Roads' under the MRWA Functional Road Hierarchy and are also under the control of the City of Belmont.

- Hargreaves Street (20m road reserve) a two lane road without a central median, running north-west to south-east within the subject land, providing a connection between Stoneham Street (no right turn out) and Great Eastern Highway (left in/left out only):
- Daly Street (20m road reserve) a two lane road without a central median, running north-west to south-east within the subject land, providing a connection between Stoneham Street (left out only onto Stoneham Street) and Great Eastern Highway (left in/left out only);
- Grandstand Road (south) (20m road reserve) a two lane road without a central median, running north-west to south-east within the subject land, providing a connection between Resolution Drive and Great Eastern Highway (left in/left out only); and
- Raconteur Drive (20m road reserve) operates as a one-way road from Grandstand Road to Matheson Road and is currently closed at the Grandstand Road intersection outside of event periods at Ascot Racecourse. Two-way access between Resolution Drive and Matheson Road is possible via the eastern extent of Resolution Drive.

# 2.6.3 PEDESTRIAN NETWORK AND CYCLING

### 2.6.3.1 PEDESTRIAN NETWORK

The extent and quality of the existing pedestrian infrastructure within, and surrounding, the subject land (with the exception of Great Eastern Highway) is poor and of a standard commensurate with the nature of existing development across the subject land (i.e. primarily light industrial/commercial unit style development).

However, Great Eastern Highway bordering the subject land to the south features good quality footpaths on both sides of the corridor. Within the vicinity of the subject land, the safe crossing of Great Eastern Highway by pedestrians is facilitated via traffic signal-controlled intersections at both Stoneham Street/Belgravia Street and Resolution Drive/Hardey Road intersections with Great Eastern Highway.

Each of the major road corridors running through the subject land (Grandstand Road, Resolution Drive and Stoneham Street) include footpaths along one side of the street – Grandstand Road along the eastern side adjacent to the Ascot Racecourse, Raconteur Drive along the northern side to connect to Grandstand Road, Resolution Drive along the eastern side adjacent to the Ascot Waters development and Stoneham Street along the western side adjacent to the Belmont Trust Land.

Local access streets (Hargreaves Street, Daly Street and southern section of Grandstand Road) providing access in a northerly direction from Great Eastern Highway are car dominated with no existing footpaths present.

#### 2.6.3.2 CYCLING

A number of existing shared paths and cycling connections are located within the subject land along primary routes, including Stoneham Street, Raconteur Drive and Grandstand Road. There is demand to upgrade facilities on Stoneham Street and Resolution Drive. Protected bicycle lanes and a shared path on Resolution Drive is essential, however the provision of 'on street' bicycle lanes on Stoneham Street will require further investigation dependent on the ultimate form of the road reserve.

A number of shared paths are also located within the Ascot Waters development directly to the north of the subject land. The Graham Farmer Freeway Principal Shared Path (PSP) is also located within close proximity to the subject land providing regional cycling connections and can be accessed via the shared path along the southern side of the Swan River.

The extent and quality of the existing cycling infrastructure within and surrounding the subject land is of a high standard, largely as a result of the Great Eastern Highway upgrades. Local connections are provided along Stoneham Street, Resolution Drive and Grandstand Road and further to the north within the Ascot Waters development. Regional connections are provided via high quality shared use paths along the Swan River Foreshore (via Belmont Trust Land towards the Graham Farmer Freeway PSP to access Perth CBD).

## 2.6.4 PUBLIC TRANSPORT

A number of existing bus routes operate within, or in close proximity to, the subject land. These include the Circle Route (998/999) via Raconteur Drive/Grandstand Road providing connections north to destinations including Bayswater Station, Morley Bus Station/Shopping Centre and south to destinations including Belmont Forum Shopping Centre, Oats Street Station and Curtin University.

In addition, existing bus routes (36, 40, 295, 296 and 299) operate along high frequency bus corridor of Great Eastern Highway, providing connections east to destinations including Perth Airport, Guildford, Midland and to the west to destinations including Victoria Park Transfer Station and Perth CBD.

Pedestrian access to existing public transport facilities is considered average with no bus stops currently located within the subject land. The closest bus stops are located on Grandstand Road immediately to the north of the subject land (close to the main pedestrian entry/exit to Ascot Racecourse). There are options to make improvements to public transport access if land uses within the subject land change over time to support additional public transport patronage.

## 2.7 ROAD TRAFFIC NOISE

As discussed in section 1.3.3.1, SPP 5.4 sets out specific requirements for addressing potential noise impacts from major transport arteries on adjacent noise-sensitive uses.

It has been identified that Great Eastern Highway, Resolution Drive and Grandstand Road are all likely to require consideration under SPP 5.4. In this respect any subdivision or development proposed adjacent to these roads will require an acoustic assessment to be undertaken and included as part of any application to demonstrate that the proposed design will meet the internal noise level requirements of SPP 5.4.

# 2.8 EXISTING INFRASTRUCTURE AND SERVICING

### 2.8.1 WATER SUPPLY

The Serpentine Trunk Main is located along Grandstand Road and Daly Street. A 915 steel distribution main is also located along Grandstand Road through the subject land. Existing development within the subject land is well serviced with a mixture of 100, 150 and 200 dia reticulation pipes made of asbestos cement, cast iron, PVC and steel.

### 2.8.2 WASTEWATER

Wastewater infrastructure general to the Ascot area is serviced by gravity style wastewater drainage infrastructure. A mixture of concrete and plastic arterial pipes on grade service all areas to local pump stations throughout the City of Belmont.

Lots within, and surrounding, the subject land are serviced by two main arterial sewer routes; a 225mm collector flowing north to south and a 225mm collector flowing east to west. Both collectors flow to the Redcliffe Pump Station 5 located on Stoneham Street. The Redcliffe Pump Station 5 collects all sewerage west of the Ascot Racecourse within the Ascot suburb and discharges it to the Redcliffe Pump Station 2 located on Abernethy Road.

### Golden Gateway | Structure Plan

### 2.8.3 POWER SUPPLY

Data obtained from the Western Power Network Mapping Tool indicates that the subject land is serviced by the Belmont Substation and the forecast network capacity for 2015 is >30MVA. There are High and Low Voltage power lines in the vicinity of the subject land.

### 2.8.4 GAS SUPPLY

Correspondence from ATCO Gas identifies Medium Low Pressure (MLP) gas mains (pressure indicated at 70kPa) along the majority of roads within the subject land.

## 2.8.5 TELECOMMUNICATIONS

The subject land is well serviced by telecommunications infrastructure with optical fibre running in or adjacent to the subject land. This infrastructure is owned by various telecommunications providers including Telstra, Optus and others. The National Broadband Network (NBN) has yet to be rolled out in the subject area. However, NBN Co has advised that fibre to the node (FTTN) technology rollout has been planned for October-December 2017.

Golden Gateway | Structure Plan

## 3 STRUCTURE PLAN

## 3.1 VISION AND OBJECTIVES

#### 3.1.1 VISION

The objectives and design principles underpinning the Golden Gateway Structure Plan have been formulated around the following vision:

"The development of the Golden Gateway will transform this degraded and fragmented area into a vibrant precinct of residential and mixed use development, with strengthened connections to the Swan River and Ascot Waters, that derive best value from these attributes while respecting the area's rich culture and heritage."

#### 3.1.2 OBJECTIVES

The overarching objectives for the Golden Gateway Precinct as established by the project team and reinforced through stakeholder engagement are as follows:

- Improve self-containment of facilities reduce car dependence
- Improve people's connection to the Swan River

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- Create accessible, quality public realm within the precinct
- 4. Ensure **heritage values** are retained
- 5. Identify appropriate uses/densities in conjunction with infrastructure improvement
- 6. Optimise value of strategic sites planning certainty

## 3.2 DESIGN PREPARATION

The Structure Plan design has been informed by a thorough analysis of the existing site conditions and the potential opportunities and issues offered by the location. The key outcomes of this analysis are noted in **Figures 8 and 9** and described overleaf:

### 3.2.1 OPPORTUNITIES

#### Land use

- Opportunity for residential development to be accommodated in the precinct given the accessibility to high amenity riverside amenity.
- Opportunity for retail convenience and food and beverage land uses to be integrated into development outcomes.
- 3. Potential for higher density development given precinct location, proximity to high amenity open space destinations, Perth CBD, localised employment and high frequency public transport.
- 4. Existing primary school adjacent the precinct offers opportunity to attract a diverse demographic, including young families.
- Consider mixed use development in core area to broaden activity opportunities and long term transition of the precinct, and to offer improved amenities for the existing Ascot community.

#### **Built form**

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Opportunity for landmark building form and massing to inner core areas to perform key gateway functions.

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- 2. Future building form to appropriately interface with adjacent public realm.
- 3. Local activity hub potential within the precinct providing local centre retail, cafe/mini main street offerings in a shared street atmosphere.

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 Existing street block depths south of Resolution Drive are well suited for typical multiple dwelling apartment development parcels.

#### Public realm

- Existing character and destination status of adjacent Swan River open space provides significant public amenity and recreation opportunities for future residents.
- Promote pedestrian and cycle network connectivity through the site to strengthen access to the Swan River for both the existing Ascot community as well as future residents in the Golden Gateway Precinct.
- Significant tree canopies within the Belmont Trust Land and peripheral open space offer significant 'green horizon' views to the precinct.

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- Opportunity to provide strong open space 'cross-link' as a 'green ribbon' link to the Swan River.
- Celebrate the heritage significance of the Ascot Kilns and the potential for integration of the heritage structures to maximise amenity for residents.

#### Movement

- Utilise existing local street network of Hargreaves Street, Daly Street and Grandstand Road to deliver a robust structure for future development access and vehicle circulation.
- Generous existing road reserve dimensions provide ability for reconfigured pedestrian friendly streetscapes offering shade trees, soft landscaping and convenient on-street parking embayments.
- Potential for alteration to the priority road network of Stoneham Street and Resolution Drive for the benefits of precinct consolidation and integration, in particular, the potential to downgrade priority of Stoneham Street for benefits to foster a stronger relationship between the Ascot community and the Swan River.
- 4. Investigate alternative road alignments that celebrate key view lines of surrounding visual features and future gateway elements.

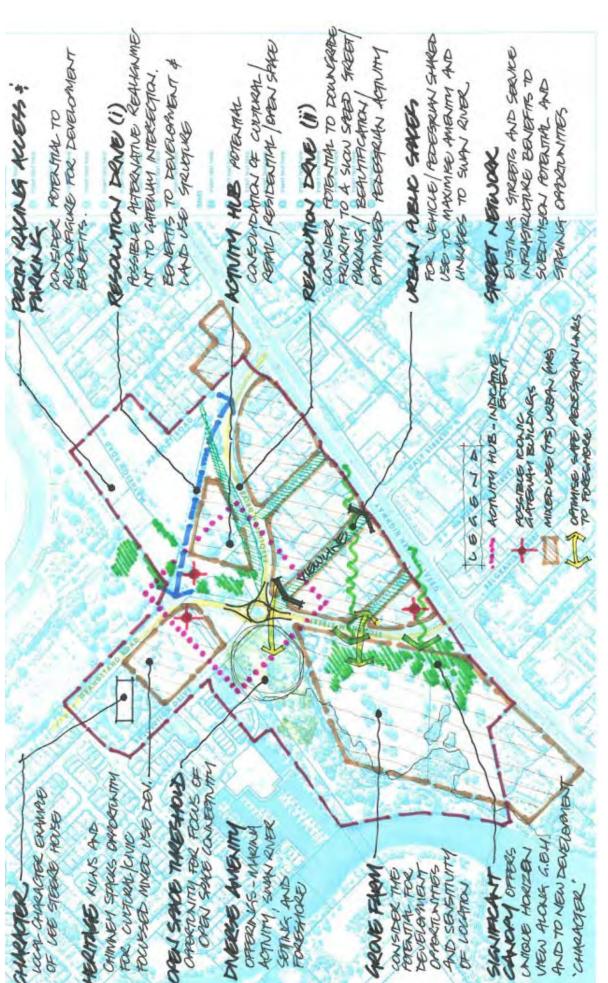


Figure 8 - Opportunities

## 3.2.2 ISSUES AND CONSTRAINTS

#### Land use

- 1. Service corridor extends northwards through Grandstand Road alignment.
- Overland stormwater drainage, controlled by Water Corporation, extends eastwest through the subject land located immediately north of Resolution Drive.
- Careful consideration of existing residential development on periphery of precinct area
- 4. Development adjacent Great Eastern Highway may be subject to noise attenuation.

#### **Built form**

- 1. Proposed development of Kilns area, which is subject to a Local Development Plan, to be considered in surrounding built form design.
- 2. Perth Airport restrictions based on flight path contours will potentially limit maximum building height.
- 3. Existing development is largely commercial and is located on a fractured land tenure base of multiple cadastral parcels.
- 4. Some future development may require land assembly to maximise development potential and desirable outcomes, and to rationalise redundant public reserves.

#### Public realm

- Chimney locations in the Ascot Kilns area to be considered, surrounding public spaces and view lines should respect and celebrate these historic features.
- Existing significant trees to be considered for integration into public realm, where appropriate.
- Informal open space node to Hardey Road (east) to be considered, recognising relative disconnection of this area from other POS to the south of Resolution Drive.
- 4. Limited or no availability of suitable quality water from the superficial aquifer for the purpose of irrigation within the Golden Gateway area.

#### Movement

- Existing roundabout impinges on precinct assimilation for all adjoining land quadrants.
- 2. Limited connection opportunities available to residents north of Resolution Drive.
- Stoneham Street and its multi-lane configuration acts as a pedestrian barrier for development to interact with the POS area.

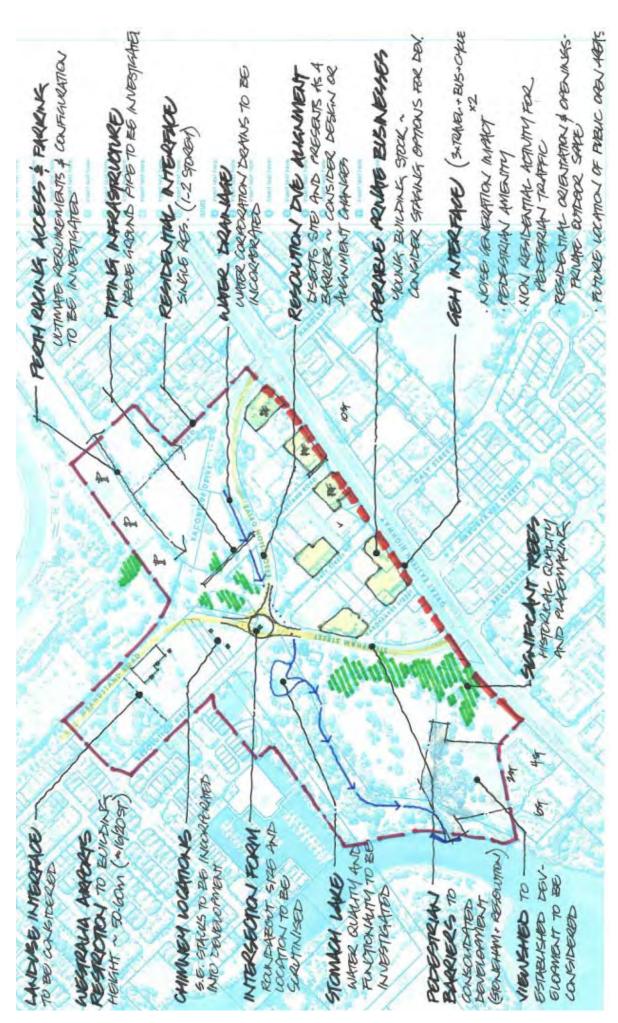


Figure 9 - Issues and Constraints

# 3.2.2.1 CONCEPT PLAN DEVELOPMENT – SITE ANALYSIS

One of the main challenges in testing development scenarios was to address the significant disunification of the precinct created by the heavily engineered road system, and the impact this has on local connectivity between the Precinct and the areas main natural attribute – the Swan River

**Figures 10** and **11 below** were produced to stimulate discussion, during the stakeholder engagement process, about ways in which the physical barrier to the Swan River could be removed, or at least, reduced. The stakeholder engagement process produced a number of specific considerations for the initial design phase to develop scenarios (refer **Figure 12**).

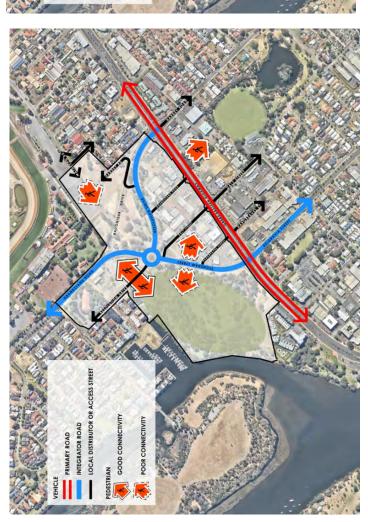


Figure 10 - Existing access and connectivity summary



Figure 11 - Opportunity through altered vehicle priority for improved connectivity and access – to be considered further in design scenario testing

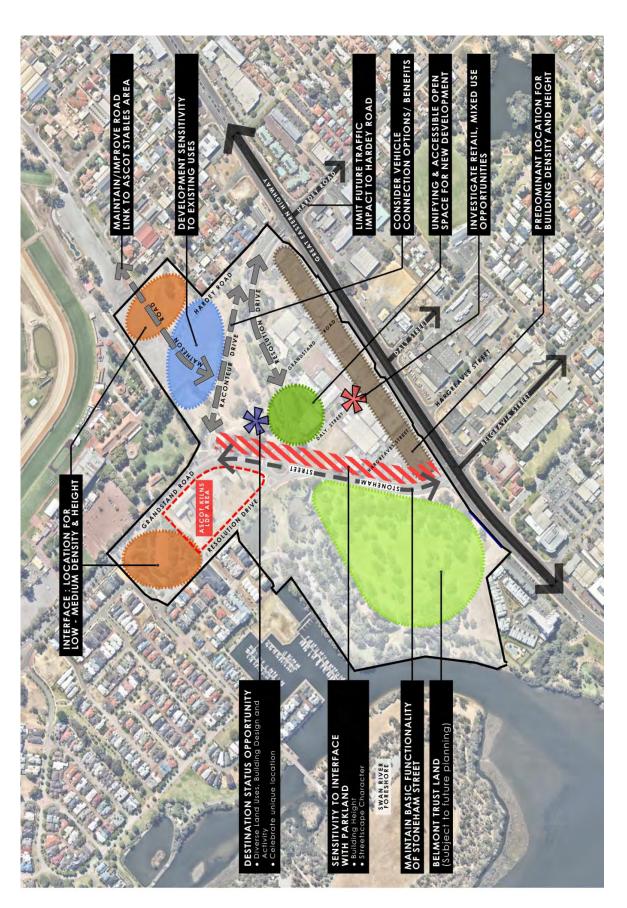


Figure 12 - Community engagement design feedback (summarised)

## 3.2.3 CONCEPT SCENARIO FORMULATION

Analysis of the subject land and key design principles resulted in the preparation of three development scenarios for testing and stakeholder discussion (refer **Figure 13**). The initial phase of high level scenario testing involved the preparation of Framework Diagrams, exploring structural opportunities and benefits to the following:

- Landuse preferred structure and location.
- Circulation enhancing connections, preferred hierarchy with future flexibility & rigour.
- **Character** celebrating local qualities for unique place setting and to enhance the existing amenity.

# Preliminary sketches exploring alternative land use and movement structures

The Framework Diagrams were discussed and analysed with the community and Council technical officers as key elements for the next phase of scenario evaluation. Various scenarios achieved the project objectives better than others, particularly with regard to public amenity and community integration with the Swan River foreshore.

## Preliminary sketches developing framework scenarios

The next phase of scenario refinement resulted in the examination of appropriate land uses, building forms and public realm to test the structural opportunities and benefits for each of the scenarios. These were then evaluated by the project team and the community via design workshops and web based consultation sessions.

### Preliminary design scenarios

In summary, Scenario A evaluates the development opportunities for the precinct whilst maintaining the existing road network. This scenario highlights the limitations this has on development consolidation and for connectivity of future residents with the foreshore amenity.

Scenario B evaluates the development outcome where the existing road priorities of Stoneham Street and Resolution Drive are modified to improve integration of the precinct's residents with the adjacent public amenity.

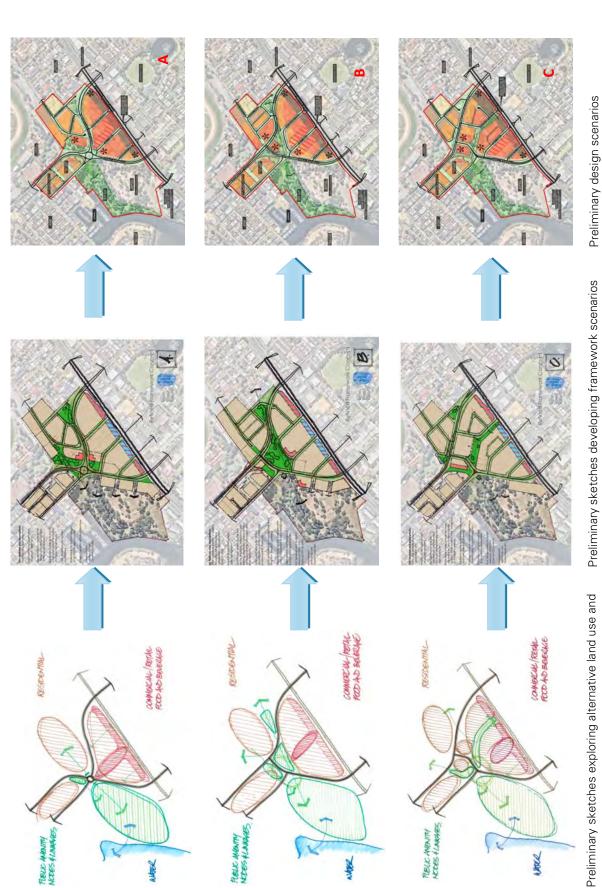
Scenario C evaluates an outcome where the original road alignment of Raconteur Drive is used to maximise future integration opportunities for development west of this road and consolidation of the precinct's future residents.

### Scenario evaluation outcome

This scenario evaluation process led to the refined design outcome produced in the preferred scenario.

The preferred scenario was informed by detailed public response to the preliminary scenarios at the community workshops, and through other stakeholder contribution. That preferred scenario was further tested and developed into the preferred Golden Gateway concept, described in detail in section 3.2.4.

It should be noted that through the preparation of the Structure Plan, further assessment of the proposed movement network was undertaken in relation to the potential impacts on the Stoneham Street-Belgravia Street and Resolution Drive-Hardey Road corridors. Based on this assessment and in conjunction with MRWA, it was considered that any modifications to the redistribution of traffic flows (i.e. via Resolution Drive) would not be supported.



Preliminary sketches exploring alternative land use and movement structures

Figure 13 – Evolution of Design Scenarios

## 3.2.4 DEVELOPMENT CONCEPT PLAN

In addition to the formal Structure Plan figures included in Part 1 – Implementation, a Development Concept Plan has been prepared to provide an illustration of the development intent (**Figure 14**). This graphical representation is indicative only and reflects a long term, mature development scenario; however, it gives an indication of how the public spaces may be developed and the relationship of the public/private interface between the public spaces and new residential areas.

#### Key concept features

The key features of the Development Concept Plan are outlined as follows:

#### Access and connectivity

- Improve connections within the precinct, prioritising green links to the Swan River and Belmont Trust Land.
- Integrate Golden Gateway with the broader Belmont catchment.
- Minimise the barrier of Stoneham Street by formalising pedestrian movement opportunities.
- Enhance vehicle accessibility and excellent circulation benefits offered by the existing movement framework.
- Reconfigure road network for enhanced development consolidation and precinct character benefits.

#### Planning and land use

- Sensitively integrate residential development of increased density with the surrounding area.
- Convenience retail, shops, restaurants and cafes located 'parkside', and within a
  pedestrian friendly street environment, to take advantage of the unique amenity and
  population growth of the location.
- Moderate building height and density to the residential interfaces of the precinct, providing an appropriate transition to existing development.
- Respect identity of existing equestrian residential precinct.

- Provide flexibility for commercial mixed-use development along Great Eastern Highway.
- Contemplate development controls to foster appropriate multi-level development to support denser living options.
- Opportunity for diversification of uses facilities, amenity, destination uses and attractions.

#### **Built Form**

- The height and scale of new buildings will form an appropriate relationship with their environment and context, including adjacent residents.
- Use building form to create a more comfortable and characterful environment, enhancing the gateway location, particularly adjacent the public realm.
- Consider suitable building form and locations to enhance the precinct's outcomes.
- Retail opportunities promoted for improved sustainability outcomes of the precinct and adjacent residents.

#### Public realm

- Enhancement of existing public streets, utilising the wide reserve widths to produce unique character and pedestrian comfort.
- A central high amenity open space spine, providing a principal access point to the foreshore park and future local scale recreation opportunities.
- Enhancement of pedestrian and cycle accessibility and amenity through green linkages.
- Prioritise the retention of established tree canopies where achievable.

#### **Destination Planning**

- Capitalise on the opportunity to leverage subject land's exceptional destination qualities.
- Creation of framework / strategies to support detailed place planning, investment attraction and place management.
- Creation of framework / strategies that will attract a diverse mix of uses, attracting visitors across different times of the day and week.



Figure 14 - Development Concept Plan



Above: Introduce transitional building height to development edges.



Above: Corner site development addressing both street frontages, with 3 storey podium height to building edges and mixed height elsewhere on site.



Above: Example of a 4 storey residential building detailing an appropriate level of articulation and surveillance through the use of balconies and architectural elements. Also illustrates an acceptable treatment to site retaining at lot edges.



Above: Example of 5 storey mixed use building featuring retail/food and beverage uses at the ground level and residential living above producing a sustainable and active development outcome. In addition, this illustrates the beneficial outcome for buildings to interact with key mature trees available within the proposed Golden Gateway public realm.

Images: Building Form Inspiration Images







Left: A 15 storey buildings providing an outstanding response to its comer location. Right: A 8 storey building examples incorporating desirable podium design and setback to tower element(s).



Above: Landmark buildings providing exceptional architectural gateways into the Golden Gateway precinct.



Above: Example of appropriate response to podium requirements to achieve active and enjoyable streetscapes with building mass setback into the site.

#### 3.3 LAND USE

The Golden Gateway Precinct will provide for a diverse range of land uses. The primary land use within the Structure Plan Area is residential, supplemented by commercial uses and local open space. A summary of the land uses and areas is provided in **Table 3**.

TABLE 3: LAND USE

Zone / Reserve	Area (Ha)
Mixed Use	9.4742
Residential	3.6961
Parks and Recreation	4.5556
Water Supply Sewerage & Drainage	0.4409
Local Roads	6.3868

As outlined in Part 1 and **Plan 2**, the subject land has been divided into precincts to acknowledge their differing characteristics in terms of:

- Street character;
- Land use mix; and
- Development scale

A statement of intent for each precinct is described in Part 1 together with development standards to ensure that the intent of the precinct is achieved.

### 3.3.1 RESIDENTIAL

The Structure Plan provides for a variety of housing choices through the designation of a range of medium to high density R-Codes (R20-R-AC0).

Higher density R100 and R-AC0 coding is located within the 'heart' of the development and within close proximity to high amenity areas such as POS and areas of activity such as Great Eastern Highway, Stoneham Street and Resolution Drive. Low to medium density is proposed for development on the periphery of the subject land to ensure an appropriate transition towards the existing residential areas surrounding.

The R-AC0 coding has been applied to all land within the Mixed Use zone. State Planning Policy 3.1 – Residential Design Codes (SPP 3.1) provides for development standards to be set out in LDPs. For the Golden Gateway Precinct, it is proposed that more detailed development standards pertaining to the Mixed Use zone will be specified in Design Guidelines to be adopted as a LPP by the City of Belmont in addition to the standards set out in Part 1 – Implementation.

Part 1 – Implementation stipulates that there is no maximum plot ratio applicable within the Mixed Use zone. Instead, matters such as bulk and scale will be influenced via other elements including setbacks, building envelope, streetscape interface, private open space and building height, which will be addressed in Design Guidelines where not outlined in Part 1 of this Structure Plan.

## 3.1.1 DWELLING PRODUCT TYPE, MIX AND YIELD

It is envisaged that the Golden Gateway Precinct will accommodate primarily multiple dwellings to contribute to the desired scale and density of the development. Opportunity to provide grouped and single dwelling development will also exist at the periphery, where the precinct adjoins existing low to medium density residential housing as reflected in the development intent of the precincts identified on **Plan 2**.

The estimated yield is indicative only, based on the build-out potential under the Structure Plan. With respect to multiple and grouped dwellings, the ultimate yield and product mix will be determined by the type of development pursued by proponents and will be subject to the market conditions at the time, although the Structure Plan does impose minimum development parameters (for setbacks and heights) as well as maximums. The ultimate yield and product mix will be determined during the construction and development phase.

The Development Concept Plan suggests a potential yield of at least 3,000 dwellings. This could accommodate a total population of up to 5,400 assuming an average household size of 1.8 people.

#### OTHER LAND USES 3.3.2

#### COMMERCIAL 3.3.2.1

Commercial development in the Golden Gateway Precinct will support the surrounding residential catchment and racing activities and optimise the value of the precinct's highly visible and connected location. The anticipated yield for the precinct estimates a total of 7,400m<sup>2</sup> commercial (non-retail) floorspace (GFA). It is envisaged that commercial activity will be mostly focused within the Great Eastern Highway Precinct (Precinct 1) and will likely occupy the first 1-2 levels of buildings across the precinct Development of commercial space is only likely to proceed based on its commercial feasibility and the prevailing market conditions at the time of development. In order to foster the progressive and timely development of the precinct, it is not intended that commercial uses will be mandated within the Mixed Use areas; however ground level design should be adaptable to enable land use to change over time. This requirement should be addressed in the Design Guidelines.

#### RETAIL 3.3.2.2

The existing residential areas of Ascot Waters and the stables area presently suffer a Drive and Great Eastern Highway providing the only nearby outlet for basic convenience ack of local shopping facilities, with the BP Service Station on the corner of Resolution items. Development of the Golden Gateway Precinct provides an opportunity to establish a local centre for the benefit of the precinct as well as the broader local catchment.

development. Daly Street will be lined with retail and food and beverage opportunities to create a high level of activity. A mixture of land uses is encouraged to support the Daly Street has been identified as the 'Main Street' and retail precinct for the proposed Food and beverage will also be encouraged along the linear open space, however the precinct including; residential, retail, food and beverage, entertainment and commercial. achievement of this will be subject to viability.

Waters and development south of Great Eastern Highway, the retail precinct will be an active and vibrant place which will attract local residents as well as passing trade from Great Eastern Highway. A Retail Needs Analysis was undertaken by Colliers in support of the Ascot Kilns LDP. This analysis suggests that each additional 250 apartments will equate to the ability to support approximately 80m2 of additional retail floorspace. As such, in addition to what has been identified for the Ascot Kilns area, and based on the indicative yield analysis undertaken, it is envisaged that the Golden Gateway Precinct would support a local centre (including small supermarket) in the order of 1,500m2 of Located within proximity to Great Eastern Highway, the Ascot Kilns development, Ascot retail floorspace (GFA).

Over and above this, it is anticipated that additional retail floorspace could be supported through passing trade from Great Eastern Highway, given the attractive position and good access. The 'Main Street' precinct has been located to provide a high level of accessibility from the wider residential catchment, passing traffic along Great Eastern Highway and Stoneham Street and residents from within the Golden Gateway Precinct. The 'Main Street' (Daly Street) will be a wide, well landscaped, street providing a high level of amenity for users and is situated along a main pedestrian route between Great Eastern Highway to the core of the development precinct.

The Development Concept Plan envisages ground level retail / food and beverage uses to create an activated edge to the public realm. Above ground commercial and residential uses will also be encouraged.

#### **UNACCEPTABLE LAND USES** 3.3.3

Part 1 - Implementation of this Structure Plan refers to corresponding zones within the notwithstanding that they are listed as a discretionary use in the Zoning Table). Having It does, however, stipulate some exclusions (uses that are considered Unacceptable, Zoning Table of LPS 15 to determine land use permissibility within the various precincts. regard for the amenity of future residents the unacceptable uses include:

- **Auction Mart**
- Caretakers Dwelling
- Fast Food Outlet / Lunch Bar

- Home Store
- Garden Centre
- Industry Light
- Motor Vehicle Repair
- Night Club
- Radio or TV Installation
- Restricted Premises
- Service Station
- Single House (with the exception of Precinct 7)
- Vet Hospital
- Warehouse

These uses have been excluded as they are considered to be inconsistent with the vision and objectives of the Structure Plan, and approval of such uses would compromise the urban fabric envisaged for the area.

### 3.3.4 BUILDING HEIGHT

Maximum building height limits apply to satisfy relevant protection of airspace, airport facilities and surfaces regulations due to the proximity of Perth Airport. Development must comply with maximum building height limitations as indicated on the Obstacle Limitations Surfaces (OLS) Ultimate Surfaces Map – maximum height of 61mAHD within the majority of the subject land, equating to approximately 19 storey buildings. The remainder of the subject land is located within the 'conical surface', being the 5% slope to 61mAHD.

A maximum building height of 15 storeys is encouraged along Great Eastern Highway given the prominence of this location and level of commercial activity envisaged for this

In order to ensure development is built to a sufficient scale to facilitate the density envisaged for the Golden Gateway Precinct, and to achieve the desired urban design outcomes, it is also considered appropriate to set minimum building heights. Priority should be given to the relationship of ground floor uses and building design with the public domain to ensure that considerations such as activation, passive surveillance and appropriate combination of uses are optimised.

Podium development is mandated at specific locations as identified on **Plan 3** (Part 1) to create an urban experience at the street level whilst optimising development opportunities. A maximum podium height of 3 storeys applies (no minimum) unless where associated with a landmark site, in which case a maximum podium height of 5 storeys applies. Podium elements are encouraged to relate to and activate the street, with the levels above the podium to be sufficiently setback.

Compatible building heights and scales have been provided along the interface between the existing Ascot Waters and 'Stables' residential areas and the subject land, with built form positioned to minimise overshadowing and amenity impacts on the adjacent existing residential development.

Minimum and maximum building heights for podium and tower elements across the subject land are shown on **Plan 3** (Part 1)

### 3.3.5 LANDMARK SITES

There are a number of key locations situated at the termination of key view lines and sites highly visible from outside of the Golden Gateway Precinct, thereby acting as landmarks for the development. These sites will also act as key nodes located along important pedestrian movement connections and will assist in linking these sites with the public realm, particularly the central POS area.

Landmark sites have been strategically distributed throughout the subject land as shown in **Plan 3** taking into consideration overshadowing impacts and amenity considerations. In this regard, higher buildings are located at key corners of Great Eastern Highway and Stoneham Street and Resolution Drive and sites at key locations along these streets internal to the precinct with an additional 5 storeys permitted.

Landmark sites should also be designed incorporating architectural or sculptural features with a point of difference. This will be a requirement of the future Design Guidelines.

Additional height (up to 5 storeys) may be permitted on landmark sites, subject to compliance with specific design criteria (refer Part 1 – Implementation, Section 5.1.1.3). Proposals involving additional height will also need to conform to the Perth Airport OLS requirements. It is possible to have some incursion of the OLS provided that approval is granted to operate with appropriate risk mitigations measures in place.

https://www.perthairport.com.au/Home/corporate/planning-and-projects/airspace-<u>protection</u> or the Department of Infrastructure and Regional Development at: https://infrastructure.gov.au/aviation/safetv/protection/index.aspx

### 3.3.6 CAR PARKING

The City wishes to encourage innovative approaches to car parking provision, such as reciprocity, carpooling programs or other innovations, that may result in reduced parking provision where appropriate. In this respect, the Structure Plan provisions will enable the Responsible Authority to consider approving a reduced parking provision where it can be demonstrated that an alternative parking proposal is sound and will result in a reduction in parking demand. Any proposed variation should be supported by a parking demand assessment undertaken by a suitably qualified professional.

An integrated approach to parking provision will be encouraged within Mixed Use and Multi-unit development, in order to make the most efficient use of parking provision and to encourage use of alternative (public) transport modes where appropriate. In this respect special provisions are proposed to challenge the 'business-as-usual' approach to car parking design. In terms of transit-based parking provision, the proposed approach follows the recommended parking provision in the draft Design WA "Apartment Design" (Draft for public comment - WAPC, October 2016). The following specific requirements are to be applied:

- dwelling, and at least 50% of the minimum required parking for non-residential uses shall be made available for general use of either residential or non-residential uses (these bays represent unallocated communal parking bays).
- b) Mixed Use development that proposed parking as outlined in 2a) above should be required, as a condition of Development Approval, to prepare a Car Parking Strategy that addresses the management of the unallocated communal parking provision, including:
- The hours during which parking bays shall be made available for general public access; and

- Location, signage and monitoring of usage of the unallocated communal parking bays.
- c) For multiple dwelling residential development, parking requirements shall be as follows:
- I. Minimum parking: in accordance with **Table 4**;
- II. Maximum parking: not to exceed double the minimums specified in Table
- 4

## TABLE 4: MULTIPLE DWELLING RESIDENTIAL PARKING

Location B	1 bay per dwelling	1.25 bays per dwelling	
Location A	0.75 bay per dwelling	1 bay per dwelling	1 bay per 4 dwellings up to 12 dwellings 1 bay per 8 dwellings for the 13th dwelling above
Parking Types	1 bedroom dwellings	2+ bedroom dwellings	Visitor Parking

Definitions:

- Location A: Within 250m of a high frequency bus route, measured in a straight line from along any part of the route to any part of the lot.
  - **Location B:** Not within Location A.
- d) The provision of car parking that is in excess of the minimum required for the site will only be approved where it is designed to be adaptable for future conversion into habitable floor space, or other useable space communal or private usage. In order for parking to be considered adaptable, it must be shown as located in a position that is suitable for an alternative use, not included in individual strata titles and constructed to comply with habitable floorspace standards.

This requirement may be waived if it can be demonstrated that complying with the requirement would not be practical or would result in a less desirable

## 3.3.7 PUBLIC OPEN SPACE

The total POS provision is commensurate with the composition of land uses and having regard to the surrounding site context.

The central Linear Park POS area ('Green Link') will be the focal point of the proposed POS provision and forms an integral part of the pedestrian network and will act as a 'green corridor' and wayfinding link for the Precinct. This POS area will provide direct pedestrian connections between the Swan River / Belmont Trust Land, proposed local park along the northern boundary and other key locations within the Golden Gateway Precinct

These POS areas are located within the walkable catchments of each residence; providing only a short walking distance for all residents to passive recreation areas. The POS will be linked by the permeable road, shared path and footpath networks and will provide attractive end views for the connecting streets.

It should also be noted that the subject land is well located within an existing urban context comprising of significant public parkland associated with the Swan River and portion of the Belmont Trust Land provided for public recreational value and the proposed POS areas will act as an extension of this existing amenity value.

A POS calculation has been prepared in accordance with Liveable Neighbourhoods (LN), as detailed in **Table 5**. A total of 0.6974ha of Open Space is provided on the Structure Plan of differing forms and functions as detailed on **Figure 15**.

In the case of mixed use development, there is no minimum requirement for the provision of POS under LN. LN states that the appropriate POS contribution for mixed use development will be determined by the WAPC on a case by case basis. A total POS provision of 3.47% is provided and will be refined at the subdivision stage. As the City is likely to manage the process of land assembly and subdivision it will be primarily responsible for defining the final area of POS, with the other relevant stakeholders (including WATC and Water Corporation).

This proposed provision is less than the standard POS requirement of 10% POS for residential development under LN, however the proposed provision is considered appropriate for a mixed-use precinct. The City's POS Strategy also sets out minimum standards of land area provision for POS based on current best practice and ease of accessibility to available open space for both residential and non-residential areas. The subject land falls within the Ascot study area of the Strategy which concludes that whilst active open space provision is considered low, the area is well equipped for passive recreational activities largely as a result of the Regional Open Space associated with the Swan River foreshore to service its local needs.

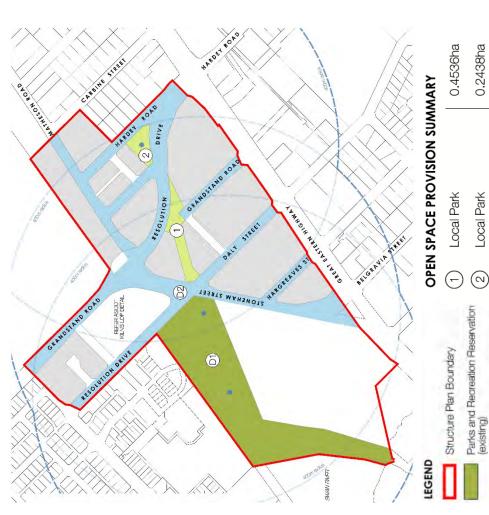
Consistent with the assessment provided in the City's POS Strategy, the subject land is well located within an existing urban context allowing the future residents to take advantage of a variety of established recreation and leisure opportunities associated with the nearby Swan River and environs. The extent of the POS provision in this instance is primarily an outcome of the concept to incorporate the existing Regional Parks and Recreation reservation (which is technically not included in the calculation of POS provision) into the design and augment its value by surrounding it with useable POS to create a central linear park at the heart of the development.

The POS to be provided in accordance with the Structure Plan and the POS Schedule and will be landscaped to a standard commensurate to, or above, LN requirements and to the satisfaction of the City of Belmont.

53

TABLE 5: PUBLIC OPEN SPACE SCHEDULE

c Open Space Required @ 10%  OVISION  Se  0.4556  0.4536  0.2438  stricted Public Open Space	PUBLIC OPEN SPACE		
cs and Recreation Reservation (existing)  Id reserves (existing)  Deductions  Gross Subdivisible Area  Gross Subdivisible Area  Creditable Public Open Space Required ® 10%  C OPEN SPACE PROVISION  icted Public Open Space  Total Unrestricted Public Open Space  Total Unrestricted Public Open Space	Gross Site Area		30.9284
to and Recreation Reservation (existing)  Deductions  Gross Subdivisible Area  Gross Subdivisible Area  Creditable Public Open Space Required @ 10%  Icted Public Open Space  Total Unrestricted Public Open Space  Total Unrestricted Public Open Space	DEDUCTIONS		
Deductions  Gross Subdivisible Area  Gross Subdivisible Area  Creditable Public Open Space Required @ 10%  C OPEN SPACE PROVISION  ricted Public Open Space  Total Unrestricted Public Open Space  Total Unrestricted Public Open Space	D1 Parks and Recreation Reservation (existing)	4.5556	
Creditable Public Open Space Required @ 10%  C OPEN SPACE PROVISION  icted Public Open Space  Total Unrestricted Public Open Space  Total Unrestricted Public Open Space	D2 Road reserves (existing)	6.3024	
Creditable Public Open Space Required @ 10% C OPEN SPACE PROVISION  icted Public Open Space  Total Unrestricted Public Open Space  Total Unrestricted Public Open Space	Total Deductions		10.8580
Creditable Public Open Space Required @ 10%  C OPEN SPACE PROVISION  ricted Public Open Space  10.4536  Total Unrestricted Public Open Space  Total Unrestricted Public Open Space	Gross Subdivisible Area		20.0704
ricted Public Open Space  Total Unrestricted Public Open Space  Total Unrestricted Public Open Space	Creditable Public Open Space Required @ 10%		2.0070
ricted Public Open Space  0.4536  Total Unrestricted Public Open Space	PUBLIC OPEN SPACE PROVISION		
Total Unrestricted Public Open Space	Unrestricted Public Open Space		
Total Unrestricted Public Open Space	POS 1	0.4536	
stricted Public Open Space	POS 2	0.2438	
	Total Unrestricted Public Open Space		0.6974
	Restricted Public Open Space		Z
TOTAL CREDITED PUBLIC OPEN SPACE 0.69	TOTAL CREDITED PUBLIC OPEN SPACE		0.6974
PERCENTAGE OF PUBLIC OPEN SPACE PROVIDED 3.47	PERCENTAGE OF PUBLIC OPEN SPACE PROVIDED		3.47%



в [74 Park Walkable Catchment 400m radius

Deduction identification portion

Road Reserves (existing)

	Local Park	Parks and Recreation Reservation (existing)	Road reserves (existing)	, OO
)	(2)		03	5

Public Open Space and Water Supply and Drainage (proposed)

4.5556ha

6,3024ha



Figure 15 - Open Space Provision

## 3.3.8 PUBLIC REALM PROVISION

A Public Realm Strategy was prepared in support of the Structure Plan (refer **Appendix F**) to develop a clear vision, principles and objectives to inform development of the public realm. A landscape masterplan has also been developed as a component of this Strategy to provide an indicative graphical representation of how the key public space areas may be developed (refer **Figure 16**).

The subject land comprises a number of different public realm space types ranging from the strong east-west Linear Park ('Greenlink'), boulevard high-use roads to local streets as shown on **Figure 16**. A cohesive approach across the public realm will consist of an urban landscape that reinforces a fluid and flowing spatial arrangement extending from the river parklands and extending this character throughout the subject land. The creation of smaller pockets of activity and open space will be defined by street trees, tree groups and sinuous tree lines. Pedestrian spaces will be sheltered by a substantial tree canopy and vehicular routes flanked by boulevard plantings. A unified paving design and materials for pedestrian areas will extend throughout the subject land extending down streets and through the central Linear Park. This will both unify and delineate the different pedestrian and vehicular spaces.

To reduce maintenance and water consumption, where possible, consideration should be made as to the use of hard surfaces or low water alternatives instead of turf. Water harvesting of hard surfaces is also exploited where possible using swales, channels and ground amendments to reduce the need for overall water consumption.

The key public realm areas are set out in the following pages.





Examples of Public Art, Rain Gardens & Swale Designs in an Urban Context (Jolimont Parkside



Figure 16 – Landscape Master Plan

The Linear Park ('Greenlink') is a valuable community asset which has great potential to be the focus of community use, a meeting place and the primary pedestrian movement corridor linking the subject land with the Swan River and associated parklands to the west (refer **Figure 17**). The Linear Park is an urban park relying on tree canopies to provide shade and 'softness' to the urban space. The size of paved areas should be able to accommodate potentially large numbers of users including cyclists, skaters, and pedestrians all within a network of footpaths linking into the surrounding road network and building entrances. The space will create a seamless and comprehensive character that embraces built form and is unaffected by the rigidity of traditional street infrastructure.

Linear Park 'Greenlink'

The informality of parklands will envelope development forming a new dynamic setting that is capable of providing informal meeting spaces, alfresco spill-out, community meeting places and potentially facilities such as active recreational spaces, outdoor fitness trail equipment and interactive sculptural elements as public art. The space will facilitate clear passive surveillance from the lower levels of buildings and will be well lit at night. The open and broad nature of spaces providing safe pedestrian circulation and sight lines, lighting and activity will enable the space to accommodate the needs of a growing local population.

Importantly, this space and its robust form diminishes the role of vehicular traffic aesthetically creating a dominance of pedestrian orientated space.



Figure 17 - Linear Park Indicative Concept

### Road and street treatments

Road hierarchies and overall legibility of the subject land will be reinforced by the type of tree planting associated with the scale of the road. The paving treatments within all streets and roads will be consistent with the material palette of the Linear Park, reinforcing a distinctive character of this place.

#### **Resolution Drive**

median planting will create a formalised sinuous corridor of canopy trees that are species, creating a canopy boulevard along its length (refer Figure 18). Verge and trees will be planted to create a boulevard aesthetic the length of the street, aiding in The landscape aesthetic of Resolution Drive will be dominated by tree planting of larger recognisably different to the scale and nature of other landscapes in the area. Like street wayfinding.



Figure 18 - Resolution Drive Plan Extract and Indicative Cross-Section

#### Stoneham Street

Stoneham Street will be identified by a boulevard of planting comprising species related to the adjoining Belmont Trust Land such as a mix of natives and introduced species emphasised at junctions and the key pedestrian crossing points (refer **Figure 19**). The boulevard will accommodate a key pedestrian connection that extends through to Matheson Road via the Linear Park.



Figure 19 - Stoneham Street Plan Extract and Indicative Cross-Section

## Hargreaves Street and Grandstand Road

Hargreaves Street and Grandstand Road will comprise street tree planting that is not a monoculture but uses a mix of street trees (refer **Figure 20**) in varying combinations to provide a dynamic and varied street tree canopy. The mix will create a character that is related to, but distinguished from, Daly Street, emphasising the different nature of the space. These streets will extend the overall public realm character established within the Linear Park and central portion of the site but in a simpler manner. Street tree planting is proposed to create a canopied streetscape and to be positioned abutting the parallel parking embayments.



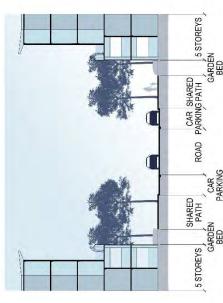


Figure 20 - Hargreaves Street Plan Extract and Indicative Cross-Section

#### Daly Street

Daly Street is proposed to function as the 'main street' and as such, the public realm has been configured to respond to retail uses (refer **Figure 21**). The pedestrian pavement will be configured to minimise clutter and encourage possibilities for alfresco seating. Importantly the paving design character established within the linear park extends through the street extending to the Linear Park. Tree groups will be used and located to define potential smaller public realm areas such as alfresco seating and informal gathering spaces. Car parking is configured at right angles to optimise numbers in support of retail and food and beverage uses within the 'main street'.



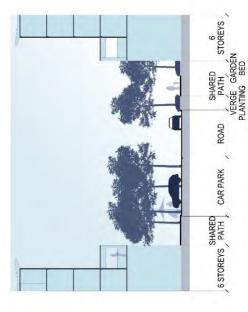


Figure 21 - Daly Street Plan Extract and Indicative Cross-Section

#### Local Streets

The streetscapes of the areas to the north and east of Resolution Drive will have a character that is dominated by street tree planting creating a heavy canopy (refer **Figure 22**). Street tree planting will consist of a variety of species that attain modest height but develop a broad canopy.

Raised paved tables can be used to provide traffic calming and to add texture to the urban streetscape reinforcing a character that promotes pedestrian safety. The selected paving treatments of local streets will change the character of streets especially in locations where separated pedestrian access is limited. All paving detail at junctions and associated with pedestrian circulation should address both the need to reduce traffic speeds, manage drainage and create a distinctive character.



Figure 22 - Local Streets Plan Extract and Indicative Cross-Section

## 3.3.9 LANDSCAPE DESIGN

## 3.3.9.1 DESIGN OBJECTIVES – AN URBAN LANDSCAPE

The public realm spaces made up of streets and the linear park combine to be a defining element of this location, that importantly the users, employees and residents experience and define the qualities of the public realm.

This location currently presents as a transient place that is passed through, however the design of the public realm will result in the creation of a cohesive network of spaces enabling the locality to be an identifiable place.

The overall landscape design objectives for the public spaces are set out below:

#### Identifiable character

- Create a contemporary urban environment that promotes safe and easy pedestrian experiences.
- Create new diverse urban landscapes that reflect the subject land's unique characteristics and close links to the river parklands.
- Create spaces that encourage and accommodate local community use and engagement.
- Establish an aesthetic that promotes positive development and investment in the location.
- Celebrate the heritage significance of the Ascot Kilns.
- Respect the social and recreational values of the Ascot Racecourse.
- Establish considered connections to the Ascot Kilns and the Ascot Racecourse in terms of tourism opportunities and amenity.

#### Valuable Landscapes

- Create a microclimate in public realm spaces and streets which encourages use and enjoyment.
- Provide visual connections to Belmont Trust Land through POS (as shown on Figure
- Provide key views and relationships that assist in orientation and legibility.
- Create highly utilised and valued public realm streets and spaces.

### Environmental/Sustainability

- Create a durable urban landscape.
- Reduce urban heat sink characteristics.
- Create urban tree canopy (in compliance with the City of Belmont's Urban Forest Strategy 2014).
- Retain vegetation wherever practical.
- Promote the use of low water demand plants.
- Pursue water harvesting, passive irrigation and integrated urban water management.

## 3.3.9.2 INTEGRATED DRAINAGE MANAGEMENT

The use and promotion of Water Sensitive Urban Design (WSUD) techniques and approaches are to be utilised wherever possible throughout the subject land. The space for nutrient stripping is limited. As the urban area is not producing a nutrient load, the focus is on slowing runoff and reducing hydrocarbons. The use of linear and incidental 'rain gardens' and 'nutrient sinks' can be implemented discretely within paving in streets and areas of open space. These devices should be fully integrated with the road drainage promoting passive irrigation of street tree vegetation and controlling hydrocarbon runoff.

Within the context of a dense inner urban area, the design of these WSUD devices need not be natural in appearance but can be incorporated within the urban public realm infrastructure as a contemporary feature.

It is intended that the east-west linear park, although containing broad pedestrian areas, will contain soft landscape areas that will accommodate local drainage that is managed through swale type structures that infiltrate water and passively irrigate trees and other vegetation used in the public realm. This will be subject to the Local Water Management Strategy (LWMS).

The use of permeable pavements and porous asphalt treatments in key locations is recommended, possibly associated with lower level threshold treatments of road junctions, should be incorporated as a component of the approach to integrated drainage management.

In order to deliver wider environmental sustainability objectives, as well as providing attractive places in which residents and visitors can enjoy, consideration should be given to the conservation of water resources and quality of groundwater. The use of water efficiency measures is encouraged and should promote the investigation of best management practices for irrigation of public open space.

The availability and quality of groundwater within the LSP area is limited at this stage. This will affect the ability of the City of Belmont to irrigate the proposed vegetation within the public realm areas. Therefore, due to the limitation of groundwater for irrigation purposes, the future irrigation of vegetation within the POS and public realm areas will need to be supplied by other sources. This may include scheme water, stormwater, irrigation (by agreement) from the Western Australian Turf Club's (now operating as Perth Racing) artesian groundwater licence, a new irrigation lake or other irrigation strategies will need to be investigated in the future. The City may encourage developers to consider the irrigation of abutting verge vegetation and street trees to ensure the high quality natural amenity of the public realm is maintained. Alternatively, non-irrigated (dry) landscape may need to be considered for the public realm areas.

## .4 MOVEMENT NETWORK

A Movement and Access Strategy was prepared by Flyt in support of the Structure Plan (refer **Appendix C**). This Strategy has been prepared using the requirements set out within the WAPC Transport Impact Assessment Guidelines (August 2016) Volume 2 – Planning Schemes, Structure Plans and Activity Centre Plans.

# 3.4.1 ROAD NETWORK AND TRAFFIC MANAGEMENT DEVICES

As outlined within this report, the Structure Plan design proposes to retain the broad framework of the existing road network and primary traffic flows in order to achieve the desired development outcome.

The proposed changes to the existing road network and associated road hierarchy as outlined in Figure 23 include:

**Resolution Drive:** This section of road will be removed and the connection will be formed by use of the historical Raconteur Drive alignment.

- Resolution Drive will connect between the Great Eastern Highway/Hardey Road traffic signal controlled intersection and relocated Grandstand Road roundabout.
- o The Grandstand Road/Resolution Drive/Stoneham Street roundabout will be relocated approximately 125m to the north-east of its existing location, and will become a three arm roundabout.
- o The existing Grandstand Road configuration of a four lane divided road (2 lanes in each direction) will be continued along the Raconteur Drive section of the road. As such a continuous four lane divided road will operate between Great Eastern Highway and Guildford Road via the Garrett Road Bridge (additional turn pockets will be provided where required).
- o A single intersection is proposed on Resolution Drive between the relocated roundabout and Great Eastern Highway and this is expected to take the form of an all movements 'seagull' intersection with separate left turn/right turn lanes into Resolution Drive and a right turn auxiliary lane from Resolution Drive. This will provide vehicular access to and from the three precincts situated north of Resolution Drive.
- **Stoneham Street:** This section of road will be retained along its existing alignment providing a connection between Great Eastern Highway/Belgravia Street traffic signal controlled intersection and the relocated Grandstand Road/Resolution Drive roundabout adjacent to the Ascot Kilns LDP area.
- o The section of Resolution Drive (north of Stoneham Street) will form a four-way intersection at Stoneham Street/Daly Street/Resolution Drive. It is expected that this intersection would be priority controlled with a signalised pedestrian crossing phase across all four approaches.
- o The existing lane arrangement along Stoneham Street will be retained, being a four lane undivided road (2 lanes in each direction). A median strip on approaches to main intersections is provided and a painted dividing line midblock. On-street parking may be considered at detailed design stage to support non-residential uses.
- o The existing access arrangements from Stoneham Street to the car park within the Belmont Trust Land will remain unchanged.

Golden Gateway | Structure Plan

- Hargreaves Street and Daly Street will continue along their existing alignments and connect between Great Eastern Highway and Stoneham Street. Grandstand Road (south) will be realigned at the northern end to connect into Daly Street. It is proposed to realign the northern section of Daly Street to accommodate the four-Hargreaves Street, Daly Street and Grandstand Road (south): It is proposed that way intersection at Stoneham Street/Daly Street/Resolution Drive.
- All intersections with Great Eastern Highway will be retained as per the existing left-in/left-out arrangement. 0
- The existing access arrangement (left-in/left-out only) at the Hargreaves Street and Stoneham Street intersection will be retained. 0
- Grandstand Road (south) intersection with Daly Street would be priority controlled with Grandstand Road (south) being the minor leg of the intersection. 0
- All roads would take the form of two lane roads (1 lane in each direction). Onstreet parking will be provided along each street where appropriate. 0
- Matheson Road: Matheson Road will continue to connect through to a realigned Resolution Drive via a modified road network, which will provide access to new development sites but not prioritise Matheson Road as a through route.
- The form of intersection between Matheson Road and Resolution Drive will be subject to further detailed design. It is expected that this intersection would be priority controlled with Matheson Road being the minor leg of the intersection (in the form of a seagull intersection). 0
- Matheson Road and internal roads would take the form of two lane roads (one lane in each direction). On-street parking will be provided where appropriate 0

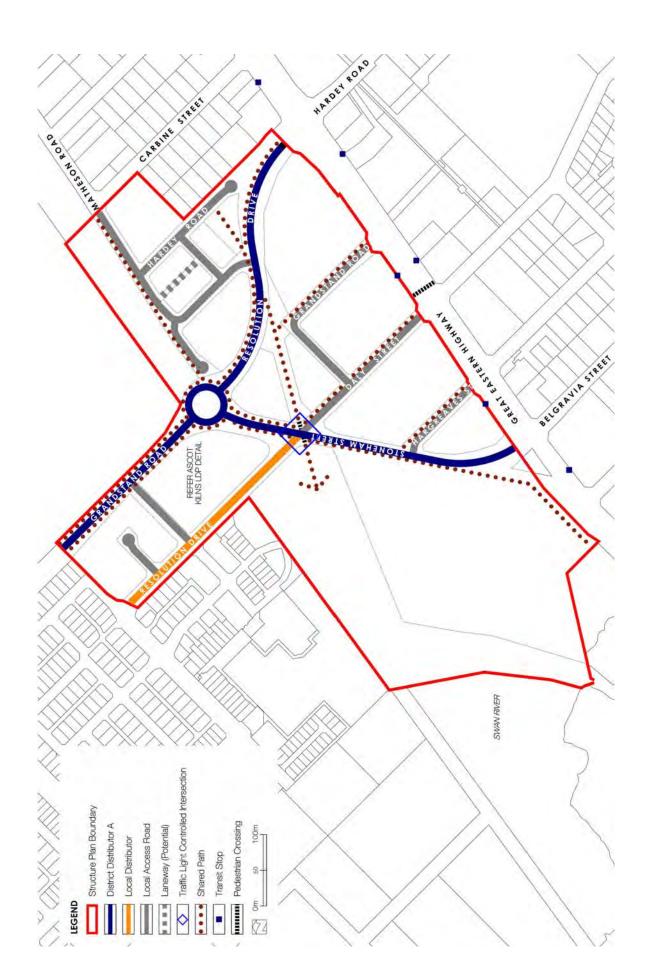


Figure 23 - Movement Network

## 3.4.2 TRAFFIC FORECASTS

As outlined in the Movement and Access Strategy, the following new vehicle trips are anticipated to be generated by the proposed development:

### Daily traffic:

- o Inbound 2,600 vehicles
- o Outbound 2,600 vehicles
- TOTAL 5,200 vehicles

## AM peak hour traffic:

- o Inbound 97 vehicles
- Outbound 697 vehicles

0

TOTAL 794 vehicles

## PM peak hour traffic:

- o Inbound 605 vehicles
- o Outbound 189 vehicles
- **TOTAL 794 vehicles**

In summary, based on the application of standard assessment techniques as outlined in the report, the proposed development results in a slight reduction in road network performance in 2031 in the PM peak period at the Resolution Drive - Great Eastern Highway intersection.

The Stoneham Street - Great Eastern Highway modelling shows that by 2031 under the base scenario (i.e. without Golden Gateway development), all approaches (other than Belgravia Street approach) would operate over capacity during the AM peak and during the PM peak at all approaches. Factoring in the proposed Golden Gateway development, the degree of saturation on the Stoneham Street and Great Eastern Highway approach increases, however the level of service remains unchanged.

The proposed road network within the subject land would significantly enhance the pedestrian and cycle connections throughout the development and also provide a catalyst for a gradual improvement in public transport service provision across the subject land/local area and improve connections across the subject land/local area and improve connections across the subject land/local area.

# 3.4.3 PEDESTRIAN AND CYCLING NETWORK

All existing shared paths surrounding and through the subject land will be maintained and many of these existing connections enhanced by additional shared path connectivity including:

- Retention of the shared path along the northern/western side of Stoneham Street.
- Improvement of shared path connections between the subject land and Ascot Waters development.
- The Stoneham Street shared path will be enhanced with a formalised connection through to Matheson Road. It is expected that the Stoneham Street/Daly Street/Resolution Drive signalised intersection will include a priority controlled pedestrian crossing phase across all four approaches.
- Shared path connections will be provided along Hargreaves Street, Daly Street and Grandstand Road (south).
- Controlled mid-block shared pedestrian/bike crossing via an at-grade signal controlled crossing at the Stoneham Street/Daly Street intersection and across Great Eastern Highway would be the preferred form of crossing.
- An at-grade signal-controlled crossing of Great Eastern Highway would require further investigation at the detailed design stage, but could take the form of the pedestrian crossing of Great Eastern Highway that is provided to the west of the subject land (location to be subject to further investigations/discussion).

### WATER MANAGEMENT Ŋ ო

### High level discussions with the Public Transport Authority (PTA) has informed the proposed changes anticipated for the existing public transport network as discussed below. The introduction of the Forrestfield Airport Link (FAL) rail connection from central Perth to Perth Airport and onto a park 'n' ride station at Forrestfield, will see the removal of four of the five existing bus routes operating along the Great Eastern Highway

**PUBLIC TRANSPORT** 

3.4.4

Subject to further consultation it is currently anticipated that the five existing bus routes will be rerouted as follows:

corridor (bus routes 36, 295, 296 and 299) and a renumbering and change of route for

the remaining bus route (bus route 40)

- Bus Route 36 to be renumbered as Bus Route 303 and operate from Midland Station to the new Redcliffe Station.
- Bus Routes 295/296/299 to feed into Forrestfield Station from Kalamunda and its
- Bus Route 40 to be renumbered Bus Route 940 Superbus (details below).

Park Transfer Station and Adelaide Terrace/St Georges Terrace. In the longer term, it is It is currently anticipated that the 940 Superbus would initially operate as a first stage from Belmont Station to Elizabeth Quay Station via Great Eastern Highway and Victoria anticipated that the Superbus would become a through routed service to Subiaco Station from Elizabeth Quay Bus Station via West Perth. The Superbus route would operate as a high frequency service and as such it is considered unlikely that the PTA would re-route this service through the subject land and instead, the service would operate along the Great Eastern Highway corridor between Redcliffe Station and Victoria Park Transfer Station.

scale development of the subject land, they would consider the option of operating a bus service which connected the subject land and Perth CBD with a bus service that development generating the requisite public transport demand to warrant the The PTA has indicated that, if sufficient public transport demand was generated by large utilised the internal road network. However this would be contingent upon the proposed investment in such a service.

## **LOCAL WATER MANAGEMENT STRATEGY** 3.5.1

The LWMS was developed to establish the concepts and broad level design measures for flood mitigation and stormwater management for the subject land. The intention of the LWMS is to guide the general stormwater management principles and to guide the preparation of the Urban Water Management Plan (UWIMP) that will be prepared at the A LWMS was prepared by Urbaqua in support of the Structure Plan (refer Appendix D). subdivision stage.

## STORMWATER MANAGEMENT 3.5.2

The key objectives for stormwater management are:

- Protection of wetlands and waterways (receiving environments) from the impacts of urban runoff
- Protection of infrastructure and assets from flooding and inundation.

The following planning measures are adopted to achieve the above objectives:

- Residential, industrial or commercial premises in existing or proposed areas must maintain floor levels at 500 mm above the 100yr ARI in the Swan River and 300 mm above the 100yr ARI in the local drainage system.
- event in residential areas and 10yr ARI event in commercial/industrial areas are to be Runoff from events greater than the 1yr ARI interval event and up to the 5yr ARI managed in accordance with the serviceability requirements of Australian Rainfall and Runoff (Engineers Australia, 2001) minor/major system.
- Stormwater in excess of the capacity of on-site retention systems will be conveyed through the existing drainage system consisting of local road drainage, Central Belmont Main Drain Basin and compensating basin.
- Major flood runoff (1% AEP) will be conveyed via overland flow within the road reserve to the compensating basin and drain prior to discharging to the Swan River.
- The design of the redeveloped urban areas should incorporate current best practice in WSUD to mitigate the potential impacts on regional water quantity and quality from redevelopment and the legacy conditions within the catchment.

- Retrofitting of stormwater management systems to achieve improved water quality outcomes should be maximised through the installation of biofilters (raingardens), amended soils and the use of structural controls to address litter, sediment and vegetative materials at source.
- Modification of the existing Central Belmont Main Drain and local drainage systems to suit the urban form whilst maintaining drainage capacity and peak flow rates.
- WSUD and best management practices promoting on-site retention of the first 15mm of rainfall from the basis of the surface water quantity management strategy for minor events.

## 3.5.3 GROUNDWATER MANAGEMENT

The key objectives for groundwater management are:

- Protecting infrastructure and assets from flooding and inundation by high seasonal groundwater levels, perching and/or soil moisture.
- Protecting groundwater dependent ecosystems from the impacts of urban runoff.
- Managing and minimising changes in groundwater levels and groundwater quality following redevelopment.

The following planning measures are adopted to achieve the above objectives:

- Retain existing surface levels as a minimum to ensure adequate separation.
- Limit basements in areas of shallow groundwater.
- Use of subsoil drainage below bio-retention areas, raingardens and tree pits to minimise local groundwater rise.

Groundwater levels provide potential clearance for basements to be installed, with two storey basements possible closer to Great Eastern Highway. Detailed designs of any infrastructure below the existing surface level (such as basements) may include tanking or other forms of damp-proofing. Any temporary lowering groundwater for construction, either for basements or sewer, may require dewatering licences from DWER.

## 3.6 EDUCATION FACILITIES

Existing education facilitates located within close proximity to the subject land include the following:

- Belmont Primary School is located at the intersection of Great Eastern Highway and Belgravia Street.
  - Redcliffe Primary School is located approximately 3km to the east.
- St Maria Goretti's Catholic School is located approximately 2.5km to the east.
- Maylands Peninsula Primary School is located approximately 2.5km to the north.
- Belmont City College is located approximately 3km to the south.

Given the nature of the development and anticipated demographic it is anticipated that there will be limited additional demand for education facilities generated in the precinct. The Golden Gateway Precinct is well located within an existing urban context allowing future residents to take advantage of existing education facilities.

## 3.7 EMPLOYMENT

Given the subject land's strategic location close to existing employment opportunities in the Belmont mixed business area, proximity to Perth CBD and commercial land uses along Great Eastern Highway, the area already enjoys a high rate of employment self-sufficiency, therefore additional employment generating land uses are not considered necessary to improve local employment opportunity. The non-residential uses anticipated for the Golden Gateway Precinct will generate a small amount of locally-based employment; however, the main purpose of these uses is to provide local services and to optimise the value of its highly visible and connected location.

# 3.8 INFRASTRUCTURE COORDINATION, SERVICING AND STAGING

An Infrastructure Assessment Report was prepared by Cardno in support of the Structure Plan (refer **Appendix E**).

## 3.8.1 WATER SUPPLY

Water Corporation does not foresee any issues with servicing the proposed scheme with potable water. Initial advice from the Water Corporation has confirmed the following with regards to required upgrades:

 Water Corporation will upgrade the headworks, pipe equal to or greater than 300mm diameter and pump stations, as and when required.

## 3.8.2 WASTEWATER

The proposed development will have significant impacts to the current wastewater infrastructure. It is not envisaged the existing Redcliffe Pump Station 5 will have sufficient capacity with a shortfall of 9.09 L/s to service the proposed development and will require a significant upgrade. This would require the upgrade of the existing pumping station to a larger type 40. A type 40 pumping station is a station capable of a 40 L/s service consisting of two pump-sets located in a common wet-well constructed from 2500mm internal diameter precast concrete pipes. Redcliffe Pump Station 2 will likely have capacity, however further planning should be coordinated with the Water Corporation to ascertain other timing of other developments in the area.

## 3.8.3 POWER SUPPLY

The Belmont substation servicing the subject land falls under the Cannington load area. Western Power's Annual Planning Report 2015/16 states "no substation capacity shortfall is forecast in the Cannington load area over the next five years." This takes into account committed, and most likely to occur, network expansion plans for the area. The Western Power Network Mapping Tool indicates that there is >30MVA spare capacity in the network until at least 2036 based on current and forecast demand. The implementation of the LSP will aim to remove the existing overhead Low Voltage/High Voltage (LV/HV) power lines and replace with underground power and upgraded LED lighting.

## 3.8.4 GAS SUPPLY

Correspondence received from ATCO Gas advised that the existing infrastructure can support the proposed development.

## 3.8.5 TELECOMMUNICATIONS

The infrastructure within a development will be installed by the developer. Alternatively, Telstra can be engaged to install infrastructure within a development at the developer's expense.

Telstra's commercial pit and pipe service will generally not be offered in developments where NBN Co has confirmed agreement to install NBN Co fibre within a development stage.

# 3.8.6 WATER CORPORATION MAIN DRAIN

Stormwater in excess of the capacity of on-site retention systems will be conveyed through the existing drainage system consisting of local road drainage, Central Belmont Main Drain Basin and compensating basin.

The significant modification to the system will be the conversion of the open drain between Resolution Drive and Stoneham Street, as recommended in the Water Corporation (2009) review of the system. It was recommended that this drain is replaced with a 1500mm pipe for safety reasons. Replacing the drain with a pipe will also allow for realignment of the system consistent with other services and future POS alignments.

Any changes to the Water Corporation drainage system will need to be undertaken in consultation with the Water Corporation and will require further detailed design, justification and agreement. This includes consideration of the modifications outlined above to ensure that the capacity of the main drain is sufficient to meet the conditions of the Water Corporation's operating license.

Downstream of Stoneham Street, within the Swan and Canning River Development Control Area, the existing compensation basin and drain that discharges to the Swan River will not be modified.

## 3.9 IMPLEMENTATION

## 3.9.1 LOCAL PLANNING POLICY

The Structure Plan outlines that design guidelines and the development requirements should be prepared and adopted by the City as a LPP. Development or subdivision should not be approved within the Structure Plan Area until the LPP is adopted, unless otherwise agreed by the Local Government.

# 3.9.2 SCHEME AMENDMENT TO FACILITATE STRUCTURE PLAN

The existing zoning arrangement within the Structure Plan Area does not currently reflect the proposed zoning as outlined within **Plan 1**. Consequently, a Scheme Amendment will be required to address this inconsistency during the implementation of the Structure Plan.

The Structure Plan Area should be rezoned to 'Special Development Precinct' to provide the appropriate base zoning to facilitate integrated development for the Precinct. In addition, in order to achieve the required planning framework for the Structure Plan area, it will also be necessary to designate a 'Development Area' over the subject land under Part 6 of LPS 15. In accordance with Clause 6.2.4 of LPS 15, the preparation and approval of a Structure Plan is required to guide subdivision and development. This process can occur concurrently with the rezoning of the Structure Plan area to 'Special Development Precinct' as described above. As a pre-requisite to the finalisation of this Scheme Amendment process, the City will require the preparation and adoption of the design guidelines and the development requirements as a LPP.

# 3.9.3 SCHEME AMENDMENT TO 'NORMALISE' STRUCTURE PLAN

In the future, when the implementation of the Structure Plan is progressed and the proposed road realignments, land reassembly and cadastral boundary changes are finalised, it will then be appropriate to undertake a further Scheme Amendment. This Scheme Amendment will remove the 'Special Development Precinct' zone over the Structure Plan Area and incorporate the zonings as depicted within **Plan 1**.

Minor amendments to the MRS will also be required to rationalise the 'Primary Regional Roads' reservation associated with Great Eastern Highway. It is likely that this would occur through an omnibus amendment

# 3.9.4 INFRASTRUCTURE FUNDING ARRANGEMENTS

The City of Belmont will establish an appropriate funding strategy for the Structure Plan Area. As part of the strategy, a Development Contribution Area (DCA) within LPS 15, under which a Development Contribution Plan (DCP) may be implemented to contribute to the funding of the public infrastructure requirements to facilitate development in the Structure Plan Area will be considered.

Infrastructure items that would be eligible to be funded under a DCP should be in accordance with State Planning Policy 3.6 Development Contributions for Infrastructure (SPP 3.6) and may include:

- Construction/upgrades to roads, intersections and pathways, including the realignment of Resolution Drive, the relocation of the Stoneham Street roundabout and the light controlled intersection of Stoneham St, Daly St and Resolution Dve.
  - Great Eastern Highway pedestrian crossing.
- Land for public open space and community facilities.
- Landscape treatment for all public realm areas, including local roads.

This Structure Plan will inform any future DCP, particularly in relation to the proposed upgrades to roads and intersection treatments as determined by the Movement and Access Strategy contained at **Appendix C** and the Public Realm Strategy contained at **Appendix F**.

## 3.9.5 LAND ASSEMBLY

There are various statutory processes required to deliver and facilitate development of the subject land, including amendments to LPS 15. Following adoption of the Structure Plan, subdivision and amalgamation applications can be lodged with the WAPC in the normal manner to assemble the land appropriately. Amalgamation is also likely to occur to enable land rationalisation. The subdivision/amalgamation process will be necessary to create some key elements of the project, such as the amalgamation of land parcels, realignment of existing roads and creation of the new subdivisional roads as shown on **Figure 24** below.



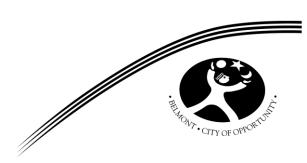


### Ordinary Council Meeting 26/02/19

Item 12.6 refers

**Attachment 17** 

**Bushfire Management Plan** 



### Golden Gateway Structure Plan

**Bushfire Management Plan** 

Prepared for City of Belmont

By Urbaqua

June 2018



### Disclaimer and Limitation

This document is published in accordance with and subject to an agreement between Urbaqua and the Client, City of Belmont, for who it has been prepared for their exclusive use. It has been prepared using the standard of skill and care ordinarily exercised by environmental professionals in the preparation of such Documents.

This report is a qualitative assessment only, based on the scope of services defined by the Client, budgetary and time constraints imposed by the Client, the information supplied by the Client (and its agents), and the method consistent with the preceding. Urbaqua has not attempted to verify the accuracy or completeness of the information supplied.

This Bushfire Management Plan provides strategic assessment of the subject site only. A subsequent Bushfire Management Plan and/or Bushfire Attack Level (BAL) Assessment may be required to support future development applications. The recommendations contained in this report are considered to be prudent minimum standards only, based on the author's experience as well as standards prescribed by relevant authorities. It is expressly stated that Urbaqua and the author do not guarantee that if such standards are complied with or if a property owner exercises prudence, that a building or property will not be damaged or that lives will not be lost in a bush fire.

Fire is an extremely unpredictable force of nature. Changing climatic factors (whether predictable or otherwise) either before or at the time of a fire can also significantly affect the nature of a fire and in a bushfire prone area it is not possible to completely guard against bushfire.

Further, the growth, planting or removal of vegetation; poor maintenance of any fire prevention measures; addition of structures not included in this report; or other activity can and will change the bushfire threat to all properties detailed in the report. The achievement of the level of implementation of fire precautions will depend on the actions of the landowner or occupiers of the land, over which Urbaqua has no control. If the proponent becomes concerned about changing factors then a Bushfire Management Plan should be requested.

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### **EXECUTIVE SUMMARY**

This bushfire management plan has been undertaken to support structure planning for the Golden Gateway Precinct in the City of Belmont (Figure 1).

A small portion of the subject land is identified as a bush fire prone area, designated by the Fire and Emergency Services (FES) Commissioner. This report has been prepared to meet the requirements of *State Planning Policy 3.7: Planning in Bushfire Prone Areas* (SPP 3.7) (2015) and the *Guidelines for Planning in Bushfire Prone Areas*, Version 1.1 (WAPC, 2017).

This plan provides advice consistent with the nature of a strategic proposal. Details in this report are consistent with *State Planning Policy 3.7: Planning for Bushfire Prone Areas* (WAPC, 2015) and the *Guidelines for Planning in Bush Fire Prone Areas* and associated appendices (V1.3, WAPC, 2017).

A vegetation class assessment was conducted for the subject land and adjacent areas for a minimum of 150 metres. As the road and lot layout is known, a bushfire attack level (BAL) assessment was undertaken and a BAL contour plan has been developed to show the indicative future BALs. This information may be used to guide the future development of the site, consistent with AS3959 Construction of buildings in Bushfire Prone Areas.

Bushfire risk to the areas proposed for future development is BAL-LOW. There is insufficient risk to warrant specific construction requirements.

The bushfire mitigation and management strategies outlined in this management plan comply with the acceptable solutions of control for each of the Bushfire Protection Criteria detailed in *Guidelines for Planning in Bushfire Prone Areas* (2017).

It is therefore considered that this bushfire management plan demonstrates compliance with the objectives and provisions of *State Planning Policy 3.7: Planning in Bushfire Prone Areas.* 

This bushfire management plan is to be endorsed by the City of Belmont and is required to be reviewed and updated where necessary.



### **CONTENTS**

EXEC	CUTIVE SUMMARY	ii
1 1.1	Introduction	
1.2	·	
2	Environmental considerations	5
2.1	3	
2.2	2 Re-vegetation/Landscape Plans	6
3	Bushfire Assessment Results	
3.1	the state of the s	
3.2	2 Assessment outputs	11
4	Identification of bushfire hazard issues	
4.		
4.2	3 - 3 - 3 - 3 - 3 - 3 - 3 - 3 - 3 - 3 -	
4.4		
	Assessment against the Bushfire Protection Criteria	
5.1		
5.2 5.3		
	Figures	
Fi.o.	re 1: Development concept plan and proposed zoning (Source: TBB)	_
	re 2: Location plan	
_	re 3: Map of Bushfire Prone Areas for the subject site (Source: DFES, 2018)	
_	re 4: Post development vegetation classification and slope	
Figu	re 5: BAL contour map	14
	Tables	
Tol-1	o 1. Vegetation elegification	,
	e 1: Vegetation classificatione 2: BAL assessment summary	
	e 3: Excerpt from AS 3959, Table 2.4.3, Distance (m) of the site from the predominant	1
_	etation class	
	e 4: Vehicular access technical requirements (WAPC, 2017)	
ıabl	e 5: Bushfire protection criteria assessment	17



### 1 INTRODUCTION

The City of Belmont has engaged Urbaqua to prepare a Bushfire management plan to support preparation of a local structure plan for the Golden Gateway project area (Figure 1) in the City of Belmont (Figure 2).

A portion of the subject land is identified as a bush fire prone area, designated by the Fire and Emergency Services (FES) Commissioner (Figure 3). This report has been prepared to meet the requirements of *State Planning Policy 3.7: Planning in Bushfire Prone Areas* (SPP 3.7) (2015) and the *Guidelines for Planning in Bushfire Prone Areas* (V1.3, WAPC, 2017).

Any identified bushfire risk will be addressed as part of the future development approvals process, consistent with the requirements of *State Planning Policy 3.7: Planning in Bushfire Prone Areas* (SPP 3.7) (2015), the Building Code of Australia and Australian Standards (AS3959-2009): Construction of buildings in bushfire prone area where these apply.

### 1.1 Proposal details

The subject land consists of approximately 31.8 hectares of land in the vicinity of Great Eastern Hwy, Resolution Dr, Grandstand Rd and Stoneham St in Ascot.

The Golden Gateway Precinct includes a large portion of 'Mixed use' land, which encompasses the historical Ascot's Bristle beehive kilns and chimney stacks and portions of the Ascot Racecourse. The precinct also contains approximately 5.3 ha of Parks and Recreation reserve which covers Belmont Trust Land. There is also a small portion of Parks and Recreation: water supply sewerage and drainage reserve, which is under the control of the Water Corporation.

The Golden Gateway Precinct will provide for a diverse range of land uses. The primary land use within the Structure Plan area is residential, supplemented by commercial uses and local open space

### 1.1.1 Planning background

The majority of the study area is zoned 'Urban' under the Metropolitan Region Scheme, with a portion zoned for 'Mixed use' and reserved for 'Parks and Recreation' under City of Belmont Local Planning Scheme No. 15.



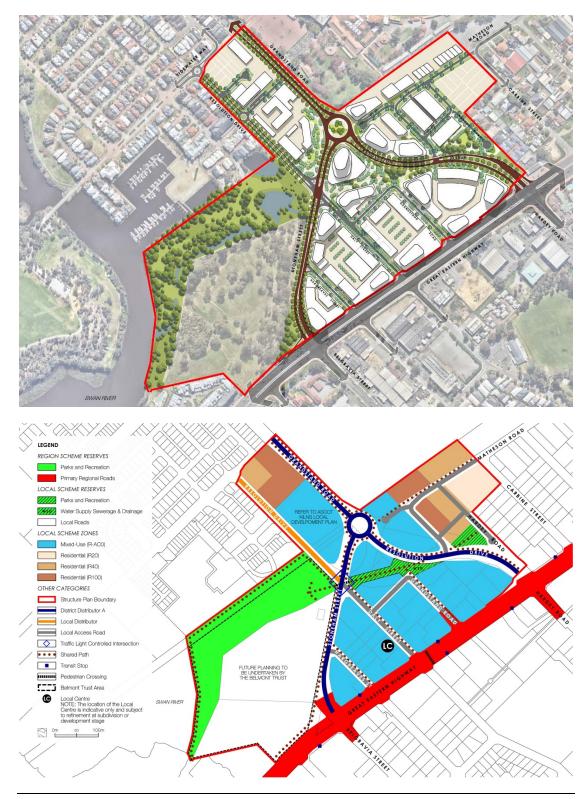
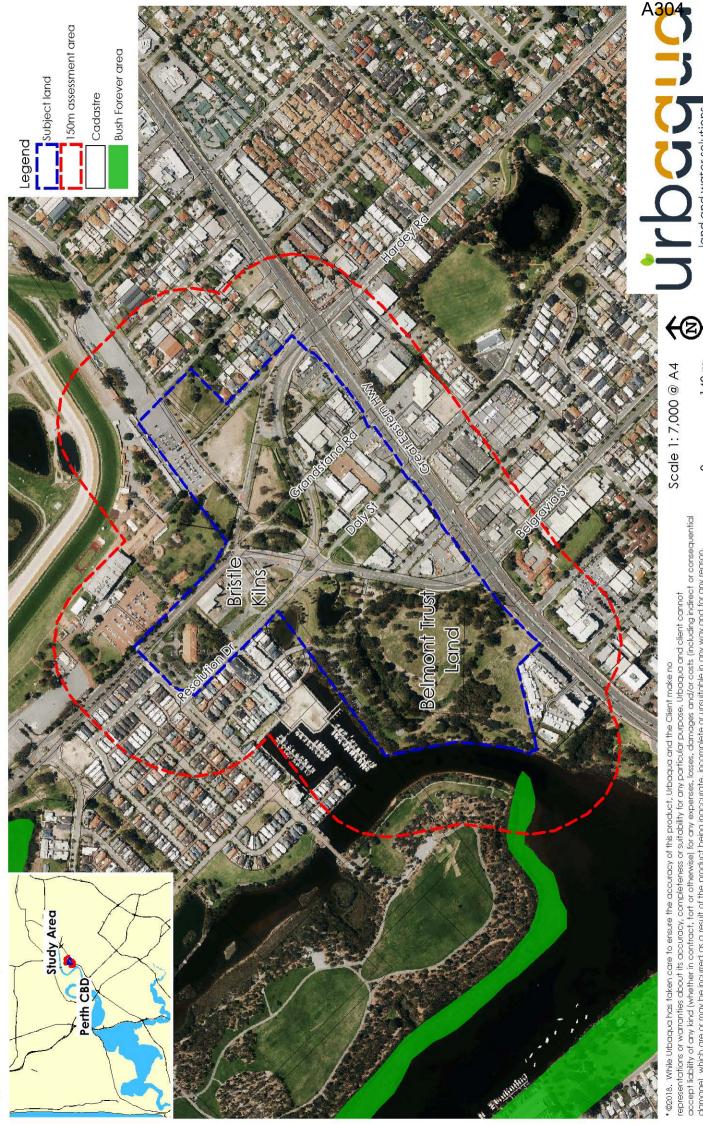


Figure 1: Development concept plan and proposed zoning (Source: TBB)



City of Belmont: Golden Gateway - Bushfire Management Plan Figure 2: Subject land



damage) which are or may be incurred as a result of the product being inaccurate, incomplete or unsuitable in any way and for any reason.

Data source: CoB, Landgate. Created by: AT. Projection: MGA: zone 50.

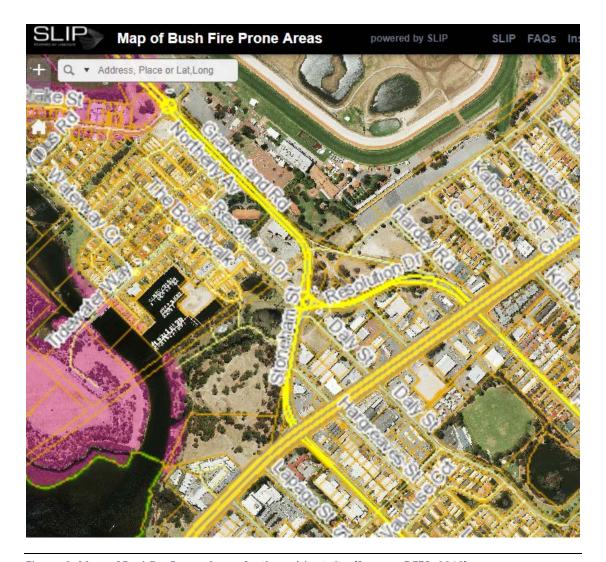


Figure 3: Map of Bushfire Prone Areas for the subject site (Source: DFES, 2018)

### 1.2 Bushfire management guidelines, specifications and minimum standards

Specifications or standards relevant to this bushfire management plan are derived from and consistent with:

- Fire and Emergency Services Act 1998
- Bush Fires Act 1954
- Planning and Development (local planning Scheme amendment) Regulations 2015
- State Planning Policy 3.7: Planning in Bushfire Prone Areas (WAPC, 2015);
- Guidelines for Planning for Bushfire Prone Areas and appendices, Version 1.3 (WAPC, 2017)
- Australian Standards (AS3959-2009): Construction of buildings in bushfire prone areas;
   and
- City of Belmont Fire Break Notice 2017-2018.



### 2 ENVIRONMENTAL CONSIDERATIONS

The subject land has been used predominantly for commercial purposes for over 50 years. The Golden Gateway Precinct includes the historical Ascot's Bristle beehive kilns and chimney stacks and portions of the Ascot Racecourse. The subject land also contains a large proportion of managed parkland which borders the Swan River. The Belmont Trust Land in the western portion of the study area was historically used for sporting purposes such baseball fields and is now maintained as parkland by the City. There are no significant environmental values located within the subject land.

Bush Forever site 313, Swan River Salt Marshes is located within the 150m assessment area. This area is separated from the subject land by the Swan River, which is approximately 70m wide at this point. The remaining areas within 150m of the subject land have no significant environmental values. They include Ascot Racecourse, Belmont Park Primary school, residential housing and commercial areas.

### 2.1 Native Vegetation - modification and clearing

The vegetation in the study area has been highly modified. Although mature trees remain in many parts of the subject land, the undergrowth has been cleared and is maintained in a modified landscaped, parkland state.

Although the grassland which covers the Belmont Trust land is managed and maintained by the City of Belmont, a small portion of regrowth exists where the tree trunks are too close together to permit mowing. This land is proposed to be developed in the future, although the development concept is not yet known. The City will continue to maintain the Belmont Trust Land in a low fire hazard state.

Some bushfire risk exists as a result of vegetation within and adjacent to Bush Forever Area 313 (Swan River Salt Marshes) located to the north west of the subject land. This vegetation is separated from the subject land; however, by a branch of the Swan River. It is also noted that the majority of vegetation on the island is maintained in a low fuel state. Where shrubs and trees exist, there is no understory and the fine fuel load is less than 2tonnes/ha.





Plate 1: Fine fuel load less than 2 tonnes/ha on the island adjacent the subject land

Vegetation also exists around a drain on the south-western side of the Ascot Quays Apartment Hotel. This vegetation is outside the subject land but within 150m of the structure plan area. The



vegetation is less than 20m in width on each side of the drain and the understory is managed (irrigated) grassland. This vegetation is not considered to represent a bushfire hazard.

### 2.2 Re-vegetation/Landscape Plans

No revegetation is proposed within the subject land.

Some landscaping of road reserves, open space and car parks is proposed. This will consist of individual trees without understory or managed parkland and as such is not considered to have the potential to create a fire hazard.



### 3 BUSHFIRE ASSESSMENT RESULTS

### 3.1 Assessment Inputs

In order to identify the potential bushfire risks, it is necessary to describe the bushfire problem associated with the subject land. The assessment takes into consideration the:

- the topography and slope of the subject land;
- type and classification of vegetation present on and adjacent to the subject land;
- distances between the classifiable vegetation; and
- current and proposed future land use.

### 3.1.1 Slope

The study area has generally flat topography and grades gently from 6mAHD in the south-east to 3mAHD in the west. The study area has a few low points of approximately 1-2mAHD through the centre of the study area, as shown in Figure 4.

The effective slope (that is the slope that will affect the behaviour of an approaching bushfire) underneath the vegetation across the River to the west is upslope.

Slope is therefore not considered to be a factor in terms of increasing bushfire hazard.

### 3.1.2 Current and future land use

The subject land comprises four key precincts:

- The area bounded by Great Eastern Highway, Stoneham Street and Resolution Drive is characterised by predominately mixed business development and small pockets of retail (food and beverage) uses along Great Eastern Highway;
- The western portion of the subject land encompassing the Belmont Trust Land (Grove Farm Reserve) is previously cleared with large mature trees sparsely located around the reserve. Grove Farm Reserve was historically used for recreation purposes, specifically a baseball field;
- The northern portion of the subject land is partially developed with the WA Turf Club Headquarters and Ascot kilns and chimney stacks; and
- The remainder of the subject land within the north-eastern corner is largely undeveloped and comprises a number of existing road reserves and WA Turf Club owned land used for overflow parking on racing event days.

The Golden Gateway Precinct will provide for a diverse range of land uses. The primary land use within the Structure Plan area is residential, supplemented by commercial uses and local open space.

### 3.1.3 Vegetation types

On the basis of a site visit on 13 March 2018, vegetation at the site and within 150m was assessed. Vegetation within 100m was classified according to the descriptions provided in AS 3959 – 2009, and includes the following three vegetation types:



- Class B Woodland Low woodland (B7): Low trees and shrubs 2-10m high; foliage cover less than 10%. Dominated by eucalypts and Acacias. Often have a grassy understorey or low shrubs. Acacias and Casuarina woodlands grade to Atriplex shrublands in the arid and semi-arid zones.
- Low threat vegetation AS3959 2.2.3.2(b) Single area of vegetation less than 1ha and not within 100m of other areas of vegetation being classified.
- Low threat vegetation AS3959 2.2.3.2(f) grassland managed in a minimal fuel condition, maintained lawns, golf courses, maintained public reserves and parklands, vineyards, orchards, cultivated gardens, commercial nurseries, nature strips and windbreaks.

The vegetation within the subject land and 150m surrounding is shown in Table 1 and Figure 4.

**Table 1: Vegetation classification** 

Photo point	Vegetation class	Vegetation type	Description
Plot 1	Low Threat Exclusion Clause 2.2.3.2 (f)	Ascot Racecourse	Grassland managed in a minimal fuel condition, maintained lawns, golf courses, maintained public reserves and parklands, vineyards, orchards, cultivated gardens, commercial nurseries, nature strips and windbreaks
Plot 2	Low Threat Exclusion Clause 2.2.3.2 (f)	Ascot Racecourse	Grassland managed in a minimal fuel condition, maintained lawns, golf courses, maintained public reserves and parklands, vineyards, orchards, cultivated gardens, commercial nurseries, nature strips and windbreaks
Plot 3	Low Threat Exclusion Clause 2.2.3.2 (f)	Managed parkland	Grassland managed in a minimal fuel condition, maintained lawns, golf courses, maintained public reserves and parklands, vineyards, orchards, cultivated gardens, commercial nurseries, nature strips and windbreaks



Photo point	Vegetation class	Vegetation type	Description
Plot 3	Low Threat Exclusion Clause 2.2.3.2 (f)	Managed parkland	Grassland managed in a minimal fuel condition, maintained lawns, golf courses, maintained public reserves and parklands, vineyards, orchards, cultivated gardens, commercial nurseries, nature strips and windbreaks
Plot 3	Low Threat Exclusion Clause 2.2.3.2 (f)	Managed parkland	Grassland managed in a minimal fuel condition, maintained lawns, golf courses, maintained public reserves and parklands, vineyards, orchards, cultivated gardens, commercial nurseries, nature strips and windbreaks
Plot 4	Low Threat Exclusion Clause 2.2.3.2 (f)	Public reserve maintained in low threat state	Grassland managed in a minimal fuel condition, maintained lawns, golf courses, maintained public reserves and parklands, vineyards, orchards, cultivated gardens, commercial nurseries, nature strips and windbreaks
Plot 4	Low Threat Exclusion Clause 2.2.3.2 (f)		Grassland managed in a minimal fuel condition, maintained lawns, golf courses, maintained public reserves and parklands, vineyards, orchards, cultivated gardens, commercial nurseries, nature strips and windbreaks



Pho poi	Vegetation class	Vegetation type	Description
Plot 8	Low Threat Exclusion Clause 2.2.3.2(b)	Regrowth	Single area of vegetation less than 1ha and not within 100m of other areas of vegetation being classified
Plot 5	Class B: Woodland	B07 - Low Woodland	Low trees and shrubs 2-10m high; foliage cover less than 10%. Dominated by eucalypts and Acacias. Often have a grassy understorey or low shrubs. Acacias and Casuarina woodlands grade to Atriplex shrublands in the arid and semiarid zones.
10 Plot 6	Low Threat Exclusion Clause 2.2.3.2 (f)	Drain	Grassland managed in a minimal fuel condition, maintained lawns, golf courses, maintained public reserves and parklands, vineyards, orchards, cultivated gardens, commercial nurseries, nature strips and windbreaks
Plot 6	Low Threat Exclusion Clause 2.2.3.2 (f)	Managed parkland	Grassland managed in a minimal fuel condition, maintained lawns, golf courses, maintained public reserves and parklands, vineyards, orchards, cultivated gardens, commercial nurseries, nature strips and windbreaks



Photo point	Vegetation class	Vegetation type	Description
Plot 7	Low Threat Exclusion Clause 2.2.3.2 (f)	Commercia I office Iandscapin g	Grassland managed in a minimal fuel condition, maintained lawns, golf courses, maintained public reserves and parklands, vineyards, orchards, cultivated gardens, commercial nurseries, nature strips and windbreaks
Plot 8	Low Threat Exclusion Clause 2.2.3.2 (f)	Primary school	Grassland managed in a minimal fuel condition, maintained lawns, golf courses, maintained public reserves and parklands, vineyards, orchards, cultivated gardens, commercial nurseries, nature strips and windbreaks

### 3.2 Assessment outputs

Consistent with Appendix Two of the *Guidelines for Planning in Bushfire Prone Areas* (V1.3, WAPC, 2017), as this bushfire management plan is to support an application where the indicative development footprint is known, a Bushfire Attack Level (BAL) assessment has been undertaken in accordance with Method 1 of AS3959: Construction of buildings in bushfire prone areas. Table 2 provides a summary of the assessment.

Table 2: BAL assessment summary

Plot	Vegetation Classification	Effective Slope	Separation Distance to the Classified Vegetation (m)	Hazard Level
6	Woodland (B)	Upslope	70m to the edge of the Parks and Recreation Reserve and 108m to the edge of the	BAL-LOW
			proposed development area (Belmont Trust Land)	

A BAL contour map has been created for the proposed development which shows indicative BAL ratings for the site (Figure 5) consistent with Appendix 3 of the *Guidelines for Planning in Bushfire Prone Areas* (V1.3, WAPC, 2017). The BAL contour map was prepared on the basis of FDI 80; the vegetation classification shown in Table 1; and slope shown on Figure 4. An excerpt from AS3959 is provided in Table 3.

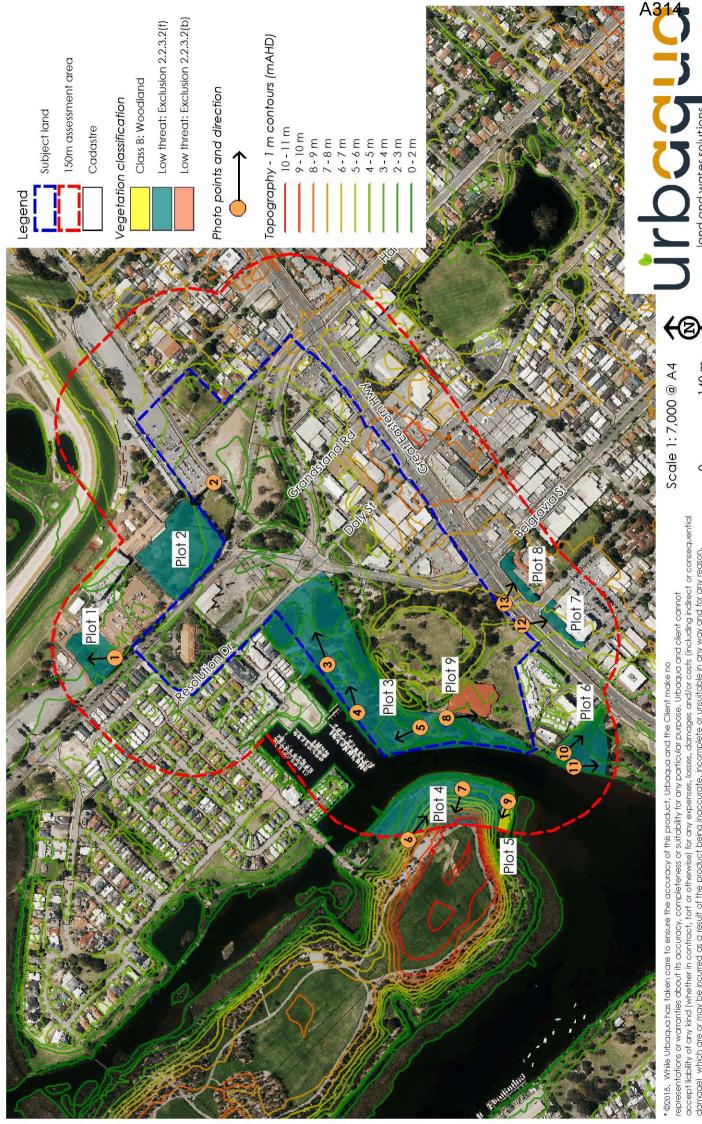


Table 3: Excerpt from AS 3959, Table 2.4.3, Distance (m) of the site from the predominant vegetation class

FDI 80 (1090 K)	Vegetation classification and slope
Bushfire attack levels (BALs)	Class B: Woodland - Upslope and flat land
BAL-FZ	<10 m
BAL-40	10-<14
BAL-29	14-<20
BAL-19	20-<29
BAL-12.5	29-<100
BAL-LOW	Beyond 100m

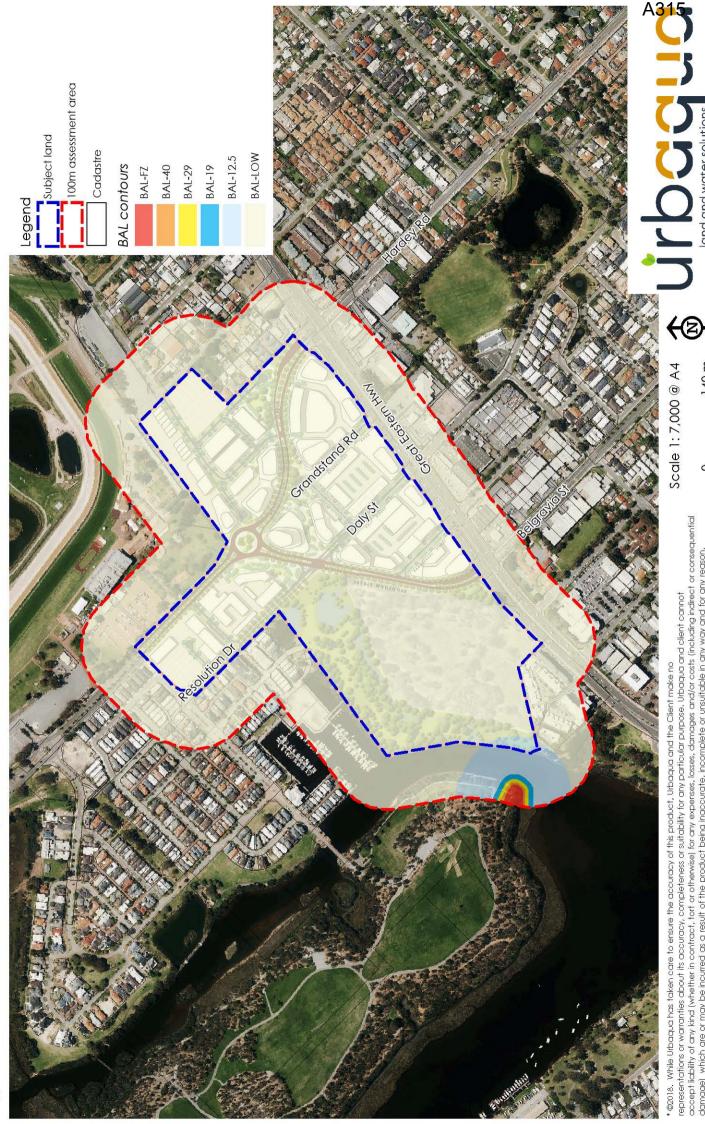


City of Belmont: Golden Gateway - Bushfire Management Plan Figure 4: Post-development vegetation classification



damage) which are or may be incurred as a result of the product being inaccurate, incomplete or unsuitable in any way and for any reason.

City of Belmont: Golden Gateway - Bushfire Management Plan Figure 5: BAL contour map



accept liability of any kind (whether in contract, tort or otherwise) for any expenses, lasses, damages and/or costs (including indirect or consequential damage) which are or may be incurred as a result of the product being inaccurate, incomplete or unsuitable in any way and for any reason.

Data source: CoB, Landgate, AS/NZS 3959-2009. Created by: AT. Projection: MGA: zone 50.

### 4 IDENTIFICATION OF BUSHFIRE HAZARD ISSUES

The subject land is adjacent to an area of vegetation which has the potential to create a bushfire risk.

It is considered that the bushfire risk to the proposed development can be adequately managed through appropriate location and siting and design of development, as well as necessary vehicular access and water supply which will be provided to the development.

Bushfire hazard to the proposed development is therefore considered to be low. This conclusion is substantiated further below.

### 4.1 Location

After development, the subject land will not contain any vegetation that is considered to be a bushfire hazard.

Although fire risk exists from vegetation adjacent to the subject land, the subject land is not subject to BAL-40 or BAL-FZ and therefore this proposal does not result in the intensification of any development in areas that are subject to extreme hazard.

### 4.2 Siting and design of development

Bushfire risk from vegetation outside the subject land is likely to remain as this vegetation is associated with significant environmental values (Bush Forever Site 313). It is noted that the Swan River establishes sufficient separation between the bushfire hazard and the edge of subject land to achieve BAL ratings of BAL-12.5 and less, consistent with Method 1 of AS3959. It is noted that the public open space reserve provides a further separation such that the land to be developed in the future (the Belmont Trust Land) is rated at BAL-LOW.

As no proposed areas of development will be subject to BAL-40 or BAL-FZ, it is considered that development has been sited to avoid areas of extreme bushfire risk. All habitable dwellings will be constructed to meet the requirements of AS3959 Construction of buildings in Bushfire Prone Areas where necessary.

### 4.3 Vehicular access

The subject site is afforded excellent access from an integrated regional (existing and future) road network. The subject land is bounded by Great Eastern Highway to the south which provides access to the west towards the Perth CBD, Graham Farmer Freeway and onto South Perth, Melville and Fremantle via Canning Highway. To the east, Great Eastern Highway provides access to Perth Airport, Tonkin/Roe Highway and onto Guildford, Midland and the Swan Valley. These networks provide excellent access to and egress from the subject land.

The proposed local road network provides for at least two different access and egress routes to the proposed residential and commercial areas. The localised road network includes a network of local distributor and access roads providing access to key regional and district roads such as Great Eastern Highway and the Garret Road bridge which include Grandstand Road, Resolution Drive and Stoneham Street.



All roads and transport infrastructure will be designed and constructed to meet the requirements of the *Guidelines for Planning in Bushfire Prone Areas* (Version 1.3 WAPC, 2017) Appendix Four, Table 4, as replicated in Table 4 below.

Table 4: Vehicular access technical requirements (WAPC, 2017)

Technical Requirement	Public road	Cul-de-sac	Private driveway	Emergency access way	Fire service access routes
Minimum trafficable surface (m)	6	6	4	6	6
Horizontal clearance (m)	6	6	6	6	6
Vertical clearance (m)	4	N/A	4.5	4.5	4.5
Maximum grade over <50m	1 in 10	1 in 10	1 in 10	1 in 10	1 in 10
Minimum weight capacity (t)	15	15	15	15	15
Maximum cross fall	1 in 33	1 in 33	1 in 33	1 in 33	1 in 33
Curves minimum inner radius (m)	8.5	8.5	8.5	8.5	8.5
Additional specialist requirements					

### 4.4 Water

The proposed development is currently serviced by a reticulated water supply, together with fire hydrants, in accordance with the specifications of the Water Corporation and Department of Fire and Emergency Services (DFES).

Contractors or others carrying out building or other works at the site must not cover hydrants and/or the markings indicating their location. In the event activities occur that do result in hydrants or markings being covered, damaged, or removed, it will be the responsibility of the relevant contractor to rectify the situation.



### 5 ASSESSMENT AGAINST THE BUSHFIRE PROTECTION CRITERIA

The subject land is adjacent to an area of bushfire risk. Bushfire risk mitigation and management measures have been identified to reduce bushfire risk to achieve the objectives of SPP3.7, as previously outlined in Section 3.

The bushfire risk mitigation strategies proposed comply with the acceptable solutions for each of the Bushfire Protection Criteria detailed in *Guidelines for Planning in Bushfire Prone Areas* (2017). They are summarised in Table 5.

### 5.1 Compliance Table

Table 5: Bushfire protection criteria assessment

Element	Acceptable solution	Com	pliance
1. Location	A1.1 Development location	V	No development is proposed in areas subject to BAL-40 or BAL-FZ.
<ol><li>Siting and design of development</li></ol>	A2.2 Asset Protection Zone	$\overline{\checkmark}$	No development will be subject to BAL-40 or BAL-FZ. Habitable buildings will be constructed in accordance with AS3959.
3. Vehicular Access	A3.1 Two access routes	$\overline{\checkmark}$	Short and long term public access is provided which ensures a minimum 2 access routes are provided at all times.
	A3.2 Public road	$\checkmark$	All public roads meet the requirements of Table 4 of Appendix 4 of the Guidelines for Planning in Bushfire Prone Areas (WAPC, 2017)
	A3.3 Cul-de-sac	$\checkmark$	N/A - no cul-de-sacs are proposed.
	A3.4 Battle-axe	$\checkmark$	N/A - No battle-axe lots are proposed.
	A3.5 Private driveway longer than 50m	V	N/A - No lots have driveways greater than 50m in length.
	A3.6 Emergency access way	V	N/A - No emergency access ways are proposed
	A3.7 Fire service access routes	$\overline{\checkmark}$	The existing road network provides appropriate fire service access routes.
	A3.8 Firebreak widths	$\overline{\checkmark}$	N/A
4. Water	A4.1 Reticulated areas	$\overline{\checkmark}$	The development is currently serviced by reticulated water and fire hydrants which meet Water Corporation and DFES specifications
	A4.2 Non- reticulated areas	$\overline{\checkmark}$	N/A
	A4.3 Individual lots within non-reticulated areas	$\overline{\checkmark}$	N/A



### 5.2 Bushfire management strategies

As the area proposed for development is greater than 100m from any classifiable vegetation (due to the presence of the Parks and Recreation Reserve), no bushfire management strategies are considered necessary.

There is insufficient risk to warrant specific construction requirements.

It is noted that any new roads will be constructed to meet Main Roads and Local Government requirements and that water and hydrants are provided to DFES and Water Corporation standards.

### 5.3 Certification by Bushfire Consultant

I, Shelley Shepherd, certify that at the time of inspection, the BAL ratings contained within this BMP are correct.

The Bushfire Attack Level to the proposed development area is BAL-LOW. There is insufficient risk to warrant specific construction requirements and no specific management actions are required to mitigate bushfire risk to the proposed development area.

Signatura. Date: 2 May 2018





### Client: City of Belmont

Version	Prepared by	Reviewed by	Submitted to Client	
			Copies	Date
V1	SSh	HBr	Electronic	2 May 2018
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### Ordinary Council Meeting 26/02/19

Item 12.6 refers

**Attachment 18** 

**Environmental Assessment Report** 



## Environmental Report: Golden Gateway

Prepared for City of Belmont

By Essential Environmental

June 2018



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### **EXECUTIVE SUMMARY**

The City of Belmont has engaged Essential Environmental to prepare an environmental report to support structure planning vicinity of Great Eastern Hwy, Resolution Dr, Grandstand Rd and Stoneham St, Ascot, within the City of Belmont.

This report provides an analysis of the environmental constraints and considerations to development and proposes broad approaches to mitigate any impacts and/or constraints on the basis of future land use.

The majority of the site has been historically cleared, although a number of significant trees have been established predominantly along driveways and boundaries and within the Grove Farm Reserve. The study area abuts a small section of the Swan River, which is a Bush Forever site, a conservation category wetland, and an environmentally sensitive area. Development of the study area will require adequate management of bushfire risk (the subject of a separate management plan) and potential impacts on fauna species.

In addition, a range of management strategies have been proposed to effectively manage or mitigate potential environmental impacts caused as a result of the development. Proposed management actions are summarised in the table below. It is considered that urban development of the site is an acceptable land use given the current environmental condition and lack of significance of the site, and in consideration of the proposed management strategies outlined in this report.

Issue	Action	Frequency	Responsibility
Preconstruction phase	,		
Contamination	Complete preliminary site investigation for contamination in accordance with Contaminated Sites Act 2003 should areas of known contamination be disturbed.	Once	Developer
Acid sulfate soils	Complete self-assessment checklist and	Once	Developer
	consider need for a preliminary site assessment.		Consistent with DPLH and DWER guidelines
Vegetation and flora	Clearly delineate POS areas and trees to be retained.	Once	Licensed Surveyor (Developer)
Fauna and habitat	All site staff to participate in Environment, Health and Safety inductions which provide requirements for management of significant fauna and reporting procedures for environmental incidents.	Once	Developer and Construction contractor



Issue	Action	Frequency	Responsibility
Water management	Refer the local structure plan to the Department of Biodiversity, Conservation and Attractions as it contains a portion of land within and abutting the Swan River Trust Development Control Area.	Once	Developer/City of Belmont
	A Local water management strategy will be completed and used as the basis for detailed design.		Developer, in accordance with SPP 2.9: Water
	Following approval of the LWMS, UWMP(s) will be prepared prior to subdivision for approval by City of Belmont.		Resources
Bushfire	A Bushfire Management Plan will be prepared to support the LSP.	Once	Developer, in accordance with
	The Bushfire Management Plan will be revised and implemented at subdivision.		SPP 3.7: Planning in Bushfire Prone Areas
Construction phase			
Soils and topography	Ground disturbing activities should be kept to a minimum and carried out 'as required' (in stages) immediately prior to lots being released for sale as part of a 'staged' development of the site.	Ongoing during construction phase.	Construction Contractor (Developer)
Contamination	Management of any identified contamination in accordance with the Contaminated Sites Act 2003.	Ongoing during construction phase.	Construction Contractor (Developer)
Acid sulfate soils	Management of any identified ASS consistent with DPLH and DWER guidelines.	Ongoing during construction phase.	Construction Contractor (Developer)
Vegetation and flora	Maintain markings and fencing around vegetation and trees to be retained. Cleared vegetation to be mulched and stored on site.	Ongoing during construction phase.	Construction Contractor (Developer)
Fauna and habitat	Undertake clearing in the direction of the river to allow fauna to escape.	Ongoing during construction phase.	Construction Contractor (Developer)
Water management	Manage sediment transport to waterways and drainage systems consistent with the LWMS.	Ongoing during construction phase.	Construction Contractor (Developer)



Issue	Action	Frequency	Responsibility
Aboriginal heritage	In the event a site is discovered, all work in the area will cease and the Department of Planning, Lands and Heritage will be contacted.	Ongoing during construction phase.	Construction Contractor (Developer)
Construction impacts	Ensure dust and sediment runoff is adequately managed.  Ensure appropriate waste disposal of building materials.	Ongoing during construction phase.	Construction Contractor (Developer)
Post construction phase			
Soils and topography	Landscape or stabilise cleared areas immediately.	Once	Construction Contractor (Developer)
Vegetation and flora	Inspect fencing (if applicable) and replace if required.	6 months	Developer until hand over to City of Belmont
	Ensure ongoing maintenance of retained vegetation and any revegetation areas / native landscaping prior to handover.	Ongoing until handover.	



## **CONTENTS**

Exe	cutiv	e Summary	iii
1	Intro	oduction	1
	1.1	Study Area	1
	1.2	Methodology	1
	1.3	Previous Environmental Assessments And Key Requirements	1
2	Prop	posed Development	4
	2.1	Planning Context	4
3	Exist	ing Environment	5
	3.1	Land Use Context	5
		3.1.1 Historical Land Use	5
		3.1.2 Current Land Use	5
	3.2	Topography, Geology And Soils	5
		3.2.1 Topography	
		3.2.2 Geology	
		3.2.3 Acid Sulfate Soils	
		3.2.4 Contaminated Sites	
	3.3	Flora, Fauna And Vegetation	
		3.3.1 Conservation Areas	
		3.3.2 Flora	
		3.3.3 Fauna	
		3.3.4 Bushfire Risk	
	3.4	Water Resources	
		3.4.1 Public Drinking Water Source Area	
		3.4.2 Surface Water Resources	
		3.4.3 Groundwater Resources	
	3.5	Heritage	
	0.0	3.5.1 Aboriginal Heritage	
		3.5.2 European Heritage	
4	Finc	lings And Recommendations	
	4.1	Soils And Topography	
		Acid Sulfate Soils	
	4.3	Surrounding Land Use And Buffer Requirements	
	4.4	Vegetation And Flora	
	4.5	Fauna And Habitat	
	4.6	Flood Protection, Groundwater And Water Quality Management	19
	4.7	Heritage	20
	4.8	Construction Impacts	20
	4.9	Conclusion, Constraints And Opportunities	21
5	lmp	lementation Strategy	24
6	Ref	erences	26



## **Tables**

Table 1: Relevant environmental legislation	2
Table 2: Conservation significant flora likely to occur in the study area	11
Table 3: Conservation significant fauna known or likely to occur in the study area	11
Table 4: Groundwater resource allocation and availability (as of January 2016)	14
Table 5: Implementation strategy	24
Figures	
Figure 1: Study area location	3
Figure 2: Local Planning Scheme	4
Figure 3: Topography and soils	6
Figure 4: Acid sulfate soils risk	9
Figure 5: Vegetation	12
Figure 6: Water resources	15
Figure 7: Heritage	17
Figure 8: Environmental constraints	22
Figure 9: Environmental opportunities	23



#### 1 INTRODUCTION

The City of Belmont has engaged Essential Environmental to prepare an environmental report to support structure planning in the vicinity of Great Eastern Hwy, Resolution Dr, Grandstand Rd and Stoneham St, Ascot, within the City of Belmont.

This report provides an analysis of the environmental constraints and considerations to development and proposes broad approaches to mitigate any impacts and/or constraints on the basis of likely future commercial, mixed use and residential land use.

## 1.1 Study area

The study area consists of approximately 31.8 hectares of land in the vicinity of Great Eastern Hwy, Resolution Dr, Grandstand Rd and Stoneham St in Ascot. The study area currently comprises of a mixture of commercial lots, the heritage listed Ascot Brick Works and public open space (Figure 1).

The study area has 4 distinct regions: (i) the south-eastern commercial area, bound by Great Eastern Hwy, Resolution Dr and Stoneham St; (ii) west where Grove Farm Reserve is bound by Great Eastern Hwy and public open space adjacent to the Swan River; (iii) largely undeveloped land, with exception of local distributor roads, through the centre of the study area; and (iv) most northern portion within which is located the Perth Racing Administration Office.

## 1.2 Methodology

This report considers the following environmental aspects of the study area to inform preparation of a local structure plan and the future development of the area:

- Topography, soils (including acid sulfate soils), contamination;
- Vegetation, flora and fauna and bushfire risk;
- Water resources; and
- Heritage.

The following information has been provided on the basis of a desktop investigation only, using data and information that is publically available. No attempt has been made to ground-truth the information at this stage.

#### 1.3 Previous environmental assessments and key requirements

Limited environmental assessment has been undertaken for the site to date.

The following City of Belmont strategic and planning documents are considered relevant to this environmental report:

- City of Belmont Environmental Plan 2010-2016;
- City of Belmont Local Planning Scheme No. 15 scheme report supporting document:
   Environment:
- City of Belmont Local Planning Scheme No. 15 scheme report supporting document: Heritage.



- 1 - June 2018

- City of Belmont Local Planning Scheme No. 15 scheme report supporting document: Public Open Space.
- Belmont Foreshore Precinct Plan
- City of Belmont Street Trees Plan 2013.

There are a number of pieces of legislation, which govern management of the environment and have been considered as part of this assessment. These are listed in Table 1.

Table 1: Relevant environmental legislation

Legislation	Summary of relevant intent
Aboriginal Heritage Act 1972	Protects significant Aboriginal heritage, registered or unregistered.
Biosecurity and Agriculture Management Act 2007	Provides for the management of declared pests.
Contaminated Sites Act 2003	Requires the reporting of potential contaminated sites to the Department of Water and Environment Regulation.
Environment Protection and Biodiversity Conservation Act 1999 (Commonwealth) (EPBC Act)	Provides protection for Matters of National Environmental Significance (MNES).
Environmental Protection Act 1986	Provides protection for the environment as well as the licencing of prescribed premises and regulation of the clearing of remnant vegetation.
Fire and Emergency Services Act 1998	Provides for the management of bushfire risk.
Heritage of Western Australia Act 1990	Protection of places listed by the Heritage Council of WA.
Swan and Canning Rivers Management Act 2006	Establishes the Swan Canning River park and provides for the assessment of planning proposals within this area by the Swan River Trust Board.
Wildlife Conservation Act 1950 (WC Act)	Protects species of flora & fauna and communities that are listed.

The following environmental policies are also considered relevant to the management of potential environmental impacts on the site:

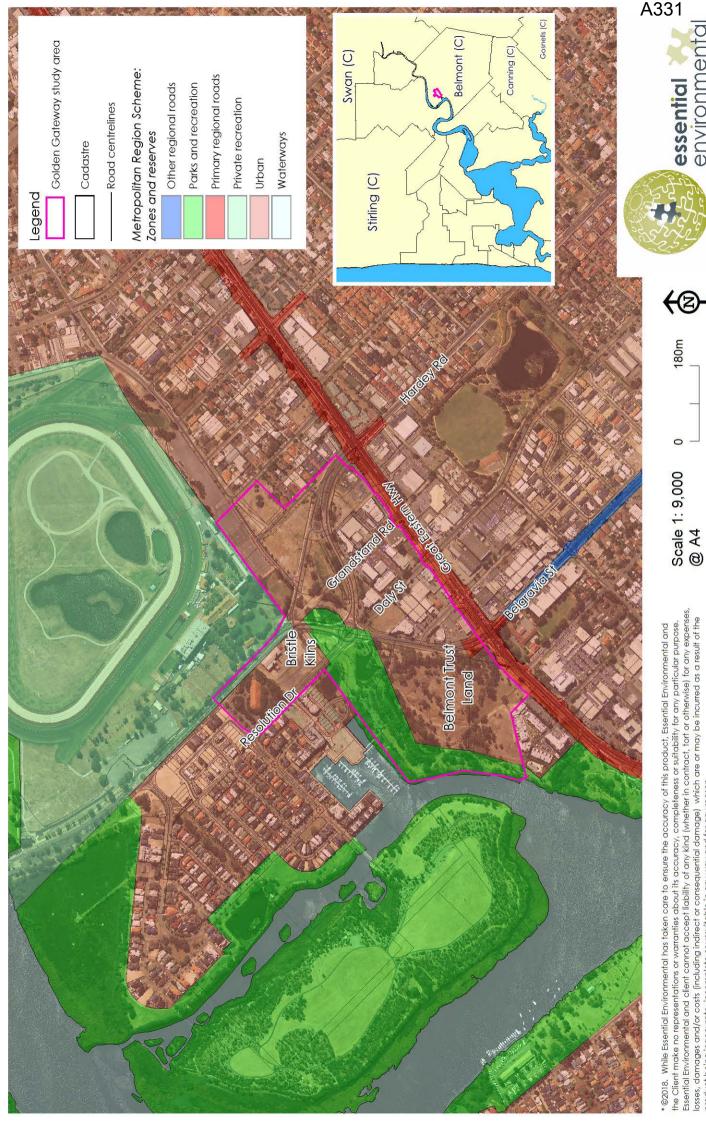
 EPA Guidance Statement No. 33 – Environmental Guidance for Land Development (EPA, 2008);

- 2 -

- Better Urban Water Management (WAPC, 2008); and
- Liveable Neighbourhoods (WAPC, 2011).



City of Belmont: Golden Gateway - Desktop environmental report Figure 1: Study area location



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## 2 PROPOSED DEVELOPMENT

## 2.1 Planning context

The majority of the study area is zoned 'Urban' under the Metropolitan Region Scheme, with a portion zoned for 'Mixed use' and reserved for 'Parks and Recreation' under the City of Belmont Local Planning Scheme No. 15 (Figure 2).

The City of Belmont Local Planning Scheme No. 15, adopted in December 2011, provides a district level framework to guide more detailed planning for the City. It requires local structure plans to be prepared to provide the level of detailed planning required to facilitate subdivision and development within the scheme area. The Western Australian Planning Commission and the City of Belmont are preparing a local structure plan to guide land use and development outcomes for the Golden Gateway precinct, the subject land.

The Golden Gateway Precinct includes a large portion of 'Mixed use' land, which encompasses the historical Ascot's Bristle beehive kilns and chimney stacks and portions of the Ascot Racecourse. The precinct also contains approximately 5.3 ha of Parks and Recreation reserve which covers the Belmont Trust Land. There is also a small portion of Parks and Recreation: water supply sewerage and drainage reserve, which is under the control of the

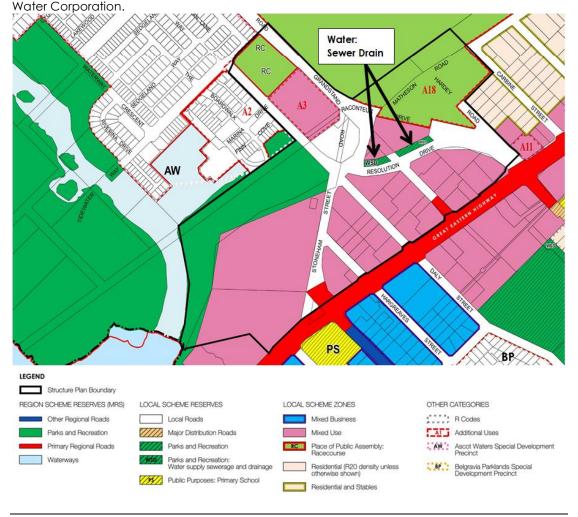


Figure 2: Local Planning Scheme



- 4 - June 2018

## 3 EXISTING ENVIRONMENT

A desktop environmental investigation of the study area has been undertaken, the findings of which are presented below.

#### 3.1 Land use context

#### 3.1.1 Historical land use

Historical aerial photography from Landgate suggests the land has been used for commercial purposes for over 50 years, with the majority of lots being approximately 1/3 hectare, accommodating warehouse facilities and such, predominantly adjacent to Great Eastern Hwy. The northern portion of the study area contains the Bristle kilns and Brick Works, which were established in 1929 and ceased operation in 1982 (Heritage Council, WA). The western portion of the study area, over the Belmont Trust Land, was historically used for sporting purposes such baseball fields (Clark, 1952), and more recently as a temporary worksite for development in the area, such as the widening of the Great Eastern Hwy.

#### 3.1.2 Current land use

Commercial property still exists adjacent to Great Eastern Hwy south of Resolution Dr and Stoneham St. East of Stoneham St, the Belmont Trust Land is largely cleared and vacant with large mature trees sparsely located though the middle of the reserve. The perimeter of the reserve is lined with small to large mature trees such as *Brachychiton acerifolius* (Illawarra Flame Tree) and *Eucalyptus grandis* (Flooded Gum).

The north and east of Resolution Dr contains a parcel of land approximately 5 hectares in size that is largely vacant, with the exception of a few mature trees, used as overflow parking servicing the Ascot Racecourse. This portion of land, as shown in Figure 2, also accommodates a 150 m Water Corporation open channel drain, which discharges via piped drainage under the Stoneham St/Resolution Drive roundabout into the Ascot Waters compensation basin on the north-western boundary of the study area. North of the Ascot Waters Compensation Basin is a second compensation basin servicing the Ascot Waters development. This compensation basin is herein referred to as 'Northern Drainage Lake'. The northern portion of the site contains the Perth Racing Administration Offices.

The Belmont Foreshore Precinct Plan (City of Belmont, 2014) was prepared to guide development and landuse within the river setting and ensure that the landscape values of the river system are conserved or enhanced. The study area, particularly Belmont Trust Land and public open space contains areas identified as parkland within the precinct plan, characterised by open lawns surrounding large individual trees. The precinct plan outlines strategic recommendations that will need to be incorporated into future planning of the Belmont Trust Land.

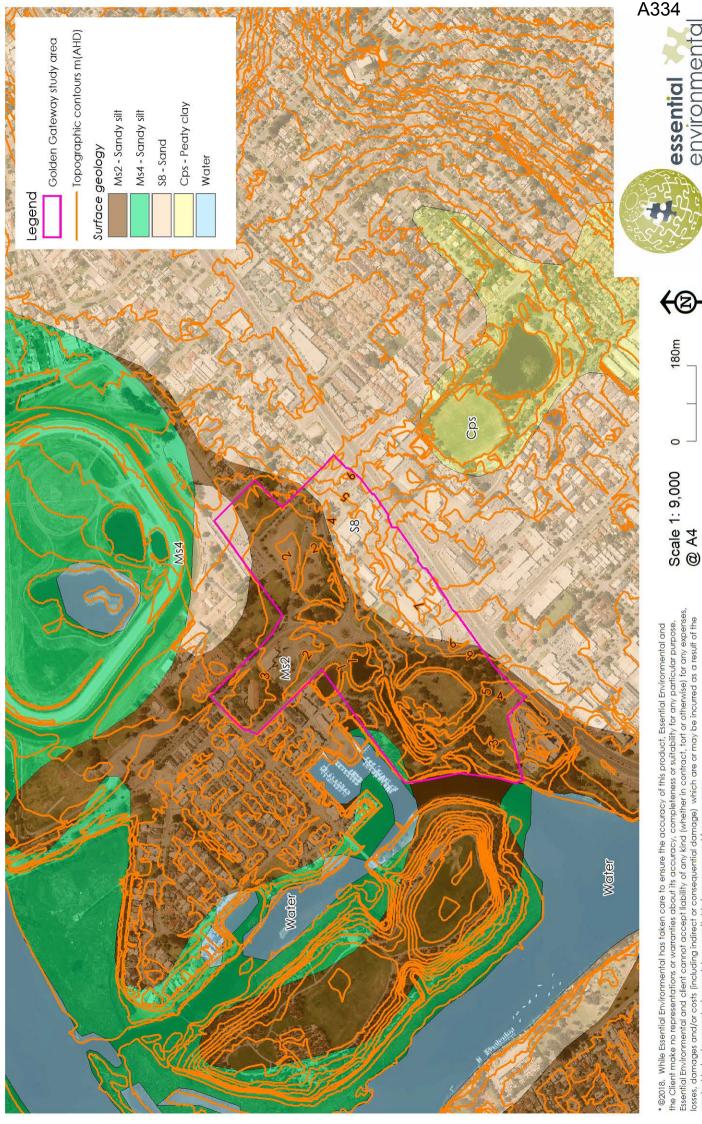
## 3.2 Topography, geology and soils

The study area has generally flat topography and grades gently from 6mAHD in the south-east to 3mAHD in the west. The study area has a few low points of approximately 1-2mAHD through the centre of the study area, as shown in Figure 3.



- 5 - June 2018

City of Belmont: Golden Gateway - Desktop environmental report Figure 3: Topography and surface geology



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Data source: DMP, Landgate. Created by: RM. Projection: MGA: zone 50.

#### 3.2.1 Geology

The surface geology is described broadly as Guildford formation: Alluvial sand and clay with shallow-marine and estuarine lenses and local basal conglomerate (WA surface geology 1:250,000 scale geological maps, Geological Survey of WA, and Geoscience Australia).

As shown in Figure 3, two-thirds of the north-western portion of the study area is classified as Ms2 – Sandy Silt, and the eastern third is classified as S8 – Sand, with a small portion of peaty clay adjacent to the Ascot Waters marina, described as follows:

- Ms2 Sandy Silt: strong brown to mild grey, mottled, blocky, disseminated fine sand, hard when dry, variable clay content. This soil type is historically resourced for clay bricks and tile manufacture. It has a low permeability and low potential for erosion. Sandy Silt has a low shrink swell potential, however is prone to flooding.
- S8 Sand: very light grey at surface, yellow at depth, fine to medium grained, sub
  rounded quartz, moderately well sorted. Sand of eolian origin is used for construction
  purposes with a high permeability and low erosion potential. Well drained given a low
  water table.
- Cps Peaty Clay: dark grey and black with variable sand content of lacustrine origin.
   This soil has low permeability, high erosion potential, and is prone to flooding.

#### 3.2.2 Acid sulfate soils

A review of Department of Water and Environmental Regulation acid sulfate soils (ASS) risk mapping identifies two-thirds of the study area, predominantly the area coinciding with surface geology Ms2-Sandy Silt (see 3.2.1), as containing a Class I 'high to moderate' risk of ASS and the remainder, coinciding with S8-Sand, classified as Class II 'moderate to low' risk occurring within 3 m of the natural soil surface (Figure 3).

In 2009, Douglas Partners undertook an Acid Sulfate Soil investigation and Waste Classification investigation to assess the soil conditions of the Ascot Water Compensation Basin because the City intended to increase the size of the basin. The results of the investigation indicate the basin contains ASS, which are generally located at and below the groundwater table (approximately 1.5 m below ground level) (Douglas Partners, 2009). Should the soil below the groundwater table be exposed or groundwater be lowered for future development, further investigation of ASS is likely to be required.

Consistent with Department of Water and Environmental Regulation guidelines, sites should be investigated for ASS if any of the following works are proposed:

- ASS disturbing subdivision or development that is subject to conditional approval requiring the investigation and management of ASS;
- soil or sediment disturbance of 100 m³ or more in an area depicted on an ASS risk map
  as Class I 'high to moderate risk of ASS occurring within 3 m of natural soil surface' (e.g.
  construction of roads, foundations, installation of underground infrastructure, drainage
  works, land forming works, dams and aquaculture ponds or sand or gravel extraction);
- soil or sediment disturbance of 100 m³ or more with excavation from below the natural
  watertable in an area depicted on an ASS risk map as Class II 'moderate to low risk of
  ASS occurring within 3 m of natural soil surface but high to moderate risk of ASS beyond
  3 m of natural soil surface';
- lowering of the watertable, whether temporary or permanent (e.g. for groundwater abstraction, dewatering, installation of new drainage, modification to existing drainage), in areas depicted in an ASS risk map as Class I 'high to moderate risk of



actual acid sulfate soils (AASS) or potential acid sulfate soils (PASS) occurrence' or Class II 'moderate to low risk of AASS or PASS occurrence within 3 m of natural soil surface':

- any dredging operations;
- extractive industry works (e.g. mineral sand mining) in any of the areas listed in Table 1
  of the guidelines; and
- flood mitigation works, including construction of levees and flood gates in any of the areas listed in Table 1 of the guideline.

Given the Class I classification for ASS, it is recommended that a self-assessment checklist is completed for the study area. Some investigation for ASS will be required if any of the above works are proposed in Class 1 areas. Investigations should be undertaken consistent with Department of Water and Environmental Regulation guidelines: Identification and investigation of acid sulfate soils and acidic landscapes (DER, 2015).

If ASS is found to be present at the site, all site works must be carried out in accordance with a Department of Water and Environmental Regulation-approved ASS management plan.

#### 3.2.3 Contaminated sites

#### **DWER Contaminated sites database**

A search of the Department of Water and Environmental Regulation Contaminated Sites database found no contaminated sites within the study area. Lot 5 Resolution Drive (160 Stoneham Street) is listed as "Possibly Contaminated, Investigation Required".

#### **Ascot Water compensation basin**

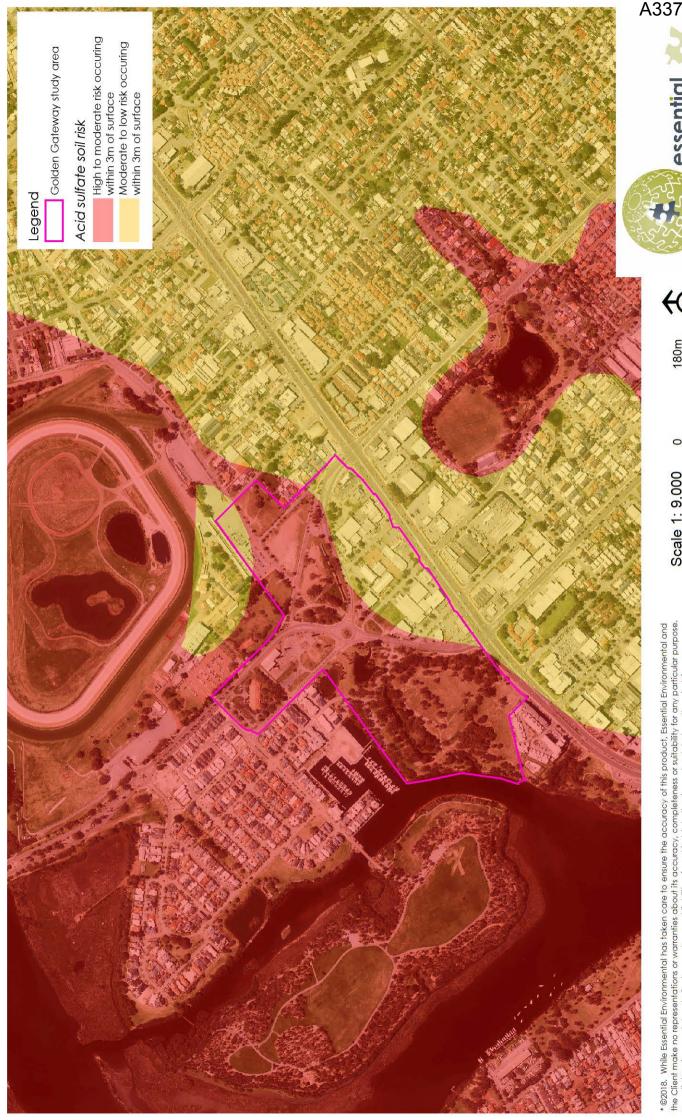
In 2009, Douglas Partners undertook a waste classification assessment at Lot 5 Stoneham St, corner of Resolution Dr and Stoneham St to assess the occurrence of acid sulphate soils; assess the nature and suitability of the soil for re-use; and assess the waste classification of the soil to be excavated, as the City of Belmont intended to increase the size of the current Ascot Waters Basin by approximately 4000 m<sup>2</sup>.

A Preliminary and Detailed Site Investigation (PSI/DSI) was also undertaken in 2012 (GHD, 2013), and a subsequent Site Management Plan was developed. Soil and groundwater contamination were investigated to assess risk to ecological and human receptors in accordance with the Department of Water and Environment Regulation. A summary of the contamination issues identified through these investigations are as follows:

- Soil Inorganic
  - Samples were tested for metals (As, Ca, Mn, Hg, Ni, Pb, Zn, Al, Fe). Exceedances of Ecological Investigation Levels (EIL, as per DER guidelines) were minimal, so metals were considered to be low risk to ecological receptors in the basin's current state. Metals were also below Health Investigation Levels (HIL-E), with the exception of lead. Further sampling indicated this was a localised test result.
  - Douglas Partners reported Asbestos Containing Materials (ACM) at several bores from 0 to 2 m below ground level (BGL). ACM was also found in samples collected at greater than 0.5 m BGL. However, no samples were taken near the surface profile (less than 0.3 m BGL) and the exposure pathway for the community or workers is considered incomplete. Overall, asbestos is considered low risk in its current state, however, further investigation needs to be undertaken.



City of Belmont: Golden Gateway - Desktop environmental report Figure 4: Acid sulfate soil risk



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Data source: DEC, Landgate, Created by: RM. Projection: MGA: zone 50.

Scale 1: 9,000 @ A4

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#### Organic

Both Douglas Partners and GHD reports indicate that encountered hydrocarbons were localised in nature and not considered to pose a risk to ecological or human receptors. However, works such as excavation would increase risk, and appropriate precautions should be taken.

#### Groundwater

#### Inorganic

Three groundwater bores were sampled to test for Fe, Zn, Ni, NH<sub>3</sub>, NO<sub>2</sub>, Total Nitrogen and Total Phosphorous. Concentrations of Zn, NH<sub>3</sub>, and Ni were reported marginally above ANZECC guidelines in all bores, and Fe concentrations were recorded 20 times above ANZECC guidelines. The exceedances are considered characteristic of winter conditions in the Swan River and natural soils in the locality (e.g. iron). Therefore, these results are not considered to reflect any potential risk to ecological or human receptors.

#### o Organic

 All samples were analysed for BTEX, Total Recoverable Hydrocarbons (TRH) and Polycyclic Aromatic Hydrocarbons (PAH). These were all reported below the DER Domestic Non-potable water criteria (GHD, 2013).

Based on these results, it is understood that the basin in its current state does not propose a risk to ecological or human receptors. Management guidelines provided in the Site Management Plan are effectively for the management of the basin expansion works and the City of Belmont have not proceeded with increasing the basin size.

## 3.3 Flora, fauna and vegetation

#### 3.3.1 Conservation areas

There are no Bush Forever sites within the study area.

Bush Forever site 313, Swan River Salt Marshes, exists to the north and west of the study area, as shown on Figure 5. The closest proximity of the Bush Forever site to the study area is adjacent to the Belmont Trust Land at the south-western boundary. Apart from this point, the study area is largely disconnected from the Bush Forever site.

An environmentally sensitive area, as mapped by the Department of Water and Environmental Regulation surrounds the Bush Forever site as described above. This area is described as 'Temperate Saltmarsh' and listed as 'vulnerable' under the *Environment Protection and Biodiversity Conservation Act 1999* (EPBC Act). Temperate salt marshes are an important habitat for local and migratory bird species (Department of Environment, 2015).

#### 3.3.2 Flora

Searches of the EPBC Protected Matters Search Tool and the former Department of Parks and Wildlife (now Department of Biodiversity, Conservation and Attractions) NatureMap database were undertaken to identify flora species of conservation significance potentially occurring within a 2 km buffer of the study area. Results are outlined in Table 2.



Table 2: Conservation significant flora likely to occur in the study area

Taxa	Common name	Conservation status	
		WC Act	EPBC Act
Dillwynia dillwynioides	-	Priority 3	
Johnsonia sericea	Waldjumi	Priority 4	
Caladenia huegelii	King Spider-orchid		Endangered
Darwinia foetida	Muchea Bell		Critically endangered
Lepidossperma rostratum	Beaked Lepidosperma		Endangered

#### 3.3.3 Fauna

Searches of the EPBC Protected Matters Search Tool and the Department of Biodiversity, Conservation and Attractions NatureMap database were undertaken to identify fauna species of conservation significance potentially occurring within a 2 km buffer of the study area. Results are outlined in Table 3.

Table 3: Conservation significant fauna known or likely to occur in the study area

Taxa	Common name	Conservation	n status
		WC Act	EPBC Act
Calyptorhynchus latirostris	Carnaby's Balck Cockatoo	Rare or likely to become extinct	Endangered
Calidris ferruginea	Curlew Sandpiper	Rare or likely to become extinct	-
Caretta caretta	Loggerhead Turtle	-	Endangered
Chelonia mydas	Green Turtle	-	Vunerable
Dermochelys coriacea	Leatherback Turtle	-	Endangered
Natator depressus	Flatback Turtle	-	Vunerable
Diomedea epomophora epomophora	Southern Royal Albatross	-	Vunerable
Dioedea exulans (sensu lato)	Wandering Albatross	-	Vunerable
Pachyptila turtur subantarctia	Fairy Prion	-	Vunerable
Thalassarche cauta steadi	White-capped Albatross	-	Vunerable
Dasyurus geoggroii	Chuditch	-	Vunerable
Actitis hypoleucos	Common Sandpiper	Protected under international agreement	-
Ardea modesta	Eastern Great Egret	Protected under international agreement	-
Merops ornatus	Rainbow Bee- eater	Protected under international agreement	-
Tringa nebularia	Common Greenshank	Protected under international agreement	-
Falco peregrinus	Peregrine Falcon	Specially protected fauna	-
Oxyura australis	Blue-billed Duck	Priority 4	

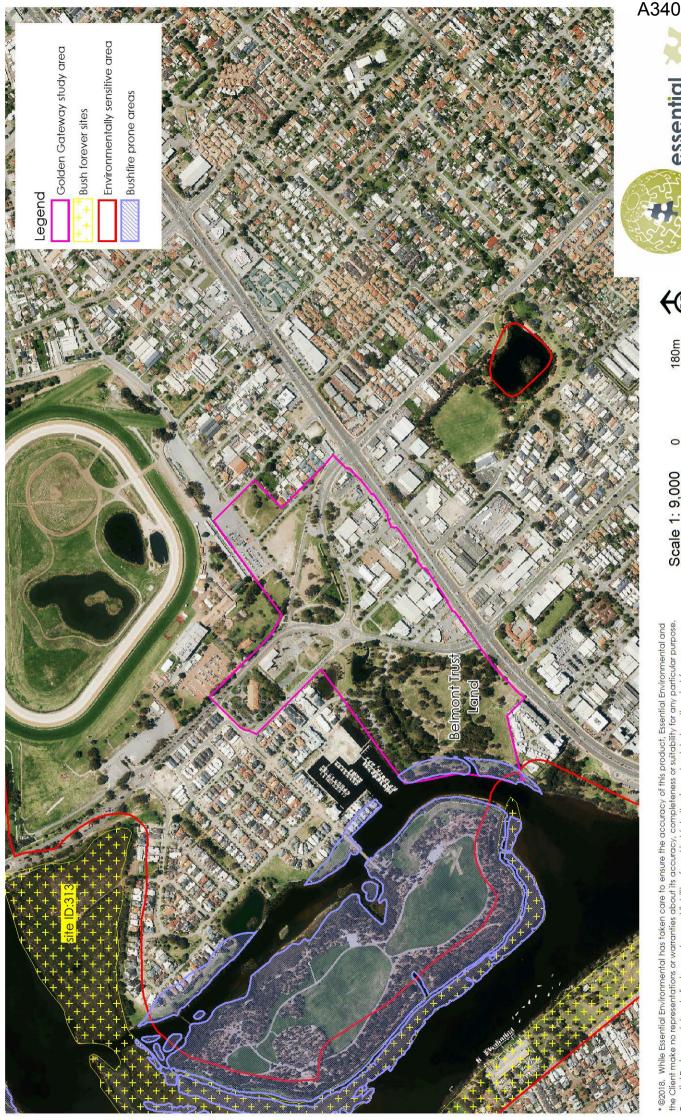
WC=Wildlife Conservation Act 1950

EPBC=Environmental Protection and Biodiversity Conservation 1999



- 11 - June 2018

City of Belmont: Golden Gateway - Desktop environmental report Figure 5: Vegetation



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#### 3.3.4 Bushfire risk

A portion of the study area along the banks of the Swan River is identified as a Bush Fire Prone Area (Figure 5), as designated by the Fire and Emergency Services (FES) Commissioner. Accordingly, any planning and development in the area must consider bushfire risk and the requirements of State Planning Policy 3.7: Planning in Bushfire Prone Areas (SPP 3.7) (2015).

The Guidelines for Planning in Bushfire Prone Areas (WAPC, 2015) refer to the need for a Bushfire Hazard Level assessment and Bushfire Attack Level Contour Map where possible to support strategic planning proposals in Bushfire Prone Areas. It is understood that this is being addressed separately from this report for the structure plan area.

#### 3.4 Water resources

#### 3.4.1 Public Drinking Water Source Area

There are no Public Drinking Water Source Areas within the study area.

#### 3.4.2 Surface water resources

The Swan River is adjacent to the western portion of the study area (Belmont Trust Land). The Swan River holds significant ecological value because it provides habitat for local and migratory birds and other fauna, with the majority of the River being identified as a conservation category wetland and environmentally protected area. Furthermore, the Swan River provides important social value for visual amenity, and recreation on the river and its reserves. The Swan River also holds significant Aboriginal and European heritage values.

The Department of Water and Environmental Regulation Floodway mapping indicates that a large area in the northern portion of the study area lies within the Swan River 100 year average reoccurrence interval (ARI) flood fringe (Figure 6).

A Water Corporation open drain exists at the centre of the study area. The open drain is approximately 150 m in length and directs runoff flows from the eastern urban and industrial areas to piped drainage under the Stoneham St/Resolution Drive roundabout to the Ascot Waters compensation basin (Figure 6). The compensation basin allows for dissipation of energy, mixing of water for oxygenation and sediment control before flowing through a further 350 m of open drain to the Swan River. A contaminated sites investigation was conducted by GHD and a Site Management Plan was subsequently developed in 2013 for the expansion of the compensation basin. The investigation identified issues of leachable metals, PAH and TPH fractions, and asbestos (see section 3.2.4).

North of the Ascot Waters Compensation Basin is a second compensation basin servicing the Ascot Water development, the Northern Drainage Lake. The Northern Drainage Lake has experienced water quality issues in the past with two fish kill incidents occurring during July and September 2012. The first incident involved approximately 300 fish deaths and the latter 100-150 fish deaths. No incidents have occurred since 2012. No water quality monitoring was undertaken by the City (pers, comm. Nicole Davey – City of Belmont coordinator-environment, 1 August 2016). However, investigations were undertaken by the Swan River Trust in 2012 in response to the fish kills. Water quality testing indicated low concentrations of algae, and higher concentrations of organic matter resulting in oxygen-depleted water. In addition, it was identified that fish often become trapped in backwaters such as this lake. It was concluded



that a combination of the above factors resulted in the fish kill incidents (pers. comms. Swan River Trust: Rivers Systems Branch, 23 August 2016).

A portion of the site is located within the Swan River Trust Development Control area (Figure 6). Land use planning and development within the Development Control Area is subject to approval of the Department of Biodiversity, Conservation and Attractions under Part 5 of the <a href="Swan and Canning Rivers Management Act 2006">Swan and Canning Rivers Management Act 2006</a> and the <a href="Swan and Canning Rivers">Swan and Canning Rivers</a> <a href="Management Regulations 2007">Management Regulations 2007</a>. This area includes the waterways of the Swan and Canning rivers and the adjoining parks and recreation reserves.

All development plans and applications for this area should be referred to Parks and Wildlife for advice in accordance with Clause 30A of the Metropolitan Region Scheme.

#### 3.4.3 Groundwater resources

The study area is within the Perth groundwater area and City of Belmont sub-area. The Department of Water and Environmental Regulation's Water Register shows no available allocation within the study area, as shown in Table 4.

Table 4: Groundwater resource allocation and availability (as of January 2016)

Management	Management	Resource	Allocation	Allocated	Remaining
Area	Sub Area		Limit	Volume	Volume
Perth	City of Belmont	Perth - Superficial Swan	1,497,000	2,243,830	-746,830

The City of Belmont currently has a groundwater licence allocation of 1,171,200 kL (licence no. 157042) located south-west of the study area along the Swan River.

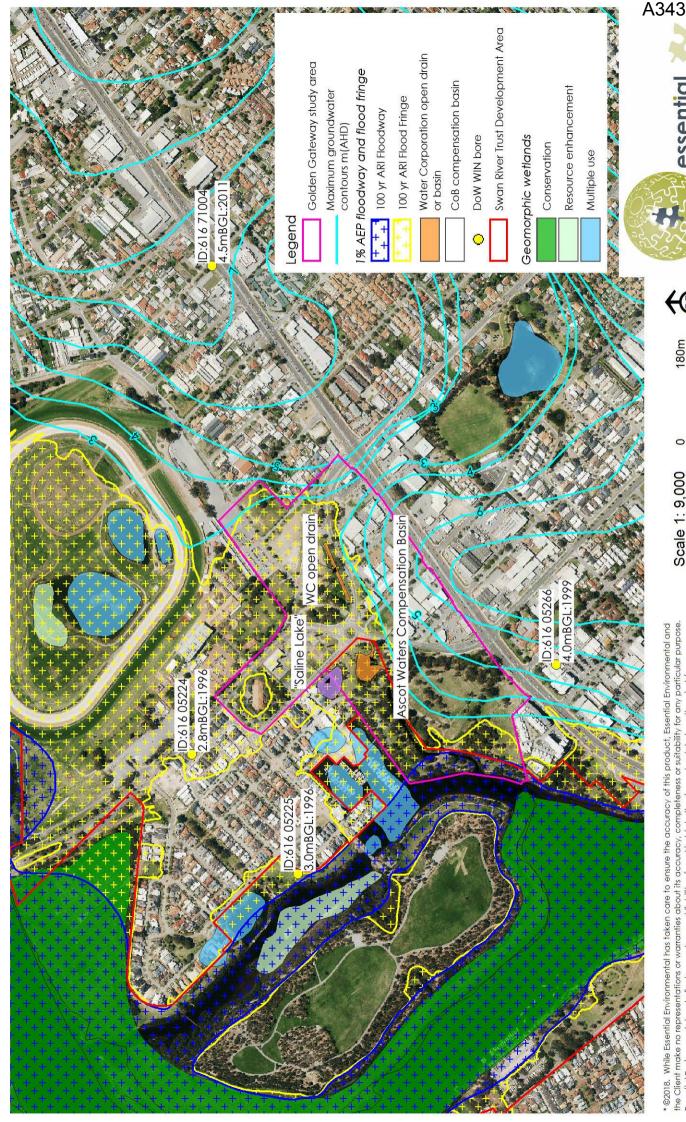
It can be inferred from the groundwater levels in the Department of Water and Environmental Regulation's Perth Ground Water Atlas that maximum groundwater levels are within 3 m of the natural surface through the northern and central portions of the study area, with groundwater flowing in a north-westerly direction toward the Swan River.

A search of the Department of Water and Environmental Regulation Water Information Network (WIN) bores showed a few bores located within the vicinity of the study area; however, none of the bores have current monitoring data. The most recently sampled bore was in 2011 (ID: 616 71004) situated 500 m east and hydrologically upstream of the study area showing a groundwater level 4.5 m below ground level (BGL). Consideration of this information together with that of another bore closer to the study area (ID: 616 05266), which has last recorded data from 1999 of 4 m BGL, indicates that the groundwater level may be lower than the mapped groundwater atlas level. Two other bores located north of the study area (ID: 616 05225 and ID: 616 05224), which have data from 1996 record groundwater at approximately 3 m BGL. These bores are part of the Ascot Waters development, which topographically sits approximately 2 m higher than the northern section of the study area and has been built-up for the purposes of the development. Therefore, it is reasonable to conclude that the groundwater level of these bores is less likely to be representative of the groundwater level within the study area than the surrounding locations.

It is noted that water resources and urban water management will be specifically addressed by the local water management strategy, which is being prepared to support the structure plan.



City of Belmont: Golden Gateway - Desktop environmental report Figure 6: Water resources



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## 3.5 Heritage

#### 3.5.1 Aboriginal heritage

A search of the Department of Planning, Lands and Heritage aboriginal heritage enquiry system showed one site overlaying the study area (Figure 7):

• Site ID 3753 – Registered site, Name: Perth, Type: Historical, mythological, hunting place, named place, natural feature.

One other site is adjacent to the study area, however not within the boundary, site ID 3536 - Registered site, Name: Swan River, Type: mythological.

#### 3.5.2 European heritage

The Bristle Kilns are beehive and tunnel kilns, with associated chimney and floor ducts, located at 197 Grandstand Rd Ascot. The Kilns were first built in 1930, manufacturing terracotta, stoneware and steel products. Production ceased in 1982 (Heritage Council, 2012). The Kilns and chimneys remain and were placed on the State Heritage List in 2003. The Bristle Kilns are a visually striking feature of the area and are viewed as an asset for restoration by the community (Strutt, 2015).



City of Belmont: Golden Gateway - Desktop environmental report Figure 7: Heritage



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product being inaccurate, incomplete or unsuitable in any way and for any reason. Data source: DAA, Heritage Council, Landgate. Created by: RM. Projection: MGA: zone 50.

#### 4 FINDINGS AND RECOMMENDATIONS

The following section presents findings of the desktop environmental assessment of the study area. It highlights a number of environmental issues, which should be considered as part of the preparation of the local structure plan and future development of the site. These relate primarily to:

- A portion of the site being within the Swan River Trust Development Control Area;
- Proximity to the Swan River and potential for offsite impacts on values;
- Bushfire risk;
- Contamination and water quality management in the compensating basins; and
- Soils and acid sulfate soils.

Key recommendations to address these issues are provided below.

## 4.1 Soils and topography

The north-western portion, approximately two-thirds of the largely undeveloped area, is classified as Sandy Silt (Ms2), which has a low permeability and will need to be considered with regards to runoff and stormwater disposal.

In order to reduce the potential for erosion and sediment transport to drains and the River, ground disturbing activities should be kept to a minimum and carried out 'as required' (in stages) immediately prior to lots being released for sale as part of a 'staged' development of the site. Where land is cleared, the area should be stabilised (e.g. through landscaping/stabilising materials/dust suppression) as soon as possible.

#### 4.2 Acid Sulfate Soils

Approximately two-thirds of the study area is mapped as being High to Moderate ASS Risk (<3 m from the surface). The WAPC Acid Sulfate Soils Planning Guidelines (WAPC, 2008) indicates that "acid sulphate soils are technically manageable in the majority of cases".

It is recommended that a self-assessment checklist is completed for the study area. ASS Investigation and, if required, Management Plans should be prepared at subdivision stage once the detailed design of the site is finalised. This should be undertaken in accordance with the Acid Sulphate Soils Guideline Series: Identification and Investigation of Acid Sulphate Soils and Acidic Landscapes (DER, 2015a) and Treatment and Management of Soils and Water in Acid Sulphate Soil Landscapes (DER, 2015b).

## 4.3 Surrounding land use and buffer requirements

The Swan River is the most important environmental attribute in proximity to the study area. Protection of the environmental values associated with the River requires consideration of compatible adjacent land uses that limit impacts. The provision of a 50 m buffer to the banks of the Swan River consistent with its designation as an environmentally protected area and conservation category wetland is generally applied.

Any proposal within the Swan River Trust Development Control Area that is likely to impact on the water quality and/or values of the Swan River should be referred to the Department of Biodiversity, Conservation and Attractions. It is recommended that consultation occur with the



Department of Biodiversity, Conservation and Attractions, Rivers and Estuaries Branch as part of the preparation of the local structure plan.

## 4.4 Vegetation and flora

The vegetation on the site is degraded and the site does not contain any areas with an intact understorey. No Declared Rare Flora are likely to be on the site and no priority species are likely to be present. It is recommended that no further vegetation assessment of the site is required and therefore, protected flora is not an impediment to the development of the area.

It is recommended that, as part of the detailed design process, any trees that can be retained in street verges, landscaped areas, parking areas and in road/entry areas should be identified and included in the detailed design plans for the area. Mature trees to be retained must be identified and clearly marked prior to commencement of any pre-construction activities.

#### 4.5 Fauna and habitat

Due to historic clearing, urbanisation activities, and lack of native remnant vegetation across the majority of the study area, particularly the understorey, any fauna habitat is considered of low value to native fauna. This is with the exception of the portion of the study area that abuts the Swan River, where the foreshore area may provide important habitat for local and migratory birds.

To minimise impacts to fauna resulting from any clearing activities, the following management strategies are proposed:

- During construction, the extent of authorised clearing will be clearly defined and demarcated to avoid accidental clearing;
- Loud noises (e.g. air horns) will be made just prior to commencement of clearing;
- Clearing works will occur in the direction of a conservation area where possible, to allow animals time to escape;
- If any injured or distressed fauna are encountered during site works the Site Supervisor
  will be instructed to immediately call the Department of Biodiversity, Conservation and
  Attractions' Wildcare Hotline (08) 9474 9055, to allow for the closest appropriate
  registered wildlife rehabilitator to attend the site; and
- Where possible, local native species will be planted along road verges and median strips in and near conservation areas and strategic ecological linkages to enhance the value of the linkage to fauna.

## 4.6 Flood protection, groundwater and water quality management

As the development is partly within the Swan River Trust Development Control Area, planning and development should consider Department of Biodiversity, Conservation and Attractions' Corporate policy statement no. 42: Planning for land use, development and permitting affecting the Swan Canning Development Control Area (June 2016) and other relevant policies. Development may be subject to a Part 5; Clause 30A(2)a or Clause 30A(2)b application process.

A portion of the study area is also within the 100 year ARI flood fringe. Any development in the flood fringe should not impact on the risk of upstream flooding.



Limited assessment of groundwater levels has been undertaken at this stage. As shown on Figure 6, the maximum groundwater contours from the Department of Water and Environmental Regulation's Perth Groundwater Atlas (2004) only extends to the southern portion of the study area and local groundwater bores have limited information. It is recommended to further investigate groundwater levels.

Surface water and groundwater management will be described in the Local Water Management Strategy and any future Urban Water Management Plans that will be prepared for each stage of development. Therefore, potential impacts on surface water and groundwater can be mitigated and managed in order to achieve the objectives of *State Planning Policy 2.9: Water Resources (WAPC, 2006)*.

A Local Water Management Strategy is being prepared in accordance with Better Urban Water Management (WAPC, 2008) to address the following:

- Identification of the site's current hydrological regime and existing environment;
- Identification of the constraints within the development area which may affect the design of the development with respect to urban stormwater drainage and management of groundwater;
- A description of the stormwater management strategy for minor and major events, including details on the proposed management practices to be employed;
- Identification and description of mechanisms to protect the water regime, including
  water quality and water levels. This will include a discussion of the overarching
  engineering principles that will be employed to mitigate any impact from run-off,
  groundwater and water quality issues, and ensure that the environment and the
  development will not be adversely impacted upon;
- Identification of the proposed water supply (including irrigation requirements) and wastewater disposal;
- Identification of monitoring requirements and derivation of agreed performance criteria for the urban water management system; and
- Identification of contingency measures to be implemented in the event that the system is not achieving agreed performance targets.

## 4.7 Heritage

A buffer area of a site of Aboriginal heritage has been identified to cross the boundary of the study area. All contractors working on the development will need to be made aware of their responsibilities under the *Aboriginal Heritage Act 1972* with regard to finding potential archaeological sites. In the event that a site is discovered, all work in the area will cease and the Department of Planning, Lands and Heritage will be contacted.

The Bristle Kilns are on the State Heritage list and future land use planning will need to take this into consideration.

## 4.8 Construction impacts

Construction activities need to be managed to minimise the impact to nearby Swan River, surrounding residents and the retained vegetation on-site. Impacts can include:

- Nuisance dust generation during bulk earthworks;
- Disturbance of ASS during earthworks and/or installation of services;



- Silt and sediment runoff to waterways and drains from uncontrolled runoff during site works:
- Inadvertent damage to trees and other vegetation earmarked for retention;
- Impacts to new stormwater drainage systems and existing environmentally sensitive areas from wind- and water-borne sediment during construction; and
- Inappropriate disposal of waste building material and poor housekeeping on building sites leading to wind-blown litter.

All of these potential impacts are manageable through appropriate engineering design and/or good site management practices.

## 4.9 Conclusion, constraints and opportunities

Several significant environmental constraints to the proposed development have been identified as follows:

- risks associated with urban stormwater runoff to the Swan River (sections 3 and 4);
- contamination risks associated with Ascot Water Compensation Basin, which will need to be considered if future work on the basin is to be undertaken (section 3.2.4);
- contamination risks associated with the Northern Drainage Lake, which may need to be considered because of previous fish kills in the lake (section 3.4.2); and
- the associated bushfire risk of the north-western portion of the study area, which will need to consider the requirements of *State Planning Policy 3.7: Planning in Bushfire Prone Areas (SPP 3.7) (2015) (section 3).*

Two figures have been developed to show the environmental constraints (Figure 8) and environmental opportunities (Figure 9). As summarised above, the environmental constraints include soil of low permeability and ASS risk to the majority of the study area. Further to this, the geology of the site may have created a perched groundwater table, and in conjunction with the close proximity of the study area to the receiving water body, groundwater levels are inferred to be close to the surface. The topography of the study area generally directs surface water flows toward the centre and south-westerly toward the Swan River, an environmentally sensitive area and conservation category wetland.

Although the Swan River is identified as an environmental constraint due to its protection requirements, the opportunities the River provides to the study area are of exceptional significance. The Swan River has long been valued for its social, recreational and visual amenity and would provide a substantial opportunity for increased land value. This can also be said for the mature trees within the study area, which provide visual amenity and urban heat island mitigation. Furthermore, deep rooted trees help maintain hydraulic control of the groundwater table by reducing recharge and using groundwater via transpiration, and promote soil stability and erosion control, especially at the river banks and at any other points where a water body receives inundation. The compensation basins identified in Figure 9 are also an opportunity for rehabilitation for improved visual amenity, flora and fauna habitat and upstream pre-treatment of surface and/or groundwater before discharge to the Swan River.



Groundwater within 3m of surface Environmentally sensitive area Directional surface water flow Golden Gateway study area Soil of low permeability and Water Corporation reserve High to moderate ASSR Bushfire prone area Water body egend City of Belmont: Golden Gateway - Desktop environmental report

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City of Belmont: Golden Gateway - Desktop environmental report Figure 9: Environmental opportunities



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Data source: Co8, Landgate. Created by: RM. Projection: MGA: zone 50.

## **5 IMPLEMENTATION STRATEGY**

Table 5 provides a preliminary schedule of activities, which should be undertaken at Preconstruction, Construction and Post-construction phases of the project to mitigate and manage potential impacts to the environment. This advice is based on the current predominantly desktop assessment contained within this report. More detailed management measures should be determined as part of more detailed investigation and planning as the proposed development progresses.

Table 5: Implementation strategy

Issue	Action	Frequency	Responsibility
Preconstruction phase			
Contamination	Complete preliminary site investigation for contamination in accordance with Contaminated Sites Act 2003 should areas of known contamination be disturbed.	Once	Developer
Acid sulfate soils	Complete self-assessment checklist and consider need for a preliminary site assessment.	Once	Developer - Consistent with DPLH and DWER guidelines
Vegetation and flora	Clearly delineate POS areas and trees to be retained.	Once	Licensed Surveyor (Developer)
Fauna and habitat	All site staff to participate in Environment, Health and Safety inductions, which provide requirements for management of significant fauna and reporting procedures for environmental incidents.	Once	Developer and Construction contractor
Water management	Refer the local structure plan to the Department of Biodiversity, Conservation and Attractions as it contains a portion of land within and abutting the Swan River Trust Development Control Area.	Once	Developer/City of Belmont
	A Local water management strategy will be completed and used as the basis for detailed design.		Developer, in accordance with SPP 2.9: Water
	Following approval of the LWMS, UWMP(s) will be prepared prior to subdivision for approval by City of Belmont.		Resources
Bushfire	A Bushfire Management Plan will be prepared to support the LSP.	Once	Developer, in accordance with
	The Bushfire Management Plan will be revised and implemented at subdivision.		SPP 3.7: Planning in Bushfire Prone Areas

Issue	Action	Frequency	Responsibility
Construction phase			
Soils and topography	Ground disturbing activities should be kept to a minimum and carried out 'as required' (in stages) immediately prior to lots being released for sale as part of a 'staged' development of the site.	Ongoing during construction phase.	Construction Contractor (Developer)
Contamination	Management of any identified contamination in accordance with the Contaminated Sites Act 2003.	Ongoing during construction phase.	Construction Contractor (Developer)
Acid sulfate soils	Management of any identified ASS consistent with DPLH and DWER guidelines.	Ongoing during construction phase.	Construction Contractor (Developer)
Vegetation and flora	Maintain markings and fencing around vegetation and trees to be retained. Cleared vegetation to be mulched and stored on site.	Ongoing during construction phase.	Construction Contractor (Developer)
Fauna and habitat	Undertake clearing in the direction of the river to allow fauna to escape.	Ongoing during construction phase.	Construction Contractor (Developer)
Water management	Manage sediment transport to waterways and drainage systems consistent with the LWMS.	Ongoing during construction phase.	Construction Contractor (Developer)
Aboriginal heritage	In the event a site is discovered, all work in the area will cease and the Department of Planning, Lands and Heritage will be contacted.	Ongoing during construction phase.	Construction Contractor (Developer)
Construction impacts	Ensure dust and sediment runoff is adequately managed.  Ensure appropriate waste disposal of building materials.	Ongoing during construction phase.	Construction Contractor (Developer)
Post construction phas	se		
Soils and topography	Landscape or stabilise cleared areas immediately.	Once	Construction Contractor (Developer)
Vegetation and flora	Inspect fencing (if applicable) and replace if required.  Ensure ongoing maintenance of	6 months Ongoing	Developer until hand over to City of Belmont
	retained vegetation and any revegetation areas / native landscaping prior to handover.	until handover.	



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## **Client: City of Belmont**

Report	Version	Prepared	Reviewed	Submitted to Client		
	by		by	Copies	Date	
Draft report	V1	RM	SSh	Electronic	2 March 2016	
Draft – amendments to study area	V2	RM	SSh	Electronic	31 August 2016	
Final	V3	SSh	HBr	Electronic	3 May 2018	
Amended Final	V4	SSh	HBr	Electronic	21 May 2018	
Final Amendments	V5	SSh	HBr	Electronic	25 June 2018	

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# Ordinary Council Meeting 26/02/19

Item 12.6 refers

**Attachment 19** 

**Movement and Access Strategy** 

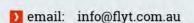






Golden Gateway

LOCAL STRUCTURE PLAN MOVEMENT AND ACCESS STRATEGY



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PROJECT	Golden Gateway Local Structure Plan Movement and Access Strategy 81113-160				
Revision	Description	Originator	Review	Date	
0	Issued for Review	MDR	CAS	07/12/16	
1	Draft Report for Review	MDR	CAS	08/03/17	
2	Revised Draft Report for Review	MDR	CAS	17/03/17	
3	Final Draft Report for Review	MDR	CAS	26/03/17	
4	Updated Final Draft Report for Review	MDR	CAS	07/04/17	
5	Updated Final Report with Revised Road Network Structure	MDR	CAS	04/05/18	
6	Updated Final Report with Revised LSP Images	MDR	CAS	17/06/18	





# Contents

1.	INTROD	DUCTION	5
1.	1 Mov	vement and Access Strategy	5
1.2	2 Stru	icture Plan	5
1.3	3 Key	lssues	6
1.4	4 Bac	kground Information	6
1.5	5 Rep	ort Structure	
2.	STRUCT	TURE PLAN OUTLINE	8
2.	1 Reg	ional Context	8
2.2	2 Prop	oosed Land Uses	9
3.	EXISTIN	ig situation	11
3.	1 Exis	ting Land Uses	11
3.2	2 Ped	estrian Network	11
	3.2.1	Pedestrian Infrastructure along Major Road Corridors	12
	3.2.2	Pedestrian Infrastructure along Minor Road Corridors	12
	3.2.3	Pedestrian Accessibility	12
3.3	З Сус	ling Network	13
3.4	4 Pub	lic Transport	14
3.5	5 Roa	d Network	15
	3.5.1	Regional Roads	17
	3.5.2	District and Local Roads	19
3.6	6 Exis	ting Traffic Volumes	21
	3.6.1	MRWA Traffic Counts Map	21
	3.6.2	MRWA Traffic Digest Information	22
	3.6.3	SCATS Traffic Count Data	22
	3.6.4	City of Belmont Local Road Traffic Count Data	24
	3.6.5	Collated Link Count Data	25
	3.6.6	City of Belmont Peak Hour Turning Movement Data	26
4.	INTERN	AL TRANSPORT NETWORKS	30
4.	1 Cha	inges to Existing Road Network	30
4.2	2 Role	e and Function of Key Roads	33
	4.2.1	Great Eastern Highway	33





	4.2	2.2	Stoneham Street	.33
	4.2	2.3	Resolution Drive	.33
	4.2	2.4	Daly Street (Main Street)	.34
	4.3	Road	d Hierarchy	.34
	4.4	Inte	rsection Controls	.36
	4.5	Pede	estrian/Cycle Network and Crossing Facilities	.36
	4.5	5.1	Existing Pedestrian/Cycle Facilities	.36
	4.5	5.2	Proposed Pedestrian/Cycle Facilities	.37
	4.6	Publ	lic Transport Routes	.42
	4.6	5.1	Existing Public Transport	.42
	4.6	5.2	Possible Future Public Transport	.42
5.	EX	TERN	AL TRANSPORT NETWORKS	.44
	5.1	Cha	nges to External Networks	.44
ĵ.	INT	EGR	ATION WITH SURROUNDING AREA	.45
	6.1	Majo	or Attractors and Generators	.45
	6.2	Stra	tegic Deficiencies	.46
7.	A١	IALYS	SIS OF INTERNAL TRANSPORT NETWORKS	.48
	7.1	Forn	n of Assessment	.48
	7.2	Asse	essment Years and Background Traffic	.48
	7.3	Mod	delled Transport Networks and Land Use Data	.50
	7.4	Time	e Periods for Assessment	.51
	7.5	Trip	Generation Assumptions	.52
	7.5	5.1	Residential Trip Generation	.52
	7.5	5.2	Commercial Trip Generation	.53
	7.5	5.3	Retail Trip Generation	.53
	7.6	Exist	ting Golden Gateway Traffic Generation	.53
	7.7	Futu	re Golden Gateway Traffic Generation	.54
	7.8	Futu	re Golden Gateway Traffic Distribution	.54
	7.9	Inte	rsection Assessment	.58
	7.9	9.1	Stoneham Street-Great Eastern Highway Intersection	.58
	7.9	9.2	Resolution Drive/Great Eastern Highway Intersection	.64
	7.9	9.3	Grandstand Road/Resolution Drive/Stoneham Street Intersection	.69
	7.9	9.4	Stoneham Street/Daly Street/Resolution Drive Intersection	.72





8.	An	alysis	of External Transport Networks	76
9.	SA	AFE ROUTES TO SCHOOL		77
9	1.1	Scho	ools Catchments	77
9	.2	Wall	k and Cycle Routes to Schools	77
	9.2	2.1	Belmont Primary School	78
	9.2.2		Belmont City College	79
10.		CON	CLUSIONS	81
1	10.1 Golden Gateway Local Structure Plan Co		den Gateway Local Structure Plan Context	81
1	0.2	Con	clusions	81
	10	.2.1	Background Growth in Traffic	81
	10	.2.2	Intersection Performance	81
	10	.2.3	Pedestrian, Cycle and Public Transport Networks	82
Appendix 1 - Transport Impact Assessment Checklist				84





## 1. INTRODUCTION

## 1.1 Movement and Access Strategy

This Movement and Access Strategy has been prepared by Flyt in support of the Local Structure Plan (LSP) which applies to the Golden Gateway Precinct in the City of Belmont. This Strategy has been prepared for the City of Belmont and Department of Planning (DoP) using the requirements set out within the Western Australian Planning Commission (WAPC) Transport Impact Assessment Guidelines (August 2016) Volume 2 – Planning Schemes, Structure Plans and Activity Centre Plans.

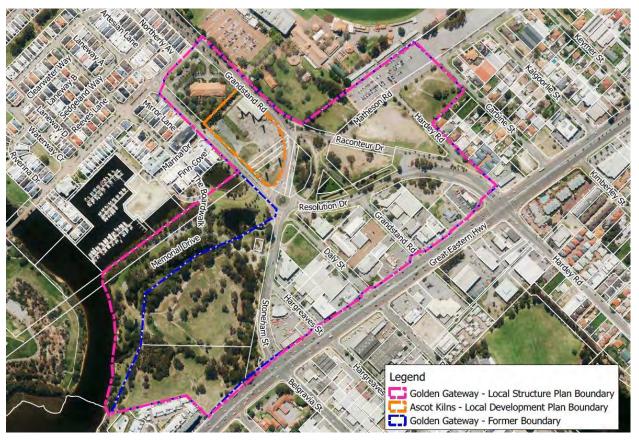
This form of guidance is the basis from which the WAPC and DoP assesses Structure Plans and therefore it should form the basis of this assessment which will result in the Movement and Access Strategy.

## 1.2 Structure Plan

The Golden Gateway Precinct is located within the City of Belmont and the LSP area is bounded by Ascot Racecourse to the north/northeast, Hardey Road to the east, Great Eastern Highway to the south, Swan River to the west and Ascot Waters to the west/northwest.

Figure 1 shows the Golden Gateway LSP boundary, as well as the former LSP boundary which was realigned in March 2016 to take into consideration the adjacent Metropolitan Regional Scheme (MRS) Reserve which contains water bodies associated with the Golden Gateway Precinct. In addition, Figure 1 shows the boundary of the Ascot Kilns Local Development Plan (LDP) area – traffic generated from the proposed Ascot Kilns LDP area has been considered within this assessment, however the form and function of vehicular access to this area was subject to consideration in the Ascot Kilns LDP reporting and not assessed within this report.

Figure 1 – Golden Gateway LSP Area (source: City of Belmont)







The Golden Gateway LSP has been developed by Taylor Burrell Barnett. As set out within the Structure Plan Report:

"The subject land encompasses a mix of uses comprising mixed business, retail (food and beverage), public uses associated with the Western Australian Turf Club, Ascot Racecourse and Ascot Kilns, Ascot Grove Farm Reserve and Swan River environs. The remainder of the subject land is largely undeveloped and devoid of vegetation".

The details of the land uses proposed are set out in Table 1 with the Structure Plan shown in Figure 2.

Table 1 – Proposed Structure Plan Land Uses

Item	Data
Estimated Number of Dwellings	3,000 dwellings
Estimated Dwelling Type - multiple dwelling (75 m² apartment size) - single dwelling	2,950 dwellings 50 dwellings
Commercial space	7,400 m <sup>2</sup> GFA
Retail space	1,500 m <sup>2</sup> GFA

# 1.3 Key Issues

The issues examined within this Movement and Access Strategy are:

- The impact of the Structure Plan on the local transport network based on the requirements set out in the Western Australian Planning Commission (WAPC) Transport Impact Assessment Guidelines (August 2016) Volume 2 Planning Schemes, Structure Plans and Activity Centre Plans;
- Addressing issues set out within the Structure Plan report and the form of development of the site; and
- Consideration of impact of development based on existing and future transport networks in the Golden Gateway locality.

# 1.4 Background Information

This Movement and Access Strategy has been structured to conform to the requirements of the WAPC Transport Impact Assessment (TIA) Guidelines, and has been completed using information from a range of sources which are quoted throughout this report. Specific to the completion of the TIA, the background information for the form and structure of the proposed development has been extracted from the Golden Gateway Structure Plan Report (April 2018) produced by Taylor Burrell Barnett.





Figure 2 – Golden Gateway Structure Plan (source: Taylor Burrell Barnett)



# 1.5 Report Structure

This Movement and Access Strategy has been structured to conform to the requirements of the WAPC Transport Impact Assessment Guidelines for the assessment of Structure Plan proposals. This introduction section forms the first of nine sections in this Movement and Access Strategy. The remaining sections cover:

- Structure plan outline;
- Existing situation;
- Internal transport networks;
- External transport networks;
- Integration with surrounding area;
- Analysis of internal transport networks;
- Analysis of external transport networks;
- Safer routes to schools analysis; and
- Conclusions.

Where referred to in the Movement and Access Strategy, appendices are provided as attachments.





## 2. STRUCTURE PLAN OUTLINE

## 2.1 Regional Context

The Golden Gateway LSP site is located in the City of Belmont and is bounded by Ascot Racecourse to the north/northeast, Hardey Road to the east, Great Eastern Highway to the south, Swan River to the west and Ascot Waters to the west/northwest. The LSP site includes the Belmont Trust Land (formerly Ascot Grove Farm Reserve), which currently consists of open parkland with a foreshore along the Swan River. Figure 3 shows the location of the site.

The site is located to the east of Perth City, along the southern foreshore of the Swan River. The site is located approximately 8km from Perth City, 4km from Perth Airport Domestic Terminal (Qantas), 9km from Perth International/Domestic Terminals and 3.5km from Belmont Forum Shopping Centre.

The site benefits from a surrounding movement network that features access to key regional road connections, a high frequency public transport corridor and high quality shared path pedestrian and cycling links.

The site benefits from good access to the regional road network, with Great Eastern Highway along the southern boundary of the site providing access west towards Perth City and onto South Perth, Melville and Fremantle via Canning Highway. To the east Great Eastern Highway provides access to Perth Airport and onto Guildford, Midland and the Swan Valley.

In addition, the site benefits from close proximity to the Garratt Road bridge crossing of the Swan River (approximately 1km north of the site), which provides access to Bayswater, Maylands, Mount Lawley and suburbs north of Perth City.

Ascot Racecourse is located immediately to the north/northeast of the site. The racecourse is regarded as Perth's premier racecourse and holds a number of featured Group Race meetings annually. These meetings attract crowds of varying sizes and on particular key race days (New Year's Day, Super Saturday in November etc.), vehicle access to the racecourse for spectators causes local parking issues and congestion.

A number of existing shared path cycling connections run through the LSP site alongside Stoneham Street, Raconteur Drive and Grandstand Road. Both shared paths and local bicycle friendly routes run through the Ascot Waters development to the north of the LSP site. The site is located close to regional cycling connections with the Graham Farmer Freeway Principal Shared Path (PSP) easily accessed via the shared path along the southern side of the Swan River.

A number of existing bus routes operate close to or through the LSP site. These include the Circle Route via Raconteur Drive/Grandstand Road providing connections north to destinations including Bayswater Station, Morley Bus Station/Shopping Centre and south to destinations including Belmont Forum Shopping Centre, Oats Street Station, Curtin University.

In addition, existing bus routes operate along Great Eastern Highway providing connections east to destinations including Perth Airport, Guildford, Midland and to the west to destinations including Victoria Park Transfer Station and Perth City.

Additional information on the road network surrounding the site are discussed in more detail in Section 3.5, additional information on cycling connections in proximity to the site are discussed in more detail in Section 3.3, and additional information on public transport connections in proximity to the site are discussed in more detail in Section 3.4.





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Figure 3 – Golden Gateway LSP Area Regional Context (source: nearmap.com)

# 2.2 Proposed Land Uses

The Golden Gateway LSP is comprised of three main land uses, residential dwellings, commercial space and retail space. It is proposed that the three land uses will primarily be provided in mixed-use development sites across the Golden Gateway LSP area. The split of the three land uses is shown in Table 2.

Table 2 - Proposed Structure Plan Land Uses

Item	Data
Estimated Number of Dwellings	3,000 dwellings
Estimated Dwelling Type - multiple dwelling (75 m² apartment size) - single dwelling Commercial space	2,950 dwellings 50 dwellings 7,400 m² GFA
Retail space	1,500 m <sup>2</sup> GFA

As noted in the Structure Plan Report, the LSP has been formulated around the following vision:

"The development of the Golden Gateway will transform this degraded and fragmented area into a vibrant precinct of residential and mixed use development, with strengthened connections to the Swan River and



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Ascot Waters, with uses, density and built form that derive best value from these attributes while respecting the area's rich culture and heritage."

The overarching objectives for the Golden Gateway Precinct as established by the project team and reinforced through stakeholder engagement include:

- Improve self-containment of facilities reduce car dependence;
- Improve peoples connection to the Swan River;
- Create accessible, quality public realm within the precinct; and
- Identify appropriate uses/densities in conjunction with infrastructure improvements.

In order to achieve the above objectives, the project team identified a number of opportunities that the Golden Gateway site presents, they include:

#### Land use:

- Opportunity for residential development to be accommodated in the precinct given the accessibility to high quality riverside amenity;
- Opportunity for retail convenience and food and beverage land uses to be integrated into development outcomes;
- Potential for higher density development given precinct location, proximity to high amenity open space destinations, Perth central business district, localised employment and high frequency public transport;
- o An existing primary school adjacent the precinct offers opportunity for family friendly dwelling diversity; and
- Opportunities to consider mixed use land use for development in core area to broaden activity opportunities and long term transition of the precinct.

#### Movement:

- Opportunity to utilise existing local street network of Hargreaves Street, Daly Street and Grandstand Road (south) to deliver a robust structure for future development access and vehicle circulation;
- o Generous existing road reserve dimensions provide ability for reconfigured pedestrian friendly streetscapes offering shade trees, soft landscaping and convenient on-street parking embayments;
- o Potential to consider alteration to the priority road network of Stoneham Street and Resolution Drive for the benefits of precinct consolidation and integration;
- o Opportunities to investigate alternative road alignments that celebrate key view lines of surrounding site characteristics and future gateway elements; and
- o Opportunities to investigate potential to downgrade priority of Stoneham Street for benefits to consolidation of development areas with existing foreshore open space.





#### 3. **EXISTING SITUATION**

#### **Existing Land Uses** 3.1

The site is bounded by Ascot Racecourse to the north/northeast, Hardey Road to the east, Great Eastern Highway to the south, Swan River to the west and Ascot Waters to the west/northwest, as shown in Figure 4.

The developed section of the LSP site, between Great Eastern Highway, Stoneham Street and Resolution Drive, consists of a range of light industrial/commercial units and various fast-food outlets fronting Great Eastern Highway. Other areas of the LSP site generally consists of undeveloped land.

As discussed in Section 1.2, the Ascot Kilns LDP area between Resolution Drive and Grandstand Road is subject to a separate LDP process, however traffic generated from the proposed Ascot Kilns LDP area has been considered within this assessment.

Immediately to the north of the Ascot Kilns site is the Perth Racing Headquarters building, which does form part of this assessment.

Figure 4 – Golden Gateway LSP Area in Context to Surrounding Development (source: Taylor Burrell Barnett) LEGEND Structure Plan Boundary



#### 3.2 Pedestrian Network

The extent and quality of the existing pedestrian infrastructure within and surrounding the Golden Gateway site is of a standard commensurate with the extent of existing development and form of land uses across the site, i.e. there are a number of existing undeveloped Lots and those that are developed primarily accommodate light industrial/ commercial unit style development. The existing local pedestrian infrastructure can be summarised as follows for the major road network and minor road network.





#### 3.2.1 Pedestrian Infrastructure along Major Road Corridors

Great Eastern Highway runs along the southern boundary of the LSP area and is a significant regional road connection within the Perth metropolitan road network. The road corridor features three general traffic lanes, a bus lane and an on-street bike lane in either direction, as well as wide, good quality footpaths on both sides of the corridor. Within the vicinity of the LSP site, crossing of Great Eastern Highway by pedestrians is facilitated via traffic signal controlled intersections at both Stoneham Street/Belgravia Street and Resolution Drive/Hardey Road intersections with Great Eastern Highway.

Each of the four major road corridors running through the Golden Gateway site (Grandstand Road, Raconteur Drive, Resolution Drive and Stoneham Street) all have footpaths along one side of the street – Grandstand Road along the eastern side of the street adjacent to the Ascot Racecourse, Raconteur Drive along the northern side of the street to connect to Grandstand Road, Resolution Drive along the eastern side of the street adjacent to the Ascot Waters development and Stoneham Street along the western side of the street adjacent to the Belmont Trust Land.

#### 3.2.2 Pedestrian Infrastructure along Minor Road Corridors

The minor roads within the LSP site are located between Great Eastern Highway and Resolution Drive/Stoneham Street, the three streets (Hargreaves Street, Daly Street and Grandstand Road - south section) provide access to the light industrial/commercial units in this area of the LSP.

None of these three streets have footpaths, which reinforces the fact that access to these land uses are primarily designed to be by private vehicle rather than on foot.

### 3.2.3 Pedestrian Accessibility

Walkscore is a commercial product that provides a geographical based rating score of a location based on availability of services within a walking catchment.

The Walkscore rating for a location central within the Golden Gateway LSP site is 43 out of 100 (the address used for the purposes of this analysis was 63 Daly Street). Walkscore measures the walkability of a location based on the distance to nearby places and pedestrian facilities, the overall scoring is ranked as follows:

**Daily errands do not require a car** 

70–89 Very Walkable: Most errands can be accomplished on foot
 50–69 Somewhat Walkable: Some errands can be accomplished on foot

25–49 Car-Dependent: Most errands require a car
 0–24 Car-Dependent: Almost all errands require a car

As such the centre of the LSP site is considered on the Walkscore ranking system to be 'Car-Dependent – most errands require a car'. Whilst the site benefits from potentially good access to primary school education, parks/reserves and some food and beverage outlets, the site is less well situated to access groceries and retail outlets on foot.

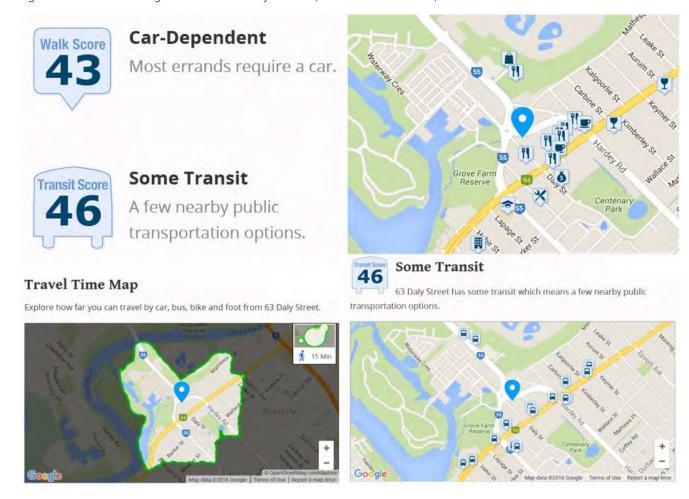
The LSP site also scores an average 46 out of 100 in terms of access to transit services (public transport services). The bus network is focused on serving the Great Eastern Highway corridor, which provides access to the employment centres of Perth City and Perth Airport, in addition the Resolution Drive corridor which provides access to Bayswater Station and the retail hub of Belmont Forum Shopping Centre.

The Walkscore ratings for a central location within the LSP site are summarised in Figure 5.





Figure 5 - Walkscore Rating for Golden Gateway LSP Site (source: walkscore.com)



# 3.3 Cycling Network

The extent and quality of the existing cycling infrastructure within and surrounding the Golden Gateway LSP site is of a high standard with local and regional links. The local and regional cycling network is shown in Figure 6.

Good on road cycling routes for experienced/confident cyclists have recently been provided adjacent to the Golden Gateway LSP site as part of the Great Eastern Highway upgrades. A number of high quality shared use paths providing connections within the LSP site along Stoneham Street, Raconteur Drive and Grandstand Road are provided.

In addition, high quality shared use paths from the LSP site are provided along the Swan River Foreshore (via Belmont Trust Land towards the Graham Farmer Freeway PSP to access Perth City), and along the shoreline within the Ascot Water development. Also streets within the Ascot Waters development are identified as local cycle friendly routes.





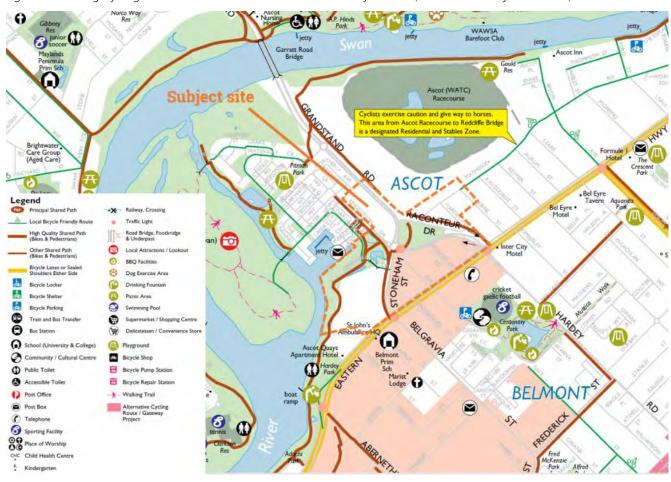


Figure 6 - Existing Cycling Network in Relation to the Golden Gateway LSP Site (source: DoT / City of Belmont)

# 3.4 Public Transport

Access to existing public transport facilities from the site is considered to be average at present, however there are options to make improvements to public transport access if land uses within the Golden Gateway site change over time to support additional public transport service provision.

As shown in Figure 7 the only bus routes that pass through the site are the recently re-numbered Circle Route bus service (98/99 became 998/999). The Circle Route buses operate through the LSP site along Grandstand Road and Resolution Drive. However, no bus stops for these services currently exist within the LSP site, the closest bus stops are located on Grandstand Road immediately to the north of the LSP area (close to the main pedestrian entry/exit to Ascot Racecourse).

Circle Route services provide a high frequency orbital public transport connection around Perth, linking inner suburbs, major activity centres, key land uses and public transport hubs including; Belmont Forum, Oats Street Station, Curtin University, Murdoch Activity Centre, Fremantle, Cottesloe, Claremont, UWA, QEII Medical Centre, Stirling Station and Morley Galleria.

The Circle Route operates on weekdays between 7am-9pm, with a weekday frequency of 10 minutes during the peak periods/daytime and 15 minutes outside of those time. The Circle Route also operates on weekends between 7am-7pm, with a 30 minute frequency. In total 131 weekday bus services operate on the Circle Route through the site – 64 weekday bus services towards Bayswater Train Station and 67 weekday bus services towards Belmont Forum. In addition, bus routes 36, 40, 295, 296 and 299 operate along Great Eastern Highway along the southern boundary of the site. All five of the bus routes serve Elizabeth Quay Bus Station, St Georges/Adelaide Terrace, Victoria Park



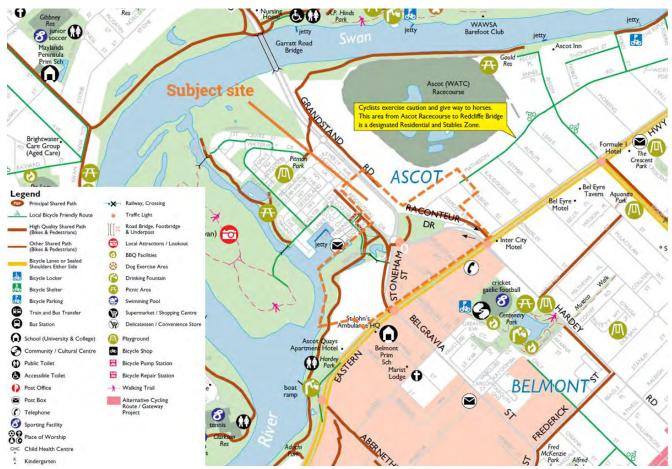


Transfer Station and Great Eastern Highway adjacent to the Golden Gateway LSP site. Additional details for each of the routes is provided below.

- Bus Route 36 continues along Great Eastern Highway and serves Guildford and Midland (terminating at Midland Station). Weekdays buses operate every 30 mins during the day and hourly at other times.
- Bus Route 40 continues along Great Eastern Highway and serves Perth Domestic Airport (Terminals 3 & 4) via Fauntleroy Avenue. Weekdays buses operate every 30 minutes.
- Bus Route 295, 296 and 299 continues along Great Eastern Highway, Great Eastern Highway Bypass, Kalamunda Road or Gooseberry Hill Road, Canning Road to Kalamunda (terminating at Kalamunda Bus Depot). Weekdays buses operate hourly.

In total 205 weekday bus services operate along Great Eastern Highway past the Golden Gateway site – 99 weekday bus services from Elizabeth Quay Bus Station to the bus stop adjacent to the site (and onto destinations to the east of the site), and 106 weekday bus services from the bus stop adjacent to the site towards Elizabeth Quay Bus Station via St Georges/Adelaide Terrace.

Figure 7 – Existing Public Transport Network in Relation to the Golden Gateway LSP Site (source: Transperth / City of Belmont)



## 3.5 Road Network

The localised road network surrounding the Golden Gateway LSP site includes major regional through routes (Great Eastern Highway to Canning Highway) coupled with a network of local distributor and access roads on either side of the Great Eastern Highway corridor.





Great Eastern Highway is classified as a Primary Distributor under the MRWA Functional Road Hierarchy, the details of which are set out in Table 3. Grandstand Road, Resolution Drive and Stoneham Street are classified District Distributor A roads and all other roads are classified as Access Roads. The overall local road hierarchy is shown in Figure 8.

The posted speed limit for Great Eastern Highway, Grandstand Road, Resolution Drive, Stoneham Street and Raconteur Drive is 60km/h, with all other roads in the immediate area of the Golden Gateway LSP site having a posted speed limit of 50km/h.

In addition, a 40km/h School Zone speed limit is in force to the south of Great Eastern Highway on Belgravia Street and Lapage Street, which run either side of Belmont Primary School. The posted speed limits extracted from the MRWA Road Information Mapping System is shown in Figure 9.

Table 3 - MRWA Road Hierarchy Criteria (source: MRWA)

#### METROPOLITAN ROAD HIERARCHY ROAD TYPES AND CRITERIA

				ROAD TYPES		
	CRITERIA AND ACTIVITY	PRIMARY DISTRIBUTOR	DISTRICT DISTRIBUTOR CATEGORY "A"	DISTRICT DISTRIBUTOR CATEGORY "B"	LOCAL DISTRIBUTOR/ INDUSTRIAL ROAD	ACCESS ROAD
1	Predominant Activity	Major networks e.g. freeways	Important network	Less important network	Minor network	Limited access to traffic, Forms part of local distribution network
2	Intersections	Controlled with appropriate measures e.g. grade separation, high speed traffic management measures	Controlled with appropriate measures E.g. traffic signals	Controlled with appropriate Local Area Traffic Management	Controlled with minor Local Area Traffic Management	Self controlling with minor measures
3	Indicative Traffic Volume (except semi- rural areas)	Above 15 000 vehicles per day	Above 8000 vehicles per day	Above 6000 vehicles per day	Maximum desirable volume: 6000 vehicles per day	Maximum desirable volume: 3000 vehicles per day
4	Frontage Access Allowed	None on Controlled Access Hwys Limited on other routes	Prefer not to have residential access and limited commercial access, generally via service roads	Residential and commercial access due to its historic status Prefer to limit when and where possible	Yes, except at intersections where side entry is preferred and traffic signals are involved	Yes
5	Pedestrians Allowed	Preferably none at grade. Crossing should be controlled	With positive measures for control and safety e.g. pedestrian signals	With appropriate measures for control and safety e.g. median/islands refuges	With minor safety measures	Yes
6	Recommended Operating Speed	60 - 110 km/h (depending on design characteristics)	60 - 80 km/h	60 – 70 km/h	50 - 60 km/h	50 km/h (desired speed)
7	Buses Allowed	Yes	Yes	Yes	Yes	If required
8	Parking Allowed	No	Generally no. Clearways where necessary	Not preferred. Clearways where necessary	Yes	Yes
9	Truck Routes	Yes	Yes	Yes	Only to service properties	Only to service properties
10	Responsibility	Main Roads Western Australia	Local Government	Local Government	Local Government	Local Government

Ideally, every road should meet all the criteria of one RH type.

However, many roads meet some of the criteria appropriate to different road types and are difficult to define.

Where precise definition of the road type is difficult, comparison with roads of similar role in other local government areas may assist.





Figure 8 - Local Road Hierarchy in Relation to the Golden Gateway LSP Site (source: MRWA)

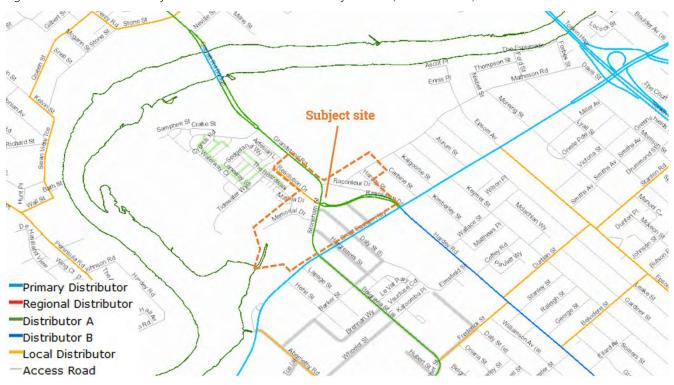
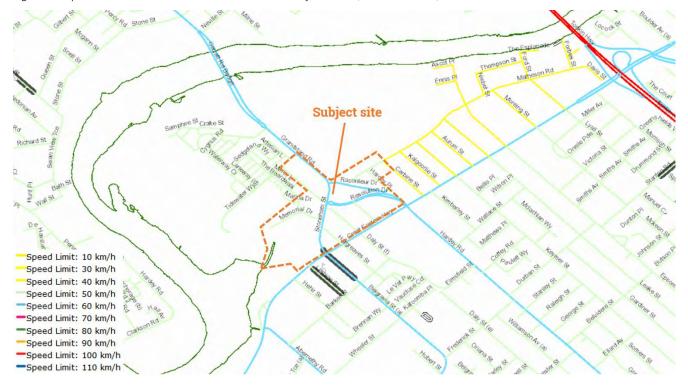


Figure 9 - Speed Limits in Relation to the Golden Gateway LSP Site (source: MRWA)



## 3.5.1 Regional Roads

The Golden Gateway site has Great Eastern Highway running along its southern boundary. Great Eastern Highway is one of the State's principal transport corridors carrying over 65,000 vehicles per day and is designated as a Primary Distributor under the control of MRWA.





Great Eastern Highway connects Perth Airport, and suburbs to the east of the airport, to central Perth. Between June 2011 and February 2013 Great Eastern Highway, from Kooyong Road in Rivervale to Tonkin Highway in Redcliffe, was subject to significant upgrade works. These works included:

- Widening Great Eastern Highway, from four to six lanes, between Kooyong Road (Rivervale) and Tonkin Highway (Redcliffe) a distance of 4.2km;
- Constructing a central median for the full length of the project;
- Upgrading all major intersections to include dedicated turning movements;
- Providing U-turn facilities at key locations in order to maintain access to businesses fronting Great Eastern Highway;
- Incorporating bus priority lanes into key intersections;
- Providing dedicated on-road cycling facilities;
- Constructing footpaths for pedestrians; and
- Relocating, replacing and protecting service utilities such as telecommunications, water, power and gas. Figure 10 shows the upgrade works completed by Main Roads WA (MRWA) in 2013. Figure 11 shows the Great Eastern Highway corridor adjacent to the Golden Gateway site before and after the works.

Figure 10 - Great Eastern Highway Upgrades – June 2011 to February 2013 (source: MRWA)

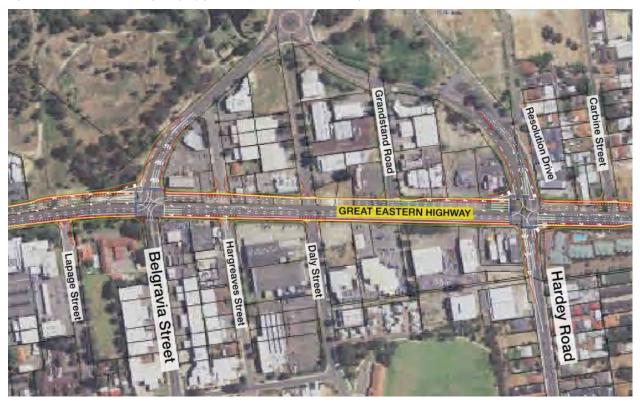


Figure 11 - Great Eastern Highway corridor adjacent to Golden Gateway Site – 2009 and 2015 view eastbound prior to Daly Street intersection (source: Google Streetview)









#### 3.5.2 District and Local Roads

The Golden Gateway site has a number of key district road connections running through the site, these include:

- Grandstand Road a four lane road with a central median, running north-south within the site, connecting the Garratt Road crossing of the Swan River with Great Eastern Highway via Stoneham Street or Resolution Drive see Figure 12;
- Stoneham Street a four lane road without a central median, running north-south within the site, connecting Grandstand Road/Resolution Drive with Great Eastern Highway and Belgravia Street see Figure 12; and
- Resolution Drive a four lane with a central median, running east-west within the site, connecting Grandstand Road/Stoneham Street with Great Eastern Highway and Hardey Road see Figure 12.

Grandstand Road, Stoneham Street, Resolution Drive and Belgravia Street are classified as District Distributor A roads, while Hardey Road is classified as a District Distributor B road. All of these roads are under the control of the City of Relmont

Figure 12 - Typical Layout of District Roads within the Golden Gateway Site (source: Google Streetview)











The Golden Gateway site has a number of local road connections running through the site:

- Hargreaves Street a two lane road without a central median, running northwest-southeast within the site, providing a connection between Stoneham Street (no right turn out) and Great Eastern Highway (left in/left out only) see Figure 13;
- Daly Street a two lane road without a central median, running northwest-southeast within the site, providing a connection between Stoneham Street (left out only onto Stoneham Street) and Great Eastern Highway (left in/left out only) see Figure 13;
- Grandstand Road (south) a two lane road without a central median, running northwest-southeast within the site, providing a connection between Resolution Drive and Great Eastern Highway (left in/left out only) see Figure 13;
- Raconteur Drive operates as a one-way road from Grandstand Road to Matheson Road and is currently closed at the Grandstand Road intersection outside of event periods at Ascot Racecourse. Two-way access between Resolution Drive and Matheson Road is possible via the eastern extent of Resolution Drive.

All of these roads are classified as Access Roads in Table 3 and are under the control of the City of Belmont.

Figure 13 - Typical Layout of District Roads within the Golden Gateway Site (source: Google Streetview)











# 3.6 Existing Traffic Volumes

A range of sources for traffic data have been examined in relation to the road network surrounding the Golden Gateway site, these were:

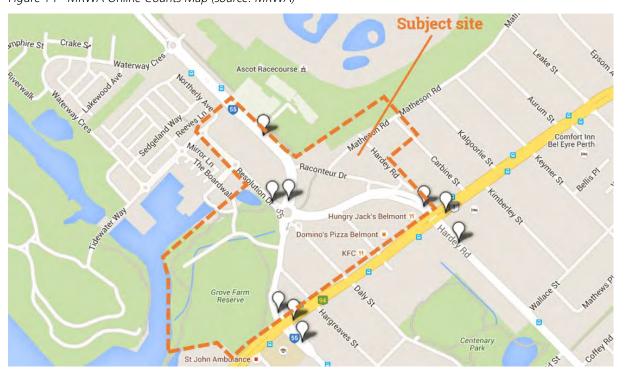
- MRWA online traffic counts map.
- MRWA online request for updated traffic digest information.
- MRWA online request for SCATS count data from the nearest traffic signal controlled intersections:
  - o Great Eastern Highway/Stoneham Street/Belgravia Street Intersection; and
  - o Great Eastern Highway/Resolution Drive/Hardey Road Intersection.
- City of Belmont traffic volume counts for the Golden Gateway local road network:
  - o Hargreaves Street (north and south);
  - o Daly Street (north and south); and
  - o Grandstand Road (north and south).
- City of Belmont peak hour turning volume count for the following intersection:
  - o Grandstand Road/Resolution Drive/Stoneham Street roundabout.

### 3.6.1 MRWA Traffic Counts Map

The MRWA online counts map was sourced through the MRWA Reporting Centre. Figure 14 sets out the count locations over the past decade where classified or volume counts have been collected by MRWA or others. The most recent two-way daily vehicle count volumes (vpd) for each count location within the Golden Gateway LSP site are provided below:

>	Garratt Road Bridge	2007/08	14,990 vpd
>	Grandstand Road (north of Raconteur Dr)	2008/09	8,670 vpd
>	Resolution Drive (north of Great Eastern Hwy)	2008/09	9,070 vpd
>	Stoneham Street (north of Great Eastern Hwy)	2004/05	12,850 vpd
)	Great Eastern Highway (east of Belgravia St)	2008/09	48,870 vpd
>	Great Eastern Highway (east of Hardey Rd)	2008/09	50,530 vpd

Figure 14 - MRWA Online Counts Map (source: MRWA)







### 3.6.2 MRWA Traffic Digest Information

Details from the MRWA online traffic digest were also reviewed to examine if any recent counts had been undertaken.

More recent counts than those displayed on the MRWA Online Counts Map are contained within the Traffic Digest Report for Resolution Drive, Stoneham Street and Great Eastern Highway (adjacent to the LSP site). The count details are displayed below:

Resolution Drive (north of Great Eastern Hwy)	2013/14	9,020 vpd
Stoneham Street (north of Great Eastern Hwy)	2013/14	15,170 vpd
Great Eastern Highway (west of Grandstand Rd)	2014/15	64,860 vpd
Great Eastern Highway (east of Hardey Rd)	2014/15	66,520 vpd

SCATS count data was also obtained from MRWA in order to verify the traffic volumes contained within the Traffic Digest Report (above). SCATS data is described in the following section.

#### 3.6.3 SCATS Traffic Count Data

An online request for SCATS data was submitted to MRWA for recent volume count information for the traffic signals at the intersections of:

- ☐ Great Eastern Highway/Stoneham Street/Belgravia Street; and
- ☐ Great Eastern Highway/Resolution Drive/Hardey Road.

Week day data was provided by MRWA for an average November 2015 period (16.11.15-20.11.15) and March 2016 period (08.03.16-11.03.16 and 14.03.16). These data collection periods were outside of school holidays and no major works or disruptions to the local or regional road network were noted.

SCATS data provides hourly volume of vehicles using each traffic lane through the intersection. Vehicle turning movements at an intersection can be determined based on the allocation of a movement from each traffic lane. Whilst this approach is accurate if only a single turning movement is permitted from a traffic lane, however if a traffic lane permits shared turning movements then on-site observations are required to determine an accurate split between the permitted turning movements.

On-site observations were completed on Wednesday 27 April 2016 and Thursday 28 April 2016. The on-site data collection was used to determine the split in turning movements from shared lanes and general operation of the two traffic signal controlled intersections and the approaches to these intersections.

Table 4 and Table 5 shows the peak hour turning volumes and percentages at the Great Eastern Highway/Stoneham Street/Belgravia Street intersection and Table 6 and Table 7 shows the peak hour turning volumes and percentages at the Great Eastern Highway/Resolution Drive/Hardey Road intersection.

Turning volumes show that in total between 5,150-5,750 vehicles travel through the Great Eastern Highway/Stoneham Street/Belgravia Street intersection during the peak hours and between 4,650-5,050 vehicles travel through the Great Eastern Highway/Resolution Drive/Hardey Street intersection during peak hours.





Table 4 - Great Eastern Highway / Stoneham Street / Belgravia Street Intersection — 2016 Turning Volumes (source: MRWA SCATS Data)

Time	Stoneh	am Street			Great E	astern Hig	_	st)	Belgrav	ia Street			Great E	astern Hig	hway (we	st)	
Period	Left	eft Ahead	Right	Total	Left	_eft Ahead ¯	Right	Total	Left	Left Ahead	Right	Total	Left	Left Ahead Right <b>Total</b>	Right	Total	TOTAL
0800- 0859	7	248	457	712	191	2,313	29	2,532	111	105	126	341	165	1,308	101	1,574	5,159
1600- 1659	6	170	269	448	77	1,636	35	1,747	64	428	217	710	537	2,227	70	2,834	5,739

Table 5 - Great Eastern Highway / Stoneham Street / Belgravia Street Intersection – 2016 Turning Percentages (source: MRWA SCATS Data)

	Total	%001	%001
way (west)	Right T	7% 1	2% 1
stern High	Left Ahead Right T	83%	%62
Great Ea	Left	10%	19%
	Total	100%	100%
	Right	37%	31%
ia Street	eft Ahead	30%	%09
Belgravi	Left	33%	%6
t)	Total	100%	100%
hway (eas	Right	1%	2%
astern Hig	Left Ahead Right	91%	94%
Great Ea	Left	%8	4%
	Total	100%	100%
	Right	64%	%09
am Street	eft Ahead	35%	38%
Stoneh	Left	1%	2%
Time	Period	0800- 0859	1600- 1659

Table 6 - Great Eastern Highway / Resolution Drive / Hardey Street Intersection – 2016 Turning Volumes (source: MRWA SCATS Data)

	TOTAL	4,666	5,038
st)	Total	1,359	2,469
thway (we	Left Ahead Right To	99	143
astern Hig	Ahead	1,271	2,289
Great E	Left	22	37
	Total	315	338
	Right	68	144
' Street	Left Ahead	138	141
Hardey	Left	88	53
÷	Total	2,663	1,951
eas) (eas	Right	132	163
astern Higł	eft Ahead	2,425	1,650
Great E	Left	106	139
	Total	329	280
	Right	61	29
ion Drive	Left Ahead	131	125
Resoluti	Left	137	127
Time	Period	0800- 0859	1600- 1659

Table 7 - Great Eastern Highway / Resolution Drive / Hardey Street Intersection — 2016 Turning Percentages (source: MRWA SCATS Data)

me	Resolut	ion Drive			Great E	astern Hig	hway (eas	££	Hardey	Street			Great E	astern Hig	hway (we	st)
riod	Left	eft Ahead	Right	Total	Left	Ahead Right T	Right	Total	Left	Ahead	Right	Total	Left	Left Ahead Right T	Right	Total
0800- 0859	42%	40%	18%	100%	4%	91%	2%	100%	78%	44%	28%	100%	2%	%86	2%	100%
00- 59	45%	45%	10%	100%	%/	85%	%8	100%	16%	42%	42%	100%	1%	%86	%9	100%





#### 3.6.4 City of Belmont Local Road Traffic Count Data

In order to gain a clear understanding of the level of traffic generated by existing land uses within the developed area of the Golden Gateway site, the City of Belmont collected traffic data for local roads during August 2016.

The City of Belmont utilised four traffic counters to collect volumetric traffic count data for six locations across two separate weeks. The following data was collected:

- Week 1: Daly Street and Grandstand Road
  - o Traffic counters installed at the northern and southern entry/exit to the local roads
  - Traffic volume data collected for 3 weekdays: Tuesday 16 to Thursday 18 August 2016
- Week 2: Hargreaves Street
  - o Traffic counters installed at the northern and southern entry/exit to the local road
  - o Traffic volume data collected for 3 weekdays: Tuesday 23 to Thursday 25 August 2016

Figure 15 shows the location of the traffic counters, as well as showing the average weekday traffic volumes at each of the count locations - Table 8, Table 9, Table 10 and Table 11 also present the traffic count data collected. Traffic data for Daly Street and Grandstand Road were collected simultaneously to ensure that any traffic moving between the two streets was collected as both an inbound and outbound movement into and out of the Golden Gateway site.

Figure 15 - Local Road Network Traffic Count Locations and August 2016 Average Weekday Traffic Volumes (map source: City of Belmont)









Table 8 - Hargreaves Street Traffic - August Weekday Traffic Volumes (source: City of Belmont Traffic)

Movement	Great Eastern Hwy	Stoneham St	TOTAL
IN	139	206	345
OUT	183	126	309
TOTAL	322	332	654

Table 9 - Daly Street Traffic - August Weekday Traffic Volumes (source: City of Belmont Traffic)

Movement	Great Eastern Hwy	Stoneham St	TOTAL
IN	288	104	392
OUT	178	365	543
TOTAL	466	469	935

Table 10 – Grandstand Road Traffic – August Weekday Traffic Volumes (source: City of Belmont Traffic)

Movement	Great Eastern Hwy	Resolution Dr	TOTAL
IN	410	641	1,051
OUT	428	517	945
TOTAL	838	1,158	1,996

Traffic data shows that Grandstand Road has approximately 2,000 daily traffic movements, this is double the number of daily traffic movements along Daly Street (approx. 950vpd) and three times more than the number of daily traffic movements along Hargreaves Street (approx. 650vpd).

The higher traffic volumes along Grandstand Road are primarily a result of the land uses at the southern end of Grandstand Road (fronting Great Eastern Highway), which are predominantly fast food outlets which generate a high volume of vehicle movements, particularly around lunch time and afternoon peak periods.

Hargreaves Street, Daly Street and Grandstand Road all have access to/from Great Eastern Highway to the south. To the north, Hargreaves Street and Daly Street have access to/from Stoneham Street and Grandstand Road has access to/from Resolution Drive. Table 10 shows that approx. 1,150 daily traffic movements access Grandstand Road to/from Resolution Drive, whilst Table 11 shows that approx. 800 daily traffic movements access Hargreaves Street and/or Daly Street to/from Stoneham Street.

This data shows that Grandstand Road carries higher volumes of traffic than Hargreaves Street and Daly Street combined. This is also true with total traffic to/from these local roads with Great Eastern Highway – Grandstand Road carries approx. 850vpd, whilst Hargreaves Street and Daly Street combined carry approx. 800vpd.

Table 11 – **Traffic to/from Stoneham Street from Hargreaves St and Daly St** – August Weekday Traffic Volumes (source: City of Belmont Traffic)

Movement	Great Eastern Hwy	Stoneham St	TOTAL
IN	427	310	737
OUT	361	491	852
TOTAL	788	801	1,589

#### 3.6.5 Collated Link Count Data

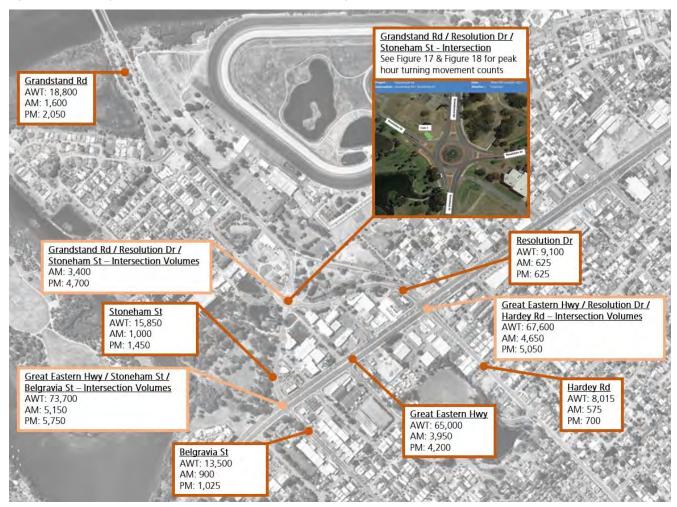
Figure 16 shows the collated link flow volumes around the Golden Gateway Precinct, in each instance the figure shows the most recent link flow volume, which are all from 2017. The data has been collated from two sources; recent count data provided by the City of Belmont and count data from the online MRWA traffic counts map.





The existing traffic count data shows two-way link volume data for average weekday traffic (AWT), for the AM peak hour of 0800-0900 (AM) and the PM peak hour of 1600-1700 (PM).

Figure 16 - Two-way Link Volume Traffic Count Data (sources: City of Belmont and Main Roads WA)



## 3.6.6 City of Belmont Peak Hour Turning Movement Data

In order to assist with gaining an accurate understanding of the operation of the Grandstand Road/Resolution Drive/Stoneham Street roundabout, a peak period turning movement survey was also undertaken.

The turning movement survey was completed using a video camera erected on a mask located between the Grandstand Road (northern leg of the intersection) and Resolution Drive (western leg of the intersection) – on the edge of the Ascot Kilns site.

The peak period turning movement survey collected full turning movement data at the roundabout (including u-turns), with the data collected for light and heavy vehicles in 15 minute time periods.

The turning movement data was collected on Wednesday 25 October 2017 during the following peak periods:

- M peak 0745-0915; and
- Mar. PM peak 1545-1715.

Figure 17 shows the turning movement data for the AM peak hour of 0800-0900, and Figure 18 shows the turning movement data for the PM peak hour of 1600-1700.





Figure 17 - Existing 2017 AM Peak Hour (0800-0900) Turning Volumes at Grandstand Road/Resolution Drive/Stoneham Street Intersection

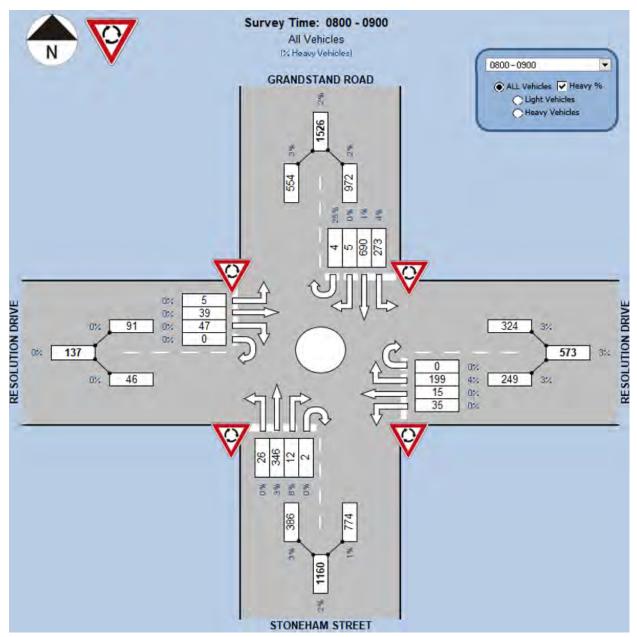
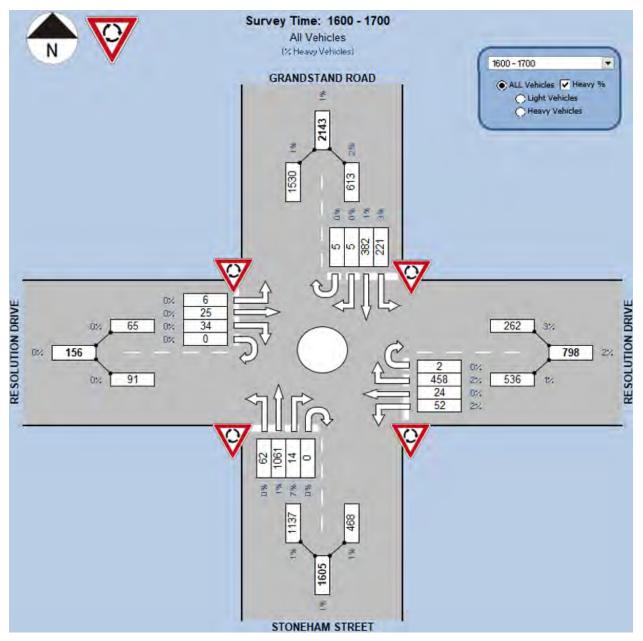






Figure 18 - Existing 2017 PM Peak Hour (1600-1700) Turning Volumes at Grandstand Road/Resolution Drive/Stoneham Street Intersection



A review of the video footage for the AM and PM peak hours shows that there was very limited queuing during these periods on the day of the survey.

Figure 19 shows a still image from the AM peak hour which shows two vehicles queuing on the Resolution Drive west arm (Ascot Waters access). During the AM peak hour on this approach to the intersection the queue did not exceed two vehicles and the maximum delay experienced was 20 seconds. The Resolution Drive east arm had a queue of 6 vehicles on two occasions in the peak hour and both times the queue cleared with a 15 second delay. At all other times the intersection operated with free flow conditions during the AM peak hour.

Figure 20 shows a still image from the PM peak hour which shows vehicles queuing on the Resolution Drive west arm (ascot Waters access). During a short 3 minute period within the peak hour this approach had a queue length of 6 vehicles with an average delay of 90 seconds. However, for the remaining 57 minutes of





the peak hour no queue exceeded 3 vehicles with maximum delays of 20 seconds. On occasions the Resolution Drive east arm and Stoneham Street south arm had queues of 6 vehicles (in the case of the Stoneham Street approach this was 6 vehicles in each of the two approach lanes), which cleared with a 15-20 second delay. At all other times the intersection operated with free flow conditions during the PM peak hour.

It should be noted that due to the viewing angle of the survey video camera, it is not possible to see the Grandstand Road approach to the intersection, and therefore it is not possible to comment on queueing on that approach to the roundabout.

Figure 19 - AM Peak Hour (0800-0900) Video Survey Image from 08:12



Figure 20 - PM Peak Hour (1600-1700) Video Survey Image from 16:51







#### 4. INTERNAL TRANSPORT NETWORKS

## 4.1 Changes to Existing Road Network

As set out in the Structure Plan Report, the movement network proposals include retention of the broad framework of the existing road network and primacy of existing major traffic movements. Within this movement network the Golden Gateway Precinct will be developed as a livable mixed use area with high levels of amenity, access to local retail, food and beverage outlets, public open space and the Swan River foreshore.

Figure 21 shows the Golden Gateway Structure Plan and the following changes to the existing road network are evident:

- Resolution Drive: Resolution Drive currently connects between the Great Eastern Highway/Hardey Road traffic signal controlled intersection and the Grandstand Road/Resolution Drive/Stoneham Street roundabout. This section of Resolution Drive will be realigned via the historical Raconteur Drive alignment.
  - o Resolution Drive will connect between the Great Eastern Highway/Hardey Road traffic signal controlled intersection and a relocated Grandstand Road/Resolution Drive/Stoneham Street roundabout adjacent to the Ascot Kilns LDP area.
  - o The Grandstand Road/Resolution Drive/Stoneham Street roundabout will be relocated approximately 125m to the northeast of its existing location, and will become a three arm roundabout
  - o The existing lane arrangements along the Grandstand Road and Resolution Drive corridors will be retained. As such, the Grandstand Road configuration of a four lane divided road (2 lanes in each direction) will be retained and the Resolution Drive two lane divided road (1 lane in each direction) will be retained with additional lanes on the approach and exit from the Great Eastern Highway intersection, as per existing lane arrangement.
  - o There will be a single intersection on the north side of Resolution Drive between Great Eastern Highway and the Grandstand Road/Resolution Drive/Stoneham Street roundabout. The intersection will be at a midway point along Resolution Drive and provide access to the northern area of the Golden Gateway Precinct and the Matheson Road corridor. Consequently, direct vehicle access onto Resolution Drive from future lots that abut the north side of Resolution Drive will not be permitted. The new intersection is expected to take the form of an all movements 'seagull' intersection with separate left turn/right turn lanes from the Golden Gateway Precinct and a right turn auxiliary lane from Resolution Drive into the Golden Gateway Precinct.
  - o Any limitation on direct vehicle access onto Resolution Drive, between Great Eastern Highway and the Grandstand Road/Resolution Drive/Stoneham Street roundabout, from the existing lots that abut the south side of Resolution Drive, will be subject to further consideration and any changes to the land assembly.
- Stoneham Street: Stoneham Street currently connects between the Great Eastern Highway/Belgravia Street traffic signal controlled intersection and the Grandstand Road/Resolution Drive/Stoneham Street roundabout. This section of road will be retained along its existing alignment, in addition Stoneham Street will be continued through to the new location of the roundabout 125m to the northeast of its existing location.





- Stoneham Street will connect between Great Eastern Highway/Belgravia Street traffic signal controlled intersection and Grandstand Road/Resolution Drive/Stoneham Street roundabout – adjacent to the Ascot Kilns LDP area.
- o The Grandstand Road/Resolution Drive/Stoneham Street roundabout will be relocated approximately 125m to the northeast of its existing location, and will become a three arm roundabout.
- o The section of Resolution Drive that provides access to the Ascot Waters development and currently creates a four arm roundabout with Grandstand Road and Stoneham Street, will now form a four-way traffic signal controlled intersection with Stoneham Street. The northern section of Daly Street within the Golden Gateway Precinct will be realigned to create the four arm intersection.
- o The existing lane arrangement along Stoneham Street will be retained. As such, the Stoneham Street configuration of a four lane undivided road (2 lanes in each direction) will be retained either side of the proposed four way traffic signal controlled intersection with Resolution Drive (Ascot Waters access road) and Daly Street.
- o The existing lane arrangements along Resolution Drive (Ascot Waters access road) and Daly Street will be retained. As such, these two roads will remain as two lane divided roads (1 lane in each direction), with a short (15m) left turn slip lane at the traffic signal controlled intersection (with the left turn slip operating under give-way control).
- The traffic signal controlled intersection of Stoneham Street/Resolution Drive (Ascot Waters access road)/Daly Street, would feature signal controlled pedestrian crossing facilities across all four approaches at the intersection. This will provide safe pedestrian crossing facilities to connect the Golden Gateway Precinct with Ascot Waters and the amenity provided by the Swan River foreshore.
- o The existing access arrangement at the Hargreaves Street and Stoneham Street intersection will be retained, as such this intersection will remain left-in/left-out only.
- o The existing access arrangements from Stoneham Street to access the riverside car park within the Belmont Trust land will remain unchanged. As such the existing all movements access arrangements will remain and the access road to the riverside car park with remain unchanged as a single lane access road in each direction.
- Hargreaves Street, Daly Street and Grandstand Road (south): the minor roads of Hargreaves Street and Daly Street currently connect between Great Eastern Highway and Stoneham Street, whilst Grandstand Road (south) connects between Great Eastern Highway and Resolution Drive. It is proposed that Hargreaves Street and Daly Street will continue along their existing alignments and connect between Great Eastern Highway and Stoneham Street. Grandstand Road (south) will be realigned at a midway point to connect into Daly Street.
  - o Hargreaves Street, Daly Street and Grandstand Road (south) intersections with Great Eastern Highway will be retained as per the existing left-in/left-out arrangement.
  - o The existing access arrangement at the Hargreaves Street and Stoneham Street intersection will be retained, as such this intersection will remain left-in/left-out only.
  - o Daly Street will form a four-way traffic signal controlled intersection with Stoneham Street and Resolution Drive (Ascot Waters). The northern section of Daly Street within the Golden Gateway Precinct will be realigned to create the four arm intersection.
  - o The traffic signal controlled intersection of Stoneham Street/Resolution Drive (Ascot Waters access road)/Daly Street, would feature signal controlled pedestrian crossing facilities across all four approaches at the intersection. This will provide safe pedestrian crossing facilities to connect the Golden Gateway Precinct with Ascot Waters and the amenity provided by the Swan River foreshore.





- Grandstand Road (south) intersection with Daly Street would be priority controlled with Grandstand Road (south) being the minor leg of the intersection.
- o Hargreaves Street, Daly Street and Grandstand Road (south) would take the form of two lane roads (1 lane in each direction). On-street parking will be provided along each street where appropriate.
- Matheson Road: Matheson Road provides access to residential/stable land holdings located between Ascot Racecourse and Great Eastern Highway. A connection between Matheson Road and Great Eastern Highway is made via a number of north-south connecting streets, and connection from Matheson Road to Resolution Drive is via a modified road connection.
  - Matheson Road will continue to connect through to a realigned Resolution Drive via a modified road network, which will provide access to new development Lots but not prioritise Matheson Road as a through route.
  - The form of intersection between Matheson Road and Resolution Drive will be subject to further detailed design. It is expected that this intersection would be priority controlled with Matheson Road being the minor leg of the intersection (in the form of a seagull intersection). This will ensure the Grandstand Road/Resolution Drive corridor is the primary route through the Golden Gateway Precinct.
  - Matheson Road and internal development roads would take the form of two lane roads (one lane in each direction). On-street parking will be provided along streets where appropriate.



Figure 21 – Golden Gateway Structure Plan Road Network and Built Form (source: Taylor Burrell Barnett)







## 4.2 Role and Function of Key Roads

### 4.2.1 Great Eastern Highway

The Great Eastern Highway corridor will present itself as a strong, unified commercial and mixed-use edge to the Golden Gateway development.

Commercial uses will be encouraged at ground-level and above with residential development to occupy upper storeys.

Building(s) at the junctions of Stoneham Street and Resolution Drive will feature a distinctive and iconic built form which mark an 'arrival' point to the Golden Gateway.

Great Eastern Highway will remain in its current form and no changes are proposed to the existing road connections with Great Eastern Highway nor the form of intersections between Great Eastern Highway and connecting roads.

#### 4.2.2 Stoneham Street

Stoneham Street will be the primary interface between the Golden Gateway development and the Swan River. It is proposed that future planning for the Belmont Trust Land, located to the west of Stoneham Street, should ensure strong physical links are maintained between the Swan River and future Golden Gateway population and workforce.

Stoneham Street will continue to be a major district road corridor and provide for high capacity traffic movements. The form of Stoneham Street will be retained as a four lane divided road (two lanes in each direction) with a median strips on approaches to main intersections and a painted dividing line mid-block. Indented on-street parking may be considered adjacent to the Golden Gateway development at detailed design stage to support local ground floor commercial activities along the corridor.

The intersection of Stoneham Street/Resolution Drive (Ascot Waters access road)/Daly Street will operate as a traffic signal controlled intersection. The intersection would feature signal controlled pedestrian crossing facilities across all four approaches. This will provide safe pedestrian crossing facilities to connect the Golden Gateway Precinct with Ascot Waters and the amenity provided by the Swan River foreshore.

Development addressing Stoneham Street is to provide an appropriate interface to the Belmont Trust Land to ensure a high standard of visual amenity and surveillance within a mixed-use environment.

#### 4.2.3 Resolution Drive

Resolution Drive is intended to accommodate primarily residential uses with the potential commercial uses at ground level. Ground level development on the south side may be residential or commercial but will be designed to accommodate non-residential use.

Resolution Drive will be realigned along the Raconteur Drive alignment and the existing Grandstand Road/Resolution Drive/Stoneham Street roundabout will be relocated approximately 125m to the northeast of its existing location, and will become a three arm roundabout.

The existing lane arrangements of Grandstand Road and Resolution Drive will remain unchanged with the same configuration of lanes on approach and exit to the relocated Grandstand Road/Resolution Drive/Stoneham Street roundabout.





Direct vehicle access onto Resolution Drive will not be permitted for future lots which abut the north side of Resolution Drive. However, any vehicle access restrictions for existing lots on the south side of Resolution Drive will be subject to further consideration and any changes to the land assembly.

Trees will line either side of Resolution Drive to create an attractive pedestrian environment connecting with the central open space area.

#### 4.2.4 Daly Street (Main Street)

It is proposed that Daly Street will become the community centre for the development. The precinct is intended to perform a local centre function, anchored by a small supermarket and supported with local specialty shops, and restaurant/cafes. Retail development must present a 'Main Street' character, with active edges to the street.

It is proposed that a leafy boulevard will provide a shared vehicle pedestrian space, providing a pleasant public realm against an active street frontage.

Daly Street will take the form of two lane roads (one lane in each direction). On-street parking will be provided along each street within the Golden Gateway Precinct where appropriate.

## 4.3 Road Hierarchy

Figure 22 shows the proposed road hierarchy. This is also explained in more detail below.

- Primary Distributor:
  - o Great Eastern Highway would be retained as a Primary Distributor road
- Distributor A:
  - Stoneham Street would be retained as a Distributor A road
  - Resolution Drive/Grandstand Road would be retained as a Distributor A road
  - Belgravia Street would be retained as a Distributor A road
- Distributor B:
  - Hardey Road would be retained as a Distributor B road
- Local Distributor:
  - Resolution Drive (Ascot Waters access road) between Stoneham Street and Tidewater Way would be retained as a Local Distributor road
- Access Roads:
  - o Hargreaves Street would be retained as an Access Road
  - o Daly Street would be retained as an Access Road
  - o Grandstand Road (south) would be retained as an Access Road
  - o Matheson Road would be retained as an Access Road





Figure 22 – Golden Gateway Structure Plan Road Hierarchy (source: Taylor Burrell Barnett)



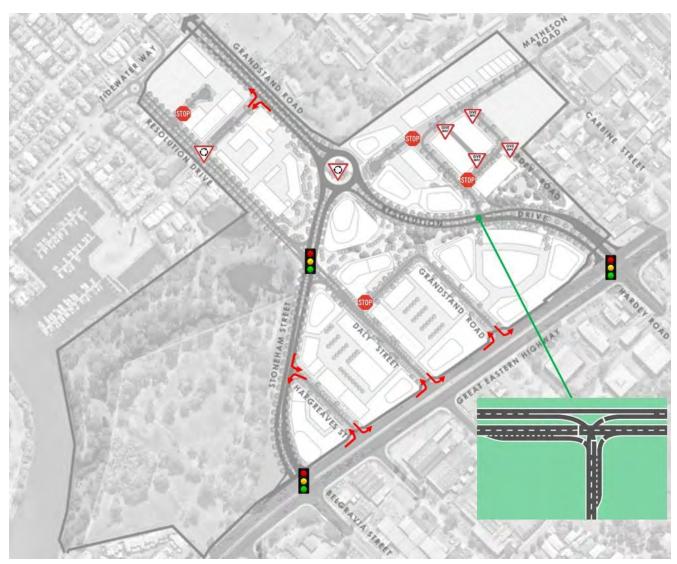




## 4.4 Intersection Controls

The forms of proposed intersection controls detailed in Section 4.1 are shown Figure 23.

Figure 23 - Proposed Intersection Controls for Golden Gateway Development



# 4.5 Pedestrian/Cycle Network and Crossing Facilities

## 4.5.1 Existing Pedestrian/Cycle Facilities

The extent and quality of the existing pedestrian infrastructure within and surrounding the Golden Gateway site is of a standard commensurate with the extent of existing development and form of land uses across the site, i.e. there are a number of existing undeveloped Lots and those that are developed primarily accommodate light industrial/commercial unit style development. The existing local pedestrian infrastructure is summarised as follows for the major road network and minor road network.

- Major Road Corridors:
  - o Great Eastern Highway within the vicinity of the LSP site, the crossing of Great Eastern Highway by pedestrians is facilitated via traffic signal controlled intersections at both





- Stoneham Street/Belgravia Street and Resolution Drive/Hardey Road intersections with Great Eastern Highway.
- o Grandstand Road, Raconteur Drive, Resolution Drive and Stoneham Street all have footpaths along one side of the street Grandstand Road along the eastern side of the street adjacent to the Ascot Racecourse, Raconteur Drive along the northern side of the street to connect to Grandstand Road, Resolution Drive along the eastern side of the street adjacent to the Ascot Waters development and Stoneham Street along the western side of the street adjacent to the Belmont Trust Land.

#### Minor Road Corridors:

o Hargreaves Street, Daly Street and Grandstand Road (south) – provide access to the light industrial/commercial units in this area of the site. None of these three street have footpath, which reinforces the fact that access to these land uses are primarily designed to be by private vehicle rather than on foot.

The extents of the existing cycle network are set out in Section 3.3. The extent and quality of the existing cycling infrastructure within and surrounding the Golden Gateway LSP site is of a high standard with local and regional links provided to the site. The local and regional cycling network is shown in Figure 6.

Good on road cycling routes for experienced/confident cyclists have recently been provided adjacent to the Golden Gateway LSP site as part of the Great Eastern Highway upgrades. A number of high quality shared use paths providing connections within the LSP site along Stoneham Street, Raconteur Drive and Grandstand Road are provided.

In addition, high quality shared use paths from the LSP site are provided along the Swan River Foreshore (via Belmont Trust Land towards the Graham Farmer Freeway PSP to access Perth City), and along the shoreline within the Ascot Water development. Also, streets within the Ascot Waters development are identified as local cycle friendly routes.

#### 4.5.2 Proposed Pedestrian/Cycle Facilities

Figure 24 shows the proposed shared path connections through the Golden Gateway development. It should be noted that all existing shared paths surrounding and through the site will be maintained and many of these existing connections enhanced by additional shared path connectivity including:

- Retention of the shared path along the northern/western side of Stoneham Street.
- The Stoneham Street shared path will be enhanced with a formalised connection through to Matheson Road (Matheson Road is currently identified as a good road riding environment) the form of crossing of Resolution Drive will be subject to further detailed design.
- Shared path connections will be provided along Hargreaves Street, Daly Street and Grandstand Road (south). These will provide connectivity between the Great Eastern Highway on-road bike lanes and the shared path network along Stoneham Street and through the public open space corridor running east-west through the Golden Gateway site.
- In addition to the existing pedestrian crossing facilities at the Great Eastern Highway intersections with Stoneham Street and Resolution Drive, it is proposed that a controlled mid-block shared pedestrian/bike crossing is provided. An at-grade signal controlled crossing would be the preferred form of crossing and provide connectivity between the Golden Gateway development and land uses and community facilities located to the south of Great Eastern Highway.
- An at-grade signal controlled crossing of Great Eastern Highway would require further investigation at the detailed design stage, but could take the form of the pedestrian crossing of Great Eastern Highway that is provided to the west of the Golden Gateway site.







Figure 24 – Golden Gateway Structure Plan Pedestrian and Cycle Facilities (source: Taylor Burrell Barnett)

The indicative cross sections shown in Figure 25, Figure 26 and Figure 27 show the internal road network within the Golden Gateway development and enhanced pedestrian and cycling environment, including opportunities to safely cross internal streets.





Figure 25 – Indicative Hargreaves Street Cross Section (source: EPCAD)



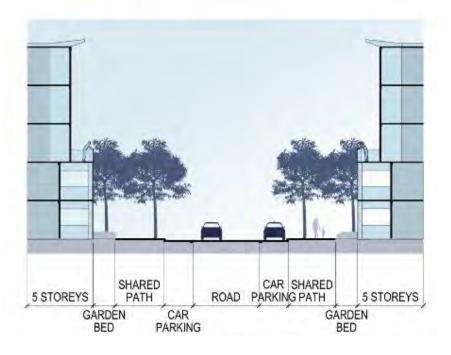






Figure 26 – Indicative Daly Street (Main Street) Cross Section (source: EPCAD)



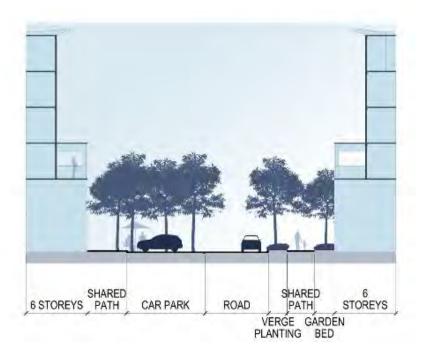
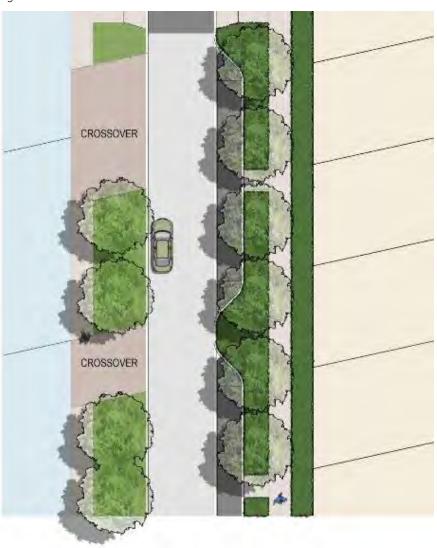
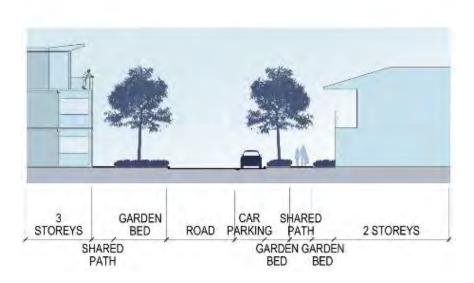






Figure 27 – Indicative Cross Section for Residential Area to the North East of Resolution Drive (source: EPCAD)









## 4.6 Public Transport Routes

### 4.6.1 Existing Public Transport

Access to existing public transport facilities from the Golden Gateway site is considered to be 'average' at present, however there are options to make improvements to public transport access if land uses within the Golden Gateway site change over time to support additional public transport service provision.

The Circle Route buses operate through the LSP site along Grandstand Road and Resolution Drive. However, no bus stops for these services currently exist within the LSP site, the closest bus stops are located on Grandstand Road immediately to the north of the LSP area (close to the main pedestrian entry/exit to Ascot Racecourse).

The Circle Route services provide a high frequency orbital public transport connection around Perth, linking inner suburbs, major activity centres, key land uses and public transport hubs including; Belmont Forum, Oats Street Station, Curtin University, Murdoch Activity Centre, Fremantle, Cottesloe, Claremont, UWA, QEII Medical Centre, Stirling Station and Morley Galleria.

In addition, bus routes 36, 40, 295, 296 and 299 operate along Great Eastern Highway along the southern boundary of the site. All five of the bus routes serve Elizabeth Quay Bus Station, St Georges/Adelaide Terrace, Victoria Park Transfer Station and Great Eastern Highway adjacent to the Golden Gateway LSP site.

#### 4.6.2 Possible Future Public Transport

In order to facilitate higher density development in the Golden Gateway precinct, a step change in public transport provision and public transport use will be required to ensure residents, employees and visitors have the potential to travel to/from Golden Gateway by a sustainable form of transport and take up that opportunity.

High level discussions with the Public Transport Authority (PTA) Transperth Service Development Team has informed the information provided below.

It is currently anticipated by the PTA that the introduction of the Forrestfield Airport Link (FAL) rail connection from central Perth to Perth Airport and onto a park 'n' ride station at Forrestfield, will see the removal of four of the five existing bus routes operating along the Great Eastern Highway corridor (bus routes 36, 295, 296 and 299) and a renumbering and change of route for the remaining bus route (bus route 40).

Subject to consultation it is currently anticipated that the five existing bus routes will be rerouted as follows:

- Bus Route 36 to be renumbered as Bus Route 303 and operate from Midland Station to the new Redcliffe Station:
- Bus Routes 295/296/299 to feed into Forrestfield Station from Kalamunda and its surrounds; and
- Bus Route 40 to be renumbered Bus Route 940 Superbus (details below).

It is currently anticipated that the 940 Superbus would initially operate as a first stage from Redcliffe Station to Elizabeth Quay Station via Great Eastern Highway (past the Golden Gateway development) and Victoria Park Transfer Station and Adelaide Terrace/St Georges Terrace.

It is anticipated that the first stage of the 940 Superbus route is funded as part of the FAL project.

In the longer term, it is anticipated that the Superbus would become a through routed service to Subiaco Station from Elizabeth Quay Bus Station via West Perth. This would be subject to funding, as well as the longer term infrastructure requirements from the PTA being in place, including bus lanes along Adelaide Terrace/St Georges Terrace and bus layover capacity at Subiaco Station.





The Superbus route would operate as a high frequency service and as such it is considered unlikely that the PTA would re-route this service through the Golden Gateway site and instead the service would operate along the Great Eastern Highway corridor between Redcliffe Station and Victoria Park Transfer Station.

The PTA has indicated that, if sufficient public transport demand was generated by large scale development of the Golden Gateway site, they would consider the option of operating a bus service which connected the Golden Gateway site and central Perth with a bus service that terminated/turned back from the Golden Gateway site – utilising the internal road network within the Golden Gateway site. However this would be contingent upon the Golden Gateway site generating the requisite public transport demand to warrant the investment in such a service.





# 5. EXTERNAL TRANSPORT NETWORKS

# 5.1 Changes to External Networks

Aside from the intersection controls proposed in section 4.4, there are no planned alterations to the external transport network.

The existing cycling, pedestrian and public transport networks are all within proximity of the site and there are no known major changes proposed that would impact the site. Where future improvements are being considered adjacent to the Structure Plan area, the City of Belmont should prioritise safe pedestrian and cyclist movements to tie-in with the proposed Golden Gateway networks.





### 6. INTEGRATION WITH SURROUNDING AREA

## 6.1 Major Attractors and Generators

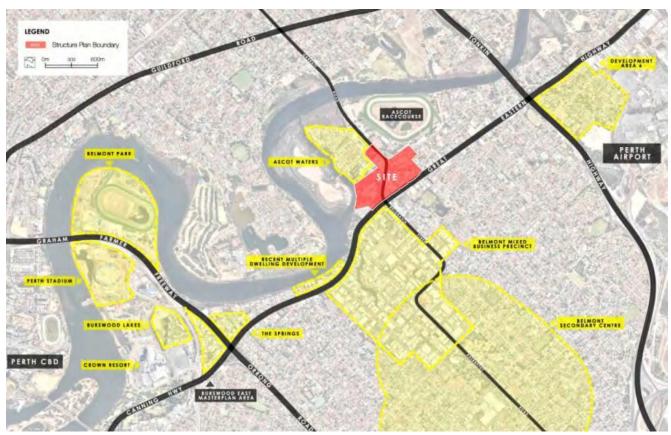
The Golden Gateway site is located at the axis of the key movement corridors of Great Eastern Highway, Stoneham Street, Grandstand Road and Resolution Drive and includes key strategic sites such as the Belmont Trust Land, Ascot Kilns and Western Australian Turf Club headquarters and associated land.

Figure 28 shows the location of the Golden Gateway site within a district context. The land is located approximately 5km south-east of the Perth central business district, 3km north of Belmont Forum Shopping Centre and mixed business area, and 5km northeast of Victoria Park entertainment precinct. Within its immediate context, the subject land is located adjacent the Swan River and Ascot Racecourse.

It is also well connected to regional movement networks such as the Graham Farmer Freeway, Orrong Road, Canning Highway and Tonkin Highway. The Garratt Road Bridge also provides a key connection to the north across the Swan River.

Within the local context, the subject land can be regarded as lacking in basic convenience shopping facilities. The BP Service Station located on the corner of Great Eastern Highway and Resolution Drive and deli located at Epsom Avenue approximately 2km south of the subject land provide local conveniences. However, the nearest supermarket (IGA) is located approximately 2.5km to the west of the subject land at Eastgate Commercial Centre, Kooyong Road (neighbourhood centre) or Belvidere Street (neighbourhood centre) approximately 2.5km to the south. Additional services are located approximately 3km to the north-west of the subject land at Maylands Shopping Centre (neighbourhood centre) or 3km to the south at Belmont Forum (secondary centre).

Figure 28 – Golden Gateway Site – District Context Plan (source: Taylor Burrell Barnett)







The City of Belmont is located in the inner middle ring of localities around Central Perth. It is bordered by Perth Airport, the Swan River, City of Canning to the south and Town of Victoria Park to the west. It is a major employment zone and has a residential population above 40,000.

The population of the City of Belmont was recorded as 30,333 in 2006, 35,209 in 2011 and 41,344 (estimate) in 2015. The population of the City of Belmont is getting younger, reflecting a move towards smaller household sizes, more medium and higher density development and attraction of the area to younger people working and choosing to live closer to central Perth.

The age profile of the City of Belmont point to this trend with key indicators being:

- The highest age grouping population increases were experienced in the 20-34 age bracket;
- There was also a significant increase in the number of babies in the City of Belmont which would also correlate with the number of young families choosing to move into the City of Belmont between the 2006 and 2011 Census periods; and
- There is a higher proportion of these age groups in the City of Belmont than Greater Perth indicating that the area is becoming more attractive for young families and professionals.

The majority of employed people in the City of Belmont travel to jobs outside of the Council boundaries. The 2011 Census statistics show that 22% of people recorded live and work in the City of Belmont.

Although the majority of employed people within the City of Belmont in 2011 travelled outside of the Council boundaries to reach jobs, the dispersal of people was relatively widespread. Jobs within the City of Belmont was the highest recorded number with the Perth Inner and Outer central business district locations being the next highest.

Employment in adjoining Town of Victoria Park and City of Canning were high, potentially reflecting the areas of Welshpool and Kewdale located within these authorities and significant amount of higher order commercial and retail centres.

At the 2011 Census, there were nearly 47,000 jobs located within the City of Belmont and this number has continued to rise with the expansion of development in the Kewdale/Welshpool area, increased development around Perth Domestic Airport and job growth in service, retail and commercial sectors. Within Belmont, there are more employees than residents with the majority of those employees coming from outside of the City to access jobs – less than 4,000 local residents lived and worked in the City of Belmont in 2011.

# 6.2 Strategic Deficiencies

As set out in this report, the Golden Gateway site has excellent access to the existing local and regional transport networks. Critically for this site, the future networks surrounding the site should cater for local trips via better cycling and pedestrian access so as to reduce the potential for vehicle trips to be generated by the development.

This could be accommodated by:

- A high quality pedestrian connection to cross Great Eastern Highway at a mid-point between the Stoneham Street and Resolution Drive intersections, in order to access Perth bound public transport services, as well as retail, commercial and recreational land uses to the south of Great Eastern Highway;
- Ensuring the interface between existing cycling infrastructure surrounding the site and new cycling infrastructure provided within the Golden Gateway development is well planned and designed to provide a seamless and safe transition between the two; and





If sufficient public transport demand was generated by large scale development of the Golden Gateway site, which facilitated a bus service which connected the Golden Gateway site and central Perth with a bus service that terminated/turned back from the Golden Gateway site — the transition of buses into and out from the site as well as route through the site should be designed to ensure the highest number of residents and employees within the Golden Gateway site are within a 200m-400m walk distance of a bus stop to make the public transport services accessible and attractive to users.





## 7. ANALYSIS OF INTERNAL TRANSPORT NETWORKS

#### 7.1 Form of Assessment

In order to ensure that there is relevant information assessed within this TIA, the assessment has been as comprehensive as possible in the use of data collected and observations and details relevant to the Golden Gateway site.

The traffic assessment has been undertaken using focused SIDRA assessment of the key intersections to understand the immediate and future impacts. The following intersections have been assessed:

- Stoneham Street, Great Eastern Highway and Belgravia Street traffic signal controlled intersection (existing year and future year operation);
- Resolution Drive, Great Eastern Highway and Hardey Road Street traffic signal controlled intersection (existing year and future year operation);
- Grandstand Road, Resolution Drive and Stoneham Street roundabout (existing year and future year operation); and
- Stoneham Street, Resolution Drive and Daly Street traffic signal controlled intersection (future year operation).

# 7.2 Assessment Years and Background Traffic

The assessment years for the Golden Gateway development have been for the base year (2016) and for the forecast year (2031), which represents a notional date by which full site buildout is delivered.

In order to determine forecast year traffic and understand the potential background growth, Main Roads WA provided strategic model outputs for the use in this study, to inform the Project Teams understanding of forecast traffic growth on the road network surrounding the Golden Gateway site.

Main Roads WA strategic Regional Operations Model (ROM) encompasses metropolitan Perth and surrounds. Main Roads WA's current strategic model is called ROM24, which provides 24 hour time of day traffic assignment.

In order to assist with forecast traffic growth analysis for this study, Main Roads WA provided link volume plots (showing traffic volumes on each road) from the following ROM24 models:

- 2016 all day model / AM peak (7am-9am) model / PM peak (4pm-6pm) model; and
- 2031 all day model / AM peak (7am-9am) model / PM peak (4pm-6pm) model.

It has been found through previous experience working with ROM24 model outputs, that 2016 ROM24 traffic volumes are typically higher than recent observed traffic volumes (note: the source of data referred to in this report as 'recent observed traffic volumes' is the Main Roads WA Traffic Map:

https://trafficmap.mainroads.wa.gov.au/). Figure 29 shows the data collection location for the recent observed traffic volumes data.





Stoneham St

Stoneham St

Belgravia St John Ambulance

Belgravia St John Ambulance

Belgravia St John Ambulance

Figure 29 – Data Collection Locations for Recent Observed Traffic Volumes (map source: Google Maps)

The difference between 2016 ROM24 traffic volumes and recent observed traffic volumes varies from location to location across the geographical area of the ROM24 model, but it is typical that 2016 ROM24 traffic volumes are between 5%-25% higher than recent observed traffic volumes.

Table 12 shows the difference between the 2016 ROM24 traffic volumes and recent observed traffic volumes. The data shows that whilst volumes on Great Eastern Highway (GEH) are similar (2% difference), those on the surrounding roads vary considerably (20%-35% difference) and the flow on Grandstand Road has a 63% difference.

Table 12 – Recent Observed Traffic Volumes and 2016 ROM24 Traffic Volumes (All Day Two-Way Traffic)

Road	Section	Recent Observed	2016 ROM24	Difference (%)
Grandstand Rd	Waterway Cres-Raconteur Dr	18,400 (2014)	30,000	+63%
Resolution Dr	Grandstand Rd-GEH	9,100 (2014)	11,600	+27%
Hardey Rd	GEH-Wallace St	8,800 (2013)	11,100	+26%
Stoneham St	Grandstand Rd-GEH	15,200 (2014)	20,600	+35%
Belgravia St	GEH-Barker St	14,600 (2011)	17,600	+20%
GEH	Belgravia St-Hardey Rd	64,800 (2014)	66,100	+2%

Typically to address the issue of the 2016 ROM24 traffic volumes being higher than the observed traffic volumes, the 2016 ROM24 model data would be scaled back to the observed data to reflect a recast 2016 traffic volume. Then in each case the same level of annual growth between the 2016 ROM24 and 2031 ROM24 data, would be applied to the observed data to take the 2011, 2013 or 2014 observed data up to a recast 2031 traffic volume. The recast traffic volumes generated through this approach are shown in Table 13.





Table 13 – Recast 2016 and 2031 Traffic Volumes (All Day Two-Way Traffic)

Road	Section	Recast 2016	Recast 2031	Total Growth (Annual Growth)
Grandstand Rd	Waterway Cres-Raconteur Dr	19,300	26,500	37% (2.5%)
Resolution Dr	Grandstand Rd-GEH	9,500	12,500	32% (2.1%)
Hardey Rd	GEH-Wallace St	10,000	16,500	65% (4.3%)
Stoneham St	Grandstand Rd-GEH	16,200	23,800	47% (3.1%)
Belgravia St	GEH-Barker St	16,300	22,200	36% (2.4%)
GEH	Belgravia St-Hardey Rd	66,500	79,300	19% (1.3%)

However, whilst the recast ROM24 outputs would suggest that roads around the Golden Gateway site will experience significant growth in traffic volumes, between 1.3%-4.3% per annum between 2016 and 2031, a review of recent traffic growth trends presents no clear picture in relation to historic background traffic growth, as illustrated below:

- Great Eastern Highway count sites adjacent to the Golden Gateway site show no growth or a slight decrease in traffic volumes until 2012/13. After this date, there has been significant growth in traffic volumes, but that would be a direct result of the widening of Great Eastern Highway after 2012/13;
- Stoneham Street a count site adjacent to the Golden Gateway site shows a 20% decrease in traffic volumes between 2010/11 and 2013/14;
- Resolution Drive a count site adjacent to the Golden Gateway site shows a 49% increase in traffic volumes between 2010/11 and 2013/14; and
- Grandstand Road (at the Garratt Road Bridge) a count site adjacent at the Garratt Road Bridge shows a 6% increase in traffic volumes between 2012/13 and 2013/14.

The available historic traffic count data on the road network surrounding the Golden Gateway site suggests that the road network has been subject to significant fluctuations in traffic volumes as works have been completed in the local area – most notably the Great Eastern Highway upgrades.

Based on the fluctuations in historic traffic count data and therefore the limited reliability of this data to accurately reflect recent historic trends from which to base background traffic growth in the future, and the recast ROM24 outputs suggesting that there is to be significant year on year growth around the Golden Gateway site, far in excess of levels of annual growth that would be expected to be sustained and accommodated on the surrounding road network.

As such, for the purposes of this structure planning assessment we have applied 15% growth (1% growth per annum) for the Great Eastern Highway corridor and 25% growth (1.7% growth per annum) for all other road corridors around the Golden Gateway site.

# 7.3 Modelled Transport Networks and Land Use Data

The ROM24 model outputs provided by Main Roads for use in this study are from the 2016 Base Model and 2031 Base Model. Both models have consistent road network capacity within the vicinity of the Golden Gateway site. The capacity of the key links in relation to this study are:

Grandstand Road = 2 lanes in each direction;

Resolution Drive = 1 lane in each direction;

▶ Hardey Road = 1 lane in each direction;
 ▶ Stoneham Street = 2 lanes in each direction;

Belgravia Street = 2 lanes in each direction; and

☐ Great Eastern Highway = 3 lanes in each direction.





Whilst the road network capacity within the vicinity of the Golden Gateway site is clearly understood and consistent across the 2016 ROM24 Base Model and 2031 ROM24 Base Model, the land use data within each of the models has not been made available to the Project Team.

Main Road WA's Traffic Modelling Team were contacted with a request to provide information relating to any significant land use change between the 2016 and 2031 ROM24 models in relation to the following most significant sites/locations:

- The Springs development;
- Golden Gateway structure plan area (including the Kilns site);
- DA6/Redcliffe Station area;
- Domestic Terminal area;
- Belmont Forum Shopping Centre area;
- Belmont Mixed Business Precinct; and
- Any other location of significance in the area that is likely to impact upon traffic volumes on the road network surrounding the Golden Gateway site.

The Project Team's land use data enquiry was passed onto the DoP by Main Roads WA. The following broad advice was provided by the DoP:

- "The Metropolitan Land Use Forecasting System (MLUFS) is prepared by the Department and acts as an input into the Main Roads transport model.
- MLUFS is a small area forecasting model used to estimate the number and distribution of dwellings, population, employment and workforce within the Perth and Peel metropolitan area.
- For dwelling and population estimates, ABS data and WA Tomorrow population forecasts are used to determine underlying demand for dwellings across the Perth and Peel region. The distribution and development staging of these dwellings (and associated population growth) is then forecast based on growth trends, the capacity of sites available for development and other indicator data.
- The MLUFS process draws on data from a number of sources including: WA Tomorrow population forecasts; Australian Bureau of Statistics; Stock of undeveloped Region scheme and local planning scheme land; Development applications and subdivision activity; Developers' intentions; Availability of vacant lots; Capacity within developed areas for intensification of development; Proposed developments identified through other means; and Economic drivers.
- All zoning and associated amendments up to the period at which the forecast is produced are included as model inputs. Interventions that are identified after the preparation of the forecasts (such as Redcliffe Station) aren't explicitly reflected in the numbers.
- Preliminary outputs for MLUFS and WA Tomorrow are cross-referenced with Urban Land Development Outlook analysis to ensure alignment between outputs. The Urban Land Development Outlook identifies land in the Perth and Peel metropolitan area expected to be developed over the next 20 plus years.
- Forecasts are modelled based on the best available information at the time and it is expected that they will fall within an acceptable margin of error of actual growth."

As such the information provided by DoP does not provide any clarity as to any specific land use changes between 2016 and 2031 within the vicinity of the Golden Gateway site.

#### 7.4 Time Periods for Assessment

The time period for the modelling assessment was the AM peak hour (0800-0859) and PM peak hour (1600-1659) during an average weekday. As set out in Section 3.6, this is the period of time where there is most traffic on the surrounding road network and interaction between vehicles, cyclists and pedestrians.





# 7.5 Trip Generation Assumptions

The Golden Gateway LSP is comprised of three main land uses, residential dwellings, commercial space and retail space. It is proposed that the three land uses will primarily be provided in mixed-use development sites across the Golden Gateway LSP area. The split of the three land uses is shown in Table 14.

Table 14 - Proposed Structure Plan Land Uses

Item	Data
Estimated Number of Dwellings	3,000 dwellings
Estimated Dwelling Type - multiple dwelling (75 m² apartment size) - single dwelling	2,950 dwellings 50 dwellings
Commercial space	7,400 m <sup>2</sup> GFA (5,920 m <sup>2</sup> NLA)
Retail space	1,500 m <sup>2</sup> GFA (1,200 m <sup>2</sup> NLA)

It should be noted that this assessment also includes the traffic generated by the 200 dwellings on the Ascot Kilns site, which are subject to assessment as part of the Ascot Kilns Local Development Plan (LDP).

### 7.5.1 Residential Trip Generation

The assessment is based upon use of the New South Wales Roads and Traffic Authority (RTA) Guide to Traffic Generating Development (October 2002).

The RTA guide provides the following trip rates for 'medium density' residential development with average public transport accessibility (which is deemed to be representative of the future Golden Gateway development):

Smaller units and flats (up to two bedrooms):

Daily vehicle trips

3.50 per dwelling

Weekday peak hour vehicle trips

0.35 per dwelling

Larger units and town houses (three or more bedrooms):

Daily vehicle trips
 Weekday peak hour vehicle trips
 4.00 per dwelling
 0.40 per dwelling

The traffic distribution was based on an industry accepted split between inbound and outbound vehicle trips as follows:

Daily traffic distribution:

o Inbound 50%

o Outbound 50%

AM peak hour traffic distribution:

o Inbound 10%

o Outbound 90%

PM peak hour traffic distribution:

o Inbound 80% o Outbound 20%

In addition to the above is was assumed that the proposed density of development across the Golden Gateway site would result in a step change in public transport service provision across the site/local area. It was assumed that there would be an uplift in public transport service provision as per Section 4.6.2. As a result of this, it was assumed that enhance public transport service provision would lead to increased public





transport use to/from the Golden Gateway site and this would result in a 20% reduction in residential vehicle trips to/from the site.

#### 7.5.2 Commercial Trip Generation

The assessment was based upon a ratio of 30m<sup>2</sup> per employee, therefore 198 employees in 5,920m<sup>2</sup> (NLA) of commercial space.

Existing City of Belmont Town Planning Scheme allows for 1 car parking bay per employee. The assessment was based upon all 198 employees having access to a car parking bay within the Golden Gateway development (either in commercial car parks or in reciprocal access arrangement with residential car parking).

The traffic distribution was based on the following peak hour split between inbound and outbound vehicle trips as follows:

AM peak hour traffic distribution:

o Inbound 80% of daily commercial trips o Outbound 0% of daily commercial trips

PM peak hour traffic distribution:

o Inboundo Outbound0% of daily commercial tripso Outbound80% of daily commercial trips

In addition to the above it was assumed that 10% of employees would live within the Golden Gateway development and as such total commercial vehicle trips were reduced by 10% (20 employees in total) to reflect an internal walk trip rather than an external vehicle trip.

## 7.5.3 Retail Trip Generation

The assessment is based on the assumption that all retail vehicle trips are existing vehicle trips on the network and no new vehicle trips would be generated by the retail land uses. As such it is assumed that retail vehicle trips would be a linked-trips as part of an existing journey through the network. Therefore, no new vehicle trips to the retail land uses were considered as part of the assessment.

# 7.6 Existing Golden Gateway Traffic Generation

Based on the City of Belmont loop counts of existing vehicle activity in to and out from the Golden Triangle site (as documented in Section 3.6.4), the following vehicle trips are generated by the existing land uses:

Daily traffic:

o Inbound 1,800 vehicles o Outbound 1,800 vehicles o TOTAL 3,600 vehicles

AM peak hour traffic:

o Inbound 140 vehicleso Outbound 140 vehicleso TOTAL 280 vehicles

PM peak hour traffic:

o Inbound 140 vehicles o Outbound 140 vehicles o TOTAL 280 vehicles





The assessment is based on the above existing traffic volumes being removed from the network to reflect the removal of the existing land uses on the site prior to redevelopment of the Golden Gateway site.

# 7.7 Future Golden Gateway Traffic Generation

Based on the assumptions set out in Section 7.2, Section 7.3, Section 7.4, Section 7.5 and Section 7.6, the following new vehicle trips would be generated by the future Golden Gateway development:

Daily traffic:

o Inbound 2,600 vehicleso Outbound 2,600 vehicleso TOTAL 5,200 vehicles

AM peak hour traffic:

o Inbound
 o Outbound
 o TOTAL
 97 vehicles
 697 vehicles
 794 vehicles

PM peak hour traffic:

o Inbound 605 vehicles o Outbound 189 vehicles o TOTAL 794 vehicles

The intersection assessment is based on the above future traffic volumes being generated by the Golden Gateway development.

# 7.8 Future Golden Gateway Traffic Distribution

The generation of vehicle trips from the Golden Gateway site is split across five development precincts shown in Figure 30, Figure 31, Figure 32 and Figure 33. The distribution of vehicle trip generation across the five development precincts is based upon the overall split of development per precinct area, and is reflective of ultimate development buildout and may alter based on land release, availability to market and types of development progressed in the future.

The overall distribution of vehicles trips into and out from the Golden Gateway site onto the surrounding road network was based on a combination of MRWA traffic count data, MRWA SCATS data and City of Belmont traffic count data.

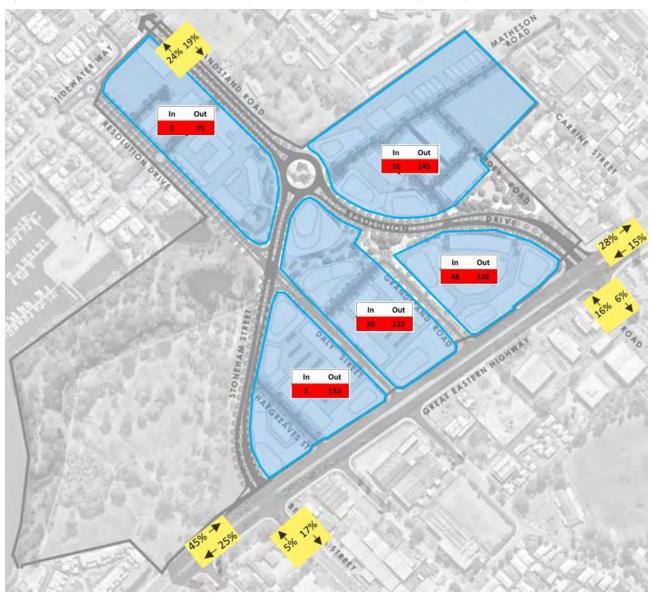
Figure 30 and Figure 31 show the indicative peak hour distribution of vehicle trips into and out from the Golden Gateway site. This reflects an even distribution of outbound trips during the AM peak between Great Eastern Highway east (towards Tonkin Highway), west (towards Perth city) and Grandstand Road north (towards Maylands). With a greater dominance during the PM peak of inbound trips from Great Eastern Highway west (from Perth city).

Figure 32 and Figure 33 show the indicative peak hour turning movements on the surround road network for the Golden Gateway development traffic only.





Figure 30 – Indicative AM Peak Hour Vehicle Trip Distribution for Golden Gateway Development Traffic







In Out

The Out

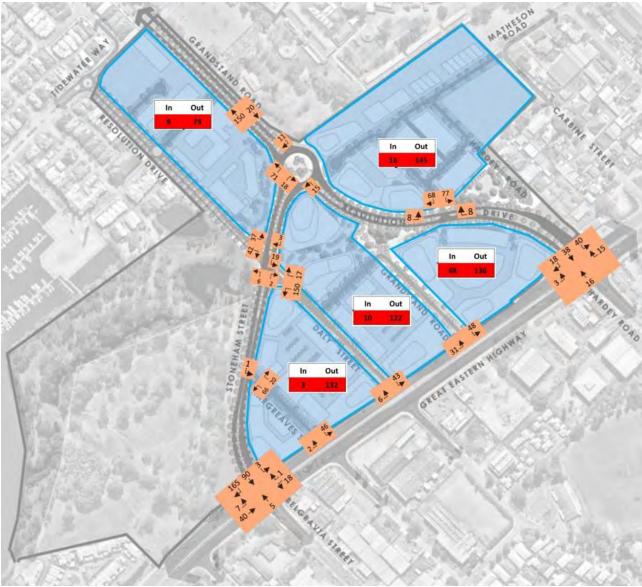
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Figure 31 – Indicative PM Peak Hour Vehicle Trip Distribution for Golden Gateway Development Traffic





Figure 32 – Indicative AM Peak Hour Turning Movements for Golden Gateway Development Traffic







GREAT EASTERN HIGHWAY

Figure 33 – Indicative PM Peak Hour Turning Movements for Golden Gateway Development Traffic

## 7.9 Intersection Assessment

The following intersections have been subject to SIDRA assessment:

- Stoneham Street/Great Eastern Highway intersection 2016, 2031 and 2031 with development;
- Resolution Drive/Great Eastern Highway intersection 2016, 2031 and 2031 with development;
- Grandstand Road/Resolution Drive/Stoneham Street intersection 2031 and 2031 with development; and
- Stoneham Street/Daly Street/Resolution Drive intersection 2031 and 2031 with development.

#### 7.9.1 Stoneham Street-Great Eastern Highway Intersection

Headline SIDRA movement summaries for the Stoneham Street/Great Eastern Highway Intersection are presented in Table 15 (AM Peak hour) and Table 16 (PM peak hour). The SIDRA intersection geometry is shown in Figure 34. The full SIDRA movement summaries are presented in Figure 35, Figure 36 and Figure 37 (AM peak hour) as well as Figure 38, Figure 39 and Figure 40 (PM peak hour).





Overall the Stoneham Street/Great Eastern Highway Intersection SIDRA modelling results show that by 2031 under the base scenario that background growth would see the intersection operating during the AM peak with all approaches over capacity (other than Belgravia Street approach) and during the PM peak with all approaches with a degree of saturation greater than 0.95 (other than Great Eastern Highway (east)).

When Golden Gateway related development traffic is included in the SIDRA modelling results for the 2031 with development scenario, there is an increase in the degree of saturation on the Stoneham Street and Great Eastern Highway approaches leading to increases in average delay and LOS.

The AM peak hour modelling results show that the existing base year (2016) Stoneham Street/Great Eastern Highway Intersection performs with an overall intersection LOS D, with the Stoneham Street approach performing with a LOS F with an average delay of 87 seconds and the Belgravia Street approach performs with a LOS E with an average delay of 75 seconds.

The AM peak hour modelling results show the future base year 2031 scenario Stoneham Street/Great Eastern Highway Intersection performs with an overall LOS F, which is the same overall LOS as the 2031 plus Golden Gateway development scenario. However, the additional AM peak hour traffic generated by the Golden Gateway site that is predicted to use the Stoneham Street corridor, would place additional pressure on the Stoneham Street approach to the Great Eastern Highway intersection and there would be a significant increase in delay from 152 seconds (2031 base scenario) to 270 seconds (2031 with development scenario).

The PM peak hour modelling results show that the existing base year (2016) Stoneham Street/Great Eastern Highway Intersection performs with an overall intersection LOS C, with the Stoneham Street approach performing with a LOS E with an average delay of 70 seconds. The Belgravia Street approach performs with a LOS E with an average delay of 61 seconds.

The PM peak hour modelling results show the future base year 2031 scenario Stoneham Street/Great Eastern Highway Intersection performs with an overall LOS E and the 2031 plus Golden Gateway development scenario performs with an overall LOS F. The additional PM peak hour traffic generated by the Golden Gateway site that is predicted to use the Great Eastern Highway (west) and Stoneham Street approaches to the intersection would place additional pressure on the operation of the intersection and would see significant increases in intersection delay – Great Eastern Highway approach would increase in delay from 69 seconds (2031 base scenario) to 157 seconds (2031 with development scenario) and the Stoneham Street approach would increase in delay from 108 seconds (2031 base scenario ) to 182 seconds (2031 with development scenario).





Table 15 – AM Peak Hour (0800-0859) Stoneham Street/Great Eastern Highway Intersection Assessment

		neham (north)	St		GEH (east)			Igravia (south)			GEH (west)		Al	l Vehicl	es
Year	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS
2016	0.87	87	F	0.85	38	D	0.50	75	Е	0.83	31	C	0.87	45	D
2031	1.03	152	F	1.04	155	F	0.66	82	F	1.02	40	D	1.04	115	F
2031 + dev	1.15	270	F	1.14	303	F	0.65	84	F	1.15	59	E	1.15	211	F

Table 16 – PM Peak Hour (1600-1659) Stoneham Street/Great Eastern Highway Intersection Assessment

	Stoneham St (north)				GEH (east)			lgravia (south)			GEH (west)		All Vehicles		
Year	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS
2016	0.82	70	Е	0.57	24	C	0.81	61	Е	0.82	26	C	0.82	33	С
2031	0.96	108	F	0.64	30	C	0.96	99	F	0.99	69	Е	0.99	64	Е
2031 + dev	1.03	182	F	0.64	31	С	1.05	172	F	1.06	157	F	1.06	126	F

Figure 34 – Stoneham Street/Great Eastern Highway Intersection – SIDRA geometry

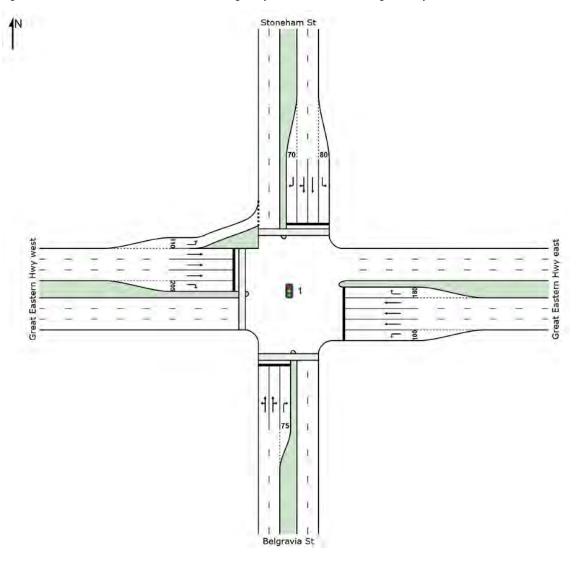






Figure 35 – 2016 AM Peak Stoneham Street/Great Eastern Highway Intersection

Mov	OD	Demand		Deg.	Average	Level of		of Queue	Prop.	Effective	Average
ID	Mov	Total	HV	Satn	Delay	Service	Vehicles	Distance	Queued	Stop Rate	Speed
South	: Belgravi	veh/h	%	v/c	sec		veh	m		per veh	km/l
1	L2	111	3.0	0.438	76.4	LOS E	8.3	59.5	0.96	0.79	26.3
2	T1	105	3.0	0.394	70.4	LOS E	7.8	55.7	0.96	0.76	28.
3	R2	126	3.0	0.497	77.2	LOS E	9.5	68.2	0.97	0.80	26.
		342	3.0	0.497	74.8	LOSE	9.5	68.2	0.96	0.78	26.
Appro	acn	342	3.0	0.497	74.0	LUSE	9.5	00.2	0.96	0.78	20.
East:	Great Eas	stern Hwy ea	st								
4	L2	191	3.0	0.211	30.1	LOS C	8.4	60.5	0.60	0.74	39.
5	T1	2313	3.0	0.851	37.9	LOS D	55.7	400.2	0.92	0.85	37.
6	R2	29	3.0	0.239	86.9	LOS F	2.3	16.4	0.98	0.72	24.
Appro	ach	2533	3.0	0.851	37.9	LOS D	55.7	400.2	0.89	0.84	37.
North	: Stonehai	m St									
7	L2	7	3.0	0.025	69.0	LOS E	0.5	3.4	0.88	0.67	27.
8	T1	248	3.0	0.856	82.0	LOS F	21.2	152.0	1.00	0.97	25.
9	R2	457	3.0	0.869	89.6	LOS F	19.8	142.1	1.00	0.95	24.
Appro	ach	712	3.0	0.869	86.8	LOS F	21.2	152.0	1.00	0.95	24.
West:	Great Ea	stern Hwy we	est								
10	L2	165	3.0	0.107	6.7	LOS A	1.4	10.1	0.16	0.59	53.
11	T1	1308	3.0	0.459	28.5	LOS C	22.4	160.5	0.70	0.62	41.
12	R2	101	3.0	0.833	96.6	LOS F	8.8	63.1	1.00	0.91	23.
Appro	ach	1574	3.0	0.833	30.5	LOS C	22.4	160.5	0.66	0.64	40.
All Ve	hicles	5161	3.0	0.869	44.8	LOS D	55.7	400.2	0.84	0.79	34.

Figure 36 – 2031 AM Peak Stoneham Street/Great Eastern Highway Intersection

Move	ement Pe	erformance	- Vehic	les				_		_	
Mov	OD	Demand	Flows	Deg.	Average	Level of	95% Back	of Queue	Prop.	Effective	Average
ID	Mov	Total	HV	Satn	Delay	Service	Vehicles	Distance	Queued	Stop Rate	Speed
		veh/h	%	v/c	sec		veh	m		per veh	km/h
South	n: Belgravi										
1	L2	139	3.0	0.582	83.5	LOS F	11.3	81.2	0.99	0.81	25.0
2	T1	131	3.0	0.521	77.1	LOS E	10.5	75.7	0.98	0.79	26.7
3	R2	157	3.0	0.657	84.6	LOS F	12.9	92.9	1.00	0.82	25.1
Appro	oach	427	3.0	0.657	81.9	LOS F	12.9	92.9	0.99	0.81	25.5
East:	Great Eas	stern Hwy eas	st								
4	L2	219	3.0	0.254	34.4	LOS C	10.9	78.3	0.64	0.75	37.6
5	T1	2660	3.0	1.036	165.6	LOS F	136.5	979.8	1.00	1.46	16.1
6	R2	33	3.0	0.289	92.8	LOS F	2.8	20.0	0.99	0.73	23.7
Appro	oach	2912	3.0	1.036	154.9	LOS F	136.5	979.8	0.97	1.40	16.9
North	: Stoneha	m St									
7	L2	9	3.0	0.025	65.9	LOS E	0.6	4.4	0.84	0.67	28.4
8	T1	310	3.0	0.840	79.1	LOS E	27.0	194.0	1.00	0.95	26.3
9	R2	571	3.0	1.031	192.7	LOS F	40.7	292.6	1.00	1.27	14.3
Appro	ach	890	3.0	1.031	151.8	LOS F	40.7	292.6	1.00	1.15	17.1
West	Great Ea	stern Hwy we	est								
10	L2	189	3.0	0.124	7.1	LOS A	2.0	14.6	0.18	0.60	53.0
11	T1	1505	3.0	0.560	34.5	LOS C	30.4	218.2	0.77	0.69	38.4
12	R2	116	3.0	1.015	168.8	LOS F	14.5	104.2	1.00	1.22	15.8
Appro	oach	1810	3.0	1.015	40.3	LOS D	30.4	218.2	0.72	0.71	36.1
All Ve	hicles	6039	3.0	1.036	114.9	LOS F	136.5	979.8	0.90	1.12	20.7





Figure 37 – 2031 AM Peak with Golden Gateway development Stoneham Street/Great Eastern Highway Intersection

Move	ement Pe	erformance	- Vehic	es		_					
Mov	OD	Demand	Flows	Deg.	Average	Level of	95% Back	of Queue	Prop.	Effective	Average
ID	Mov	Total	HV	Satn	Delay	Service	Vehicles	Distance	Queued	Stop Rate	Speed
		veh/h	%	v/c	sec		veh	m		per veh	km/h
South	: Belgravi										
1	L2	139	3.0	0.573	85.3	LOS F	11.6	83.2	0.99	0.81	24.7
2	T1	136	3.0	0.533	79.0	LOS E	11.2	80.7	0.98	0.79	26.3
3	R2	157	3.0	0.648	86.3	LOS F	13.2	95.1	1.00	0.82	24.8
Appro	ach	432	3.0	0.648	83.7	LOS F	13.2	95.1	0.99	0.81	25.2
East:	Great Eas	stern Hwy ea	st								
4	L2	237	3.0	0.301	40.5	LOS D	13.3	95.2	0.70	0.77	35.4
5	T1	2660	3.0	1.136	329.1	LOS F	194.8	1398.9	1.00	2.03	9.3
6	R2	34	3.0	0.337	97.2	LOS F	3.0	21.5	1.00	0.73	23.1
Appro	ach	2931	3.0	1.136	303.1	LOS F	194.8	1398.9	0.98	1.92	9.9
North	: Stoneha	ım St									
7	L2	12	3.0	0.027	60.0	LOS E	0.8	5.6	0.79	0.68	29.8
8	T1	400	3.0	0.867	77.7	LOS E	36.1	259.0	1.00	0.97	26.6
9	R2	736	3.0	1.151	377.6	LOS F	79.3	569.1	1.00	1.63	8.2
Appro	ach	1148	3.0	1.151	269.8	LOS F	79.3	569.1	1.00	1.39	10.9
West:	Great Ea	astern Hwy we	est								
10	L2	196	3.0	0.128	7.2	LOSA	2.2	15.8	0.18	0.60	52.9
11	T1	1545	3.0	0.638	41.7	LOS D	35.6	255.7	0.84	0.75	35.7
12	R2	116	3.0	1.148	372.5	LOS F	23.9	171.4	1.00	1.57	8.2
Appro	ach	1857	3.0	1.148	58.8	LOSE	35.6	255.7	0.78	0.79	30.4
All Ve	hicles	6368	3.0	1.151	210.9	LOS F	194.8	1398.9	0.92	1.42	13.3

Figure 38 – 2016 PM Peak Stoneham Street/Great Eastern Highway Intersection

Move	ement Pe	erformance	- Vehic	les							
Mov	OD	Demand		Deg.	Average	Level of	95% Back		Prop.	Effective	Average
ID	Mov	Total veh/h	HV %	Satn v/c	Delay sec	Service	Vehicles veh	Distance m	Queued	Stop Rate per veh	Speed km/h
South	: Belgravi		70	V/C	360		Veri	'''		per veri	KIII/II
1	L2	64	3.0	0.809	65.7	LOS E	15.5	111.6	1.00	0.94	29.6
2	T1	428	3.0	0.809	60.0	LOS E	15.7	113.0	1.00	0.94	30.3
3	R2	217	3.0	0.746	62.8	LOS E	13.3	95.4	1.00	0.87	29.5
Appro	ach	709	3.0	0.809	61.4	LOS E	15.7	113.0	1.00	0.92	29.9
East:	Great Eas	stern Hwy eas	st								
4	L2	77	3.0	0.084	22.5	LOS C	2.4	17.1	0.55	0.70	42.9
5	T1	1636	3.0	0.566	22.8	LOS C	22.7	163.3	0.75	0.67	43.7
6	R2	35	3.0	0.401	72.3	LOS E	2.2	16.0	1.00	0.73	27.4
Appro	ach	1748	3.0	0.566	23.8	LOS C	22.7	163.3	0.74	0.67	43.2
North	: Stoneha	m St									
7	L2	9	3.0	0.052	61.8	LOS E	0.5	3.6	0.93	0.67	29.4
8	T1	170	3.0	0.822	66.9	LOS E	9.9	71.3	1.00	0.94	28.7
9	R2	269	3.0	0.822	72.8	LOS E	9.5	68.5	1.00	0.93	27.4
Appro	ach	448	3.0	0.822	70.4	LOS E	9.9	71.3	1.00	0.93	27.9
West:	Great Ea	stern Hwy we	est								
10	L2	537	3.0	0.388	9.2	LOSA	8.7	62.8	0.36	0.67	51.4
11	T1	2227	3.0	0.818	28.2	LOS C	40.6	291.8	0.88	0.82	41.1
12	R2	70	3.0	0.802	77.4	LOS E	4.7	33.9	1.00	0.89	26.3
Appro	ach	2834	3.0	0.818	25.8	LOS C	40.6	291.8	0.79	0.79	42.1
All Ve	hicles	5739	3.0	0.822	33.1	LOS C	40.6	291.8	0.82	0.78	38.9





Figure 39 – 2031 PM Peak Stoneham Street/Great Eastern Highway Intersection

Move	ement Pe	erformance	- Vehic	les							
Mov	OD	Demand		Deg.	Average	Level of	95% Back		Prop.	Effective	Average
ID	Mov	Total	HV	Satn	Delay	Service	Vehicles	Distance	Queued	Stop Rate	Speed
Courth	n: Belgravi	veh/h	%	v/c	sec		veh	m		per veh	km/h
	•		0.0	0.000	400.0	1005	00.4	000.0	4.00	4.04	00.0
1	L2	80	3.0	0.960	108.9	LOS F	32.1	230.2	1.00	1.21	22.0
2	T1	536	3.0	0.960	103.6	LOS F	32.1	230.2	1.00	1.21	22.3
3	R2	271	3.0	0.895	86.7	LOS F	22.8	163.7	1.00	0.98	24.7
Appro	oach	887	3.0	0.960	98.9	LOS F	32.1	230.2	1.00	1.14	23.0
East:	Great Eas	stern Hwy ea	st								
4	L2	88	3.0	0.094	25.5	LOS C	3.3	23.6	0.54	0.70	41.4
5	T1	1881	3.0	0.643	28.5	LOS C	34.1	245.0	0.78	0.71	41.0
6	R2	40	3.0	0.487	88.4	LOS F	3.1	22.6	1.00	0.73	24.4
Appro	oach	2009	3.0	0.643	29.5	LOS C	34.1	245.0	0.77	0.71	40.5
North	: Stoneha	m St									
7	L2	12	3.0	0.064	73.7	LOS E	0.8	5.9	0.93	0.69	26.8
8	T1	213	3.0	0.956	105.2	LOS F	17.9	128.5	1.00	1.17	22.1
9	R2	336	3.0	0.956	111.6	LOS F	17.2	123.5	1.00	1.12	21.3
Appro	oach	561	3.0	0.956	108.3	LOS F	17.9	128.5	1.00	1.13	21.7
West	: Great Ea	stern Hwy we	est								
10	L2	617	3.0	0.451	11.0	LOS B	14.5	104.1	0.40	0.68	50.1
11	T1	2561	3.0	0.971	80.7	LOS F	95.5	685.9	0.95	1.13	25.9
12	R2	81	3.0	0.986	128.9	LOS F	8.2	58.6	1.00	1.15	19.2
Appro	oach	3259	3.0	0.986	68.7	LOS E	95.5	685.9	0.85	1.05	28.3
All Ve	hicles	6716	3.0	0.986	64.3	LOSE	95.5	685.9	0.86	0.96	29.3

Figure 40 – 2031 PM Peak with Golden Gateway development Stoneham Street/Great Eastern Highway Intersection

Move	ement Pe	rformance	- Vehic	les							
Mov ID	OD Mov	Demand Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back Vehicles veh	of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South	: Belgravi		70	V/C	366		Ven			per veri	KIII/II
1	L2	80	3.0	1.045	200.6	LOS F	51.0	366.2	1.00	1.56	14.1
2	T1	582	3.0	1.045	196.8	LOS F	51.0	366.2	1.00	1.56	14.1
3	R2	271	3.0	0.950	109.8	LOS F	27.0	193.9	1.00	1.07	21.4
Appro	ach	933	3.0	1.045	171.9	LOS F	51.0	366.2	1.00	1.42	15.7
East:	Great Eas	stern Hwy eas	st								
4	L2	92	3.0	0.097	26.4	LOS C	3.6	26.0	0.53	0.70	41.0
5	T1	1881	3.0	0.639	29.6	LOS C	36.0	258.5	0.77	0.70	40.5
6	R2	43	3.0	0.557	94.6	LOS F	3.6	26.0	1.00	0.75	23.5
Appro	ach	2016	3.0	0.639	30.9	LOS C	36.0	258.5	0.77	0.70	39.9
North	: Stoneha	m St									
7	L2	14	3.0	0.071	76.9	LOS E	1.0	7.3	0.93	0.69	26.2
8	T1	243	3.0	1.034	180.5	LOS F	28.5	204.6	1.00	1.42	15.1
9	R2	384	3.0	1.034	187.0	LOS F	27.4	196.4	1.00	1.33	14.7
Appro	ach	641	3.0	1.034	182.2	LOS F	28.5	204.6	1.00	1.35	15.0
West:	Great Ea	stern Hwy we	est								
10	L2	674	3.0	0.510	13.8	LOS B	20.6	148.0	0.47	0.71	48.3
11	T1	2776	3.0	1.056	190.9	LOS F	160.5	1152.3	1.00	1.62	14.5
12	R2	81	3.0	1.050	207.4	LOS F	11.2	80.1	1.00	1.28	13.4
Appro	ach	3531	3.0	1.056	157.4	LOS F	160.5	1152.3	0.90	1.44	16.7
All Ve	hicles	7121	3.0	1.056	125.7	LOS F	160.5	1152.3	0.88	1.22	19.5





### 7.9.2 Resolution Drive/Great Eastern Highway Intersection

Headline SIDRA movement summaries for the Resolution Drive/Great Eastern Highway Intersection are presented in Table 17 (AM Peak hour) and Table 18 (PM peak hour). The SIDRA intersection geometry is shown in Figure 41. The full SIDRA movement summaries are presented in Figure 42, Figure 43 and Figure 44 (AM peak hour) as well as Figure 45, Figure 46 and Figure 47 (PM peak hour).

Overall the Resolution Drive/Great Eastern Highway Intersection SIDRA modelling results show that by 2031 base scenario that background growth would have marginally reduced the level of service of the intersection compared to the 2016 existing base year scenario. The intersection AM peak hour overall performance would have gone from LOS C with an average delay of 32 seconds in 2016 to a LOS D with an average delay of 40 seconds in 2031 base scenario. The intersection PM peak hour overall performance would have gone from LOS C with an average delay of 34 seconds in 2016 to a LOS D with an average delay of 41 seconds in 2031 base scenario.

The AM peak hour modelling results show that the existing base year (2016) Resolution Drive/Great Eastern Highway Intersection performs with an overall intersection LOS C, with the Resolution Drive approach performing with a LOS D with an average delay of 54 seconds and the Hardey Road approach performing with a LOS E with an average delay of 61 seconds.

The AM peak hour modelling results show the future base year 2031 scenario Resolution Drive/Great Eastern Highway Intersection performs with an overall LOS D, which is the same overall LOS as the 2031 plus Golden Gateway development scenario. The modelling results show that the additional AM peak hour traffic generated by the Golden Gateway site has a limited impact upon the operation of the Resolution Drive/Great Eastern Highway intersection – the average delay experienced on each approach to the intersection is predicted to increase marginally within the inclusion of the development related traffic, with the LOS to remain unchanged.

The PM peak hour modelling results show that the existing base year (2016) Resolution Drive/Great Eastern Highway Intersection performs with an overall intersection LOS C, with the Resolution Drive approach performing with a LOS D and an average delay of 50 seconds and the Hardey Road approach performing with a LOS D with an average delay of 54 seconds.

The PM peak hour modelling results show the future base year 2031 scenario Resolution Drive/Great Eastern Highway Intersection performs with an overall LOS D. The modelling results show that the additional PM peak hour traffic generated by the Golden Gateway site has a marginal impact upon the operation of the Resolution Drive/Great Eastern Highway intersection – the average delay experienced on each approach to the intersection is predicted to increase marginally within the inclusion of the development related traffic, however the Great Eastern Highway (west) is predicted to experience a more significant increase in average delay from 38 seconds (2031 base scenario) and 68 seconds (2031 with development scenario).





Table 17 – AM Peak Hour (0800-0859) Resolution Drive/Great Eastern Highway Intersection Assessment

		olution (north)			GEH (east)			ardey R (south)	d		GEH (west)		All	Vehic	les
Year	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS
2016	0.83	54	D	0.83	30	C	0.36	61	Е	0.41	24	C	0.83	32	C
2031	0.86	66	Е	0.90	38	D	0.52	80	F	0.44	26	C	0.90	40	D
2031 + dev	0.95	75	Е	0.92	46	D	0.55	80	F	0.50	27	C	0.95	46	D

Table 18 – PM Peak Hour (1600-1659) Resolution Drive/Great Eastern Highway Intersection Assessment

		olution (north)			GEH (east)			ardey F (south)			GEH (west)		Al	l Vehic	les
Year	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS
2016	0.71	50	D	0.83	29	C	0.45	54	D	0.83	34	C	0.83	34	С
2031	0.90	75	Е	0.86	32	C	0.66	78	Е	0.87	38	D	0.90	41	D
2031 + dev	0.92	80	E	0.95	43	D	0.69	82	F	0.95	68	E	0.95	60	Е

Figure 41 – Resolution Drive/Great Eastern Highway Intersection – SIDRA geometry

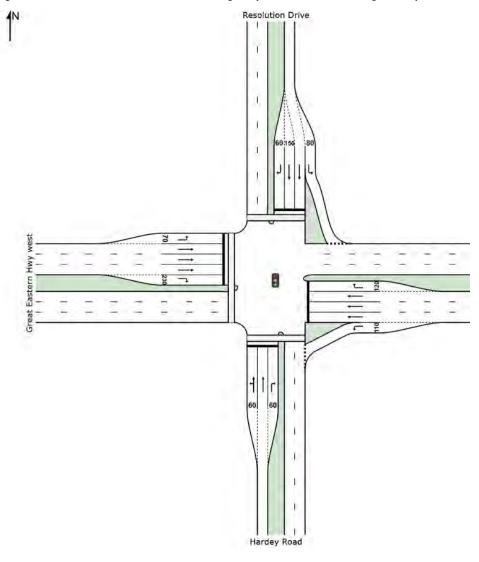






Figure 42 – 2016 AM Peak Resolution Drive/Great Eastern Highway Intersection

Move	Movement Performance - Vehicles  Mov OD Demand Flows Deg. Average Level of 95% Back of Queue Prop. Effective Average													
Mov				Deg.							Average			
ID	Mov	Total	HV	Satn	Delay	Service	Vehicles	Distance	Queued	Stop Rate	Speed			
Courth	: Hardey I	veh/h	%	v/c	sec		veh	m		per veh	km/h			
	•		0.0	0.004	20.0	1005	7.0	50.4	0.00	0.70	20.0			
1	L2	88	3.0	0.364	63.9	LOS E	7.0	50.4	0.93	0.78	29.2			
2	T1	138	3.0	0.364	58.3	LOS E	7.3	52.4	0.93	0.75	30.7			
3	R2	89	3.0	0.296	63.2	LOS E	5.6	40.0	0.92	0.77	29.4			
Appro	ach	315	3.0	0.364	61.3	LOS E	7.3	52.4	0.93	0.76	29.9			
East:	Great Eas	stern Hwy eas	st											
4	L2	106	3.0	0.069	6.7	LOS A	0.9	6.1	0.16	0.59	53.3			
5	T1	2425	3.0	0.827	28.4	LOS C	49.2	353.3	0.87	0.80	41.0			
6	R2	132	3.0	0.810	82.5	LOS F	10.0	71.5	1.00	0.90	25.4			
Appro	ach	2663	3.0	0.827	30.2	LOS C	49.2	353.3	0.85	0.80	40.2			
North	: Resolutio	on Drive												
7	L2	137	3.0	0.183	10.1	LOS B	2.5	18.2	0.33	0.65	50.8			
8	T1	131	3.0	0.828	83.8	LOS F	5.1	36.7	1.00	0.90	25.5			
9	R2	61	3.0	0.811	89.2	LOS F	4.8	34.2	1.00	0.88	24.3			
Appro	ach	329	3.0	0.828	54.1	LOS D	5.1	36.7	0.72	0.79	31.8			
West:	Great Ea	stern Hwy we	est											
10	L2	22	3.0	0.022	21.9	LOS C	0.7	5.1	0.49	0.66	43.2			
11	T1	1271	3.0	0.412	20.9	LOS C	17.4	125.1	0.64	0.57	44.7			
12	R2	66	3.0	0.405	75.0	LOS E	4.6	32.7	0.99	0.76	26.7			
Appro	ach	1359	3.0	0.412	23.6	LOS C	17.4	125.1	0.65	0.58	43.3			
All Ve	hicles	4666	3.0	0.828	32.1	LOS C	49.2	353.3	0.79	0.73	39.3			

Figure 43 – 2031 AM Peak Resolution Drive/Great Eastern Highway Intersection

Move	ement Pe	erformance	- Vehic	les	_	_		_		_	
Mov	OD	Demand		Deg.	Average	Level of	95% Back	of Queue	Prop.	Effective	Average
ID	Mov	Total	HV	Satn	Delay	Service	Vehicles	Distance	Queued	Stop Rate	Speed
		veh/h	%	v/c	sec		veh	m		per veh	km/h
	: Hardey										
1	L2	110	3.0	0.521	82.9	LOS F	11.3	81.2	0.98	0.80	25.3
2	T1	172	3.0	0.521	77.2	LOS E	11.8	84.5	0.98	0.79	26.5
3	R2	112	3.0	0.426	81.7	LOS F	9.0	64.8	0.96	0.79	25.6
Appro	ach	394	3.0	0.521	80.0	LOS F	11.8	84.5	0.97	0.80	25.9
East:	Great Eas	stern Hwy eas	st								
4	L2	122	3.0	0.079	7.1	LOSA	1.3	9.4	0.16	0.59	53.0
5	T1	2789	3.0	0.897	35.9	LOS D	75.3	540.8	0.90	0.85	37.9
6	R2	151	3.0	0.879	104.7	LOS F	14.5	104.3	1.00	0.95	22.0
Appro	ach	3062	3.0	0.897	38.1	LOS D	75.3	540.8	0.88	0.85	37.0
North	: Resolution	on Drive									
7	L2	171	3.0	0.260	13.2	LOS B	4.9	35.4	0.39	0.68	48.7
8	T1	164	3.0	0.857	102.3	LOS F	7.9	56.4	1.00	0.93	22.6
9	R2	76	3.0	0.836	107.0	LOS F	7.2	52.0	1.00	0.91	21.8
Appro	ach	411	3.0	0.857	66.1	LOS E	7.9	56.4	0.75	0.82	28.8
West:	Great Ea	stern Hwy we	est								
10	L2	25	3.0	0.024	22.4	LOS C	0.9	6.5	0.45	0.65	42.9
11	T1	1461	3.0	0.443	22.5	LOS C	23.7	170.4	0.61	0.55	43.9
12	R2	76	3.0	0.443	90.3	LOS F	6.4	46.3	0.99	0.77	24.0
Appro	ach	1562	3.0	0.443	25.8	LOS C	23.7	170.4	0.63	0.56	42.2
All Ve	hicles	5429	3.0	0.897	39.7	LOS D	75.3	540.8	0.80	0.76	36.4





Figure 44 – 2031 AM Peak with Golden Gateway development Resolution Drive/Great Eastern Highway Intersection

Move	ement Pe	rformance	- Vehic	les							
Mov	OD	Demand		Deg.	Average	Level of	95% Back		Prop.	Effective	Average
ID	Mov	Total	HV	Satn	Delay	Service	Vehicles	Distance	Queued	Stop Rate	Speed
South	: Hardey F	veh/h	%	v/c	sec		veh	m		per veh	km/h
1	L2	110	3.0	0.550	83.2	LOS F	12.0	86.3	0.98	0.81	25.3
2	T1	188	3.0	0.550	77.6	LOS F	12.5	89.6	0.98	0.80	26.4
3	R2	112	3.0	0.426	81.7	LOS F	9.0	64.8	0.96	0.79	25.6
Appro	ach	410	3.0	0.550	80.2	LOS F	12.5	89.6	0.97	0.80	25.9
East:	Great Eas	tern Hwy ea	st								
4	L2	122	3.0	0.080	7.3	LOS A	1.4	10.4	0.18	0.59	52.8
5	T1	2789	3.0	0.918	43.8	LOS D	82.6	593.4	0.92	0.91	35.0
6	R2	166	3.0	0.913	110.0	LOS F	16.6	119.0	1.00	1.00	21.4
Appro	ach	3077	3.0	0.918	45.9	LOS D	82.6	593.4	0.90	0.90	34.3
North	: Resolutio	on Drive									
7	L2	211	3.0	0.324	15.8	LOS B	7.5	53.8	0.47	0.71	47.1
8	T1	202	3.0	0.950	117.6	LOS F	10.6	75.9	1.00	1.08	20.7
9	R2	94	3.0	0.931	117.9	LOS F	9.6	68.8	1.00	1.04	20.5
Appro	ach	507	3.0	0.950	75.3	LOS E	10.6	75.9	0.78	0.92	26.9
West:	Great Eas	stern Hwy we	est								
10	L2	28	3.0	0.027	23.4	LOS C	1.0	7.5	0.46	0.66	42.4
11	T1	1598	3.0	0.495	24.5	LOS C	27.6	198.3	0.65	0.59	42.9
12	R2	76	3.0	0.418	89.1	LOS F	6.4	45.9	0.98	0.77	24.2
Appro		1702	3.0	0.495	27.4	LOSC	27.6	198.3	0.66	0.60	41.4
All Ve	hicles	5696	3.0	0.950	45.5	LOS D	82.6	593.4	0.82	0.80	34.4

Figure 45 – 2016 PM Peak Resolution Drive/Great Eastern Highway Intersection

Move	ement Pe	erformance	- Vehic	les							
Mov	OD	Demand		Deg.	Average	Level of	95% Back		Prop.	Effective	Average
ID	Mov	Total	HV	Satn	Delay	Service	Vehicles	Distance	Queued	Stop Rate	Speed
Courth	: Hardey	veh/h	%	v/c	sec		veh	m		per veh	km/h
	L2	53	3.0	0.291	56.0	LOS E	5.3	38.1	0.91	0.75	24.5
1											31.5
2	T1	141	3.0	0.291	50.4	LOS D	5.5	39.2	0.91	0.73	32.7
3	R2	144	3.0	0.448	57.8	LOS E	8.3	59.4	0.94	0.80	30.7
Appro	ach	338	3.0	0.448	54.4	LOS D	8.3	59.4	0.92	0.76	31.7
East:	Great Eas	stern Hwy eas	st								
4	L2	139	3.0	0.095	7.7	LOS A	1.5	10.9	0.23	0.61	52.6
5	T1	1650	3.0	0.593	25.7	LOS C	24.9	178.6	0.78	0.70	42.3
6	R2	163	3.0	0.832	74.8	LOS E	11.2	80.3	1.00	0.93	26.9
Appro	ach	1952	3.0	0.832	28.5	LOS C	24.9	178.6	0.76	0.71	40.9
North	: Resolutio	on Drive									
7	L2	127	3.0	0.197	23.4	LOS C	4.4	31.5	0.62	0.72	42.9
8	T1	125	3.0	0.708	72.1	LOS E	4.3	30.6	1.00	0.82	27.7
9	R2	29	3.0	0.346	74.9	LOS E	1.9	13.7	1.00	0.72	26.9
Appro	ach	281	3.0	0.708	50.4	LOS D	4.4	31.5	0.83	0.77	32.8
West:	Great Ea	stern Hwy we	est								
10	L2	37	3.0	0.042	24.0	LOS C	1.2	8.6	0.55	0.68	42.1
11	T1	2289	3.0	0.831	32.4	LOS C	42.9	307.8	0.93	0.87	39.3
12	R2	143	3.0	0.730	70.1	LOS E	9.3	66.9	1.00	0.85	27.7
Appro	ach	2469	3.0	0.831	34.4	LOS C	42.9	307.8	0.93	0.86	38.4
All Ve	hicles	5040	3.0	0.832	34.4	LOS C	42.9	307.8	0.86	0.79	38.4





Figure 46 – 2031 PM Peak Resolution Drive/Great Eastern Highway Intersection

Move	Movement Performance - Vehicles  Mov OD Demand Flows Deg. Average Level of 95% Back of Queue Prop. Effective Average													
Mov	OD	Demand	Flows	Deg.	Average	Level of	95% Back	of Queue	Prop.	Effective	Average			
ID	Mov	Total	HV	Satn	Delay	Service	Vehicles	Distance	Queued	Stop Rate	Speed			
0 11		veh/h	%	v/c	sec		veh	m		per veh	km/h			
	: Hardey I													
1	L2	110	3.0	0.521	82.9	LOS F	11.3	81.2	0.98	0.80	25.3			
2	T1	172	3.0	0.521	77.2	LOS E	11.8	84.5	0.98	0.79	26.5			
3	R2	112	3.0	0.426	81.7	LOS F	9.0	64.8	0.96	0.79	25.6			
Appro	ach	394	3.0	0.521	80.0	LOS F	11.8	84.5	0.97	0.80	25.9			
East:	Great Eas	stern Hwy ea	st											
4	L2	122	3.0	0.079	7.1	LOS A	1.3	9.4	0.16	0.59	53.0			
5	T1	2789	3.0	0.897	35.9	LOS D	75.3	540.8	0.90	0.85	37.9			
6	R2	151	3.0	0.879	104.7	LOS F	14.5	104.3	1.00	0.95	22.0			
Appro	ach	3062	3.0	0.897	38.1	LOS D	75.3	540.8	0.88	0.85	37.0			
North	: Resolutio	on Drive												
7	L2	171	3.0	0.260	13.2	LOS B	4.9	35.4	0.39	0.68	48.7			
8	T1	164	3.0	0.857	102.3	LOS F	7.9	56.4	1.00	0.93	22.6			
9	R2	76	3.0	0.836	107.0	LOS F	7.2	52.0	1.00	0.91	21.8			
Appro	ach	411	3.0	0.857	66.1	LOS E	7.9	56.4	0.75	0.82	28.8			
West:	Great Ea	stern Hwy we	est											
10	L2	25	3.0	0.024	22.4	LOS C	0.9	6.5	0.45	0.65	42.9			
11	T1	1461	3.0	0.443	22.5	LOS C	23.7	170.4	0.61	0.55	43.9			
12	R2	76	3.0	0.443	90.3	LOS F	6.4	46.3	0.99	0.77	24.0			
Appro	ach	1562	3.0	0.443	25.8	LOS C	23.7	170.4	0.63	0.56	42.2			
All Ve	hicles	5429	3.0	0.897	39.7	LOS D	75.3	540.8	0.80	0.76	36.4			

Figure 47 – 2031 PM Peak with Golden Gateway development Resolution Drive/Great Eastern Highway Intersection

Move	Movement Performance - Vehicles  Mov OD Demand Flows Deg. Average Level of 95% Back of Queue Prop. Effective Average													
Mov														
ID	Mov	Total	HV	Satn	Delay	Service	Vehicles	Distance	Queued	Stop Rate	Speed			
Courth	n: Hardey l	veh/h	%	v/c	sec		veh	m		per veh	km/h			
	•		0.0	0.550	00.0	1005	40.0	00.0	0.00	0.04	05.0			
1	L2	110	3.0	0.550	83.2	LOS F	12.0	86.3	0.98	0.81	25.3			
2	T1	188	3.0	0.550	77.6	LOS E	12.5	89.6	0.98	0.80	26.4			
3	R2	112	3.0	0.426	81.7	LOS F	9.0	64.8	0.96	0.79	25.6			
Appro	oach	410	3.0	0.550	80.2	LOS F	12.5	89.6	0.97	0.80	25.9			
East:	Great Eas	stern Hwy eas	st											
4	L2	122	3.0	0.080	7.3	LOS A	1.4	10.4	0.18	0.59	52.8			
5	T1	2789	3.0	0.918	43.8	LOS D	82.6	593.4	0.92	0.91	35.0			
6	R2	166	3.0	0.913	110.0	LOS F	16.6	119.0	1.00	1.00	21.4			
Appro	oach	3077	3.0	0.918	45.9	LOS D	82.6	593.4	0.90	0.90	34.3			
North	: Resolutio	on Drive												
7	L2	211	3.0	0.324	15.8	LOS B	7.5	53.8	0.47	0.71	47.1			
8	T1	202	3.0	0.950	117.6	LOS F	10.6	75.9	1.00	1.08	20.7			
9	R2	94	3.0	0.931	117.9	LOS F	9.6	68.8	1.00	1.04	20.5			
Appro	oach	507	3.0	0.950	75.3	LOS E	10.6	75.9	0.78	0.92	26.9			
West	: Great Ea	stern Hwy we	est											
10	L2	28	3.0	0.027	23.4	LOS C	1.0	7.5	0.46	0.66	42.4			
11	T1	1598	3.0	0.495	24.5	LOS C	27.6	198.3	0.65	0.59	42.9			
12	R2	76	3.0	0.418	89.1	LOS F	6.4	45.9	0.98	0.77	24.2			
Appro	ach	1702	3.0	0.495	27.4	LOS C	27.6	198.3	0.66	0.60	41.4			
All Ve	hicles	5696	3.0	0.950	45.5	LOS D	82.6	593.4	0.82	0.80	34.4			





#### 7.9.3 Grandstand Road/Resolution Drive/Stoneham Street Intersection

Headline SIDRA movement summaries for the Grandstand Road/Resolution Drive/Stoneham Street Intersection are presented in Table 19 (AM Peak hour) and Table 20 (PM peak hour). The SIDRA intersection geometry is shown in Figure 48. The full SIDRA movement summaries are presented in Figure 49 and Figure 50 (AM peak hour) as well as Figure 51 and Figure 52 (PM peak hour).

Overall the Grandstand Road/Resolution Drive/Stoneham Street Intersection SIDRA modelling results show that this intersection would perform with LOS A across all approaches to the intersection during the AM peak, both in the 2031 base scenario and 2031 with Golden Gateway development scenario.

Overall the Grandstand Road/Resolution Drive/Stoneham Street Intersection SIDRA modelling results show that this intersection would perform with LOS A or B across all approaches to the intersection during the PM peak, both in the 2031 base scenario and 2031 with Golden Gateway development scenario.

Table 19 – AM Peak Hour (0800-0859) Grandstand Road/Resolution Drive/Stoneham Street Intersection Assessment

		ndstand (north)			olution (south)		Sto	neham (west)	St	Al	l Vehic	les
Year	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS
2031	0.39	7.0	А	0.38	7.9	А	0.20	5.5	А	0.39	6.8	А
2031 + dev	0.41	7.1	А	0.49	8.9	А	0.25	5.9	А	0.49	7.1	А

Table 20 - PM Peak Hour (1600-1659) Grandstand Road/Resolution Drive/Stoneham Street Intersection Assessment

.,		ndstand (north)			olution (south)		Sto	neham (west)	St	Al	l Vehicl	es
Year	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS
2031	0.24	6.6	Α	0.67	8.8	Α	0.72	10.9	В	0.72	9.3	А
2031 + dev	0.29	6.5	А	0.79	11.8	В	0.80	14.4	В	0.80	11.5	В





Figure 48 – Grandstand Road/Resolution Drive/Stoneham Street Intersection – SIDRA geometry

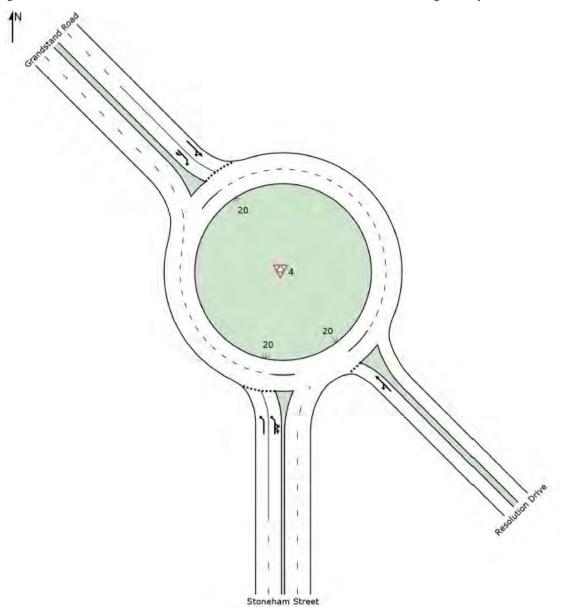






Figure 49 – 2031 AM Peak Grandstand Road/Resolution Drive/Stoneham Street Intersection

Move	ment Pe	rformance	- Vehic	les							
Mov ID	OD Mov	Demand l Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back Vehicles veh	of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South	Stoneha	m Street									
1a	L1	404	3.0	0.201	4.7	LOS A	1.1	7.7	0.41	0.53	54.6
3b	R3	59	3.0	0.201	10.9	LOS B	1.1	7.6	0.41	0.57	54.9
Appro	ach	463	3.0	0.201	5.5	LOS A	1.1	7.7	0.41	0.54	54.6
South	East: Res	olution Drive									
21b	L3	58	3.0	0.376	7.7	LOS A	1.8	12.9	0.64	0.80	52.0
22	T1	229	3.0	0.376	7.9	LOS A	1.8	12.9	0.64	0.80	53.9
Appro	ach	287	3.0	0.376	7.9	LOS A	1.8	12.9	0.64	0.80	53.5
North\	Nest: Gra	ndstand Road	d								
28	T1	314	3.0	0.388	4.4	LOS A	2.4	17.6	0.22	0.51	54.8
29a	R1	799	3.0	0.388	8.0	LOS A	2.4	17.6	0.23	0.56	53.2
Appro	ach	1113	3.0	0.388	7.0	LOS A	2.4	17.6	0.22	0.54	53.6
All Vel	nicles	1863	3.0	0.388	6.8	LOS A	2.4	17.6	0.33	0.58	53.9

Figure 50 – **2031 AM Peak with Golden Gateway development** Grandstand Road/Resolution Drive/Stoneham Street Intersection

Move	ment Pe	rformance -	- Vehic	les							
Mov ID	OD Mov	Demand f Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back Vehicles veh	of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South:	Stoneha	m Street									
1a	L1	475	3.0	0.253	5.0	LOS A	1.4	10.3	0.48	0.57	54.3
3b	R3	77	3.0	0.253	11.2	LOS B	1.4	10.1	0.49	0.61	54.5
Appro	ach	552	3.0	0.253	5.9	LOSA	1.4	10.3	0.48	0.58	54.3
South	East: Res	olution Drive									
21b	L3	73	3.0	0.487	8.7	LOS A	2.8	19.8	0.70	0.88	51.4
22	T1	293	3.0	0.487	8.9	LOS A	2.8	19.8	0.70	0.88	53.2
Appro	ach	366	3.0	0.487	8.9	LOSA	2.8	19.8	0.70	0.88	52.8
North\	Vest: Gra	ndstand Road	d								
28	T1	323	3.0	0.405	4.5	LOS A	2.7	19.1	0.27	0.51	54.6
29a	R1	810	3.0	0.405	8.1	LOS A	2.7	19.1	0.27	0.56	53.0
Appro	ach	1133	3.0	0.405	7.1	LOSA	2.7	19.1	0.27	0.55	53.5
All Vel	nicles	2051	3.0	0.487	7.1	LOSA	2.8	19.8	0.40	0.61	53.6





Figure 51 – 2031 PM Peak Grandstand Road/Resolution Drive/Stoneham Street Intersection

Move	ment Pe	rformance	- Vehic	les							
Mov ID	OD Mov	Demand Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back Vehicles veh	of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South	: Stonehai	m Street									
1a	L1	1227	3.0	0.719	10.7	LOS B	8.4	60.4	0.90	1.00	51.3
3b	R3	45	3.0	0.719	17.2	LOS B	8.1	58.2	0.91	1.02	51.6
Appro	ach	1272	3.0	0.719	10.9	LOS B	8.4	60.4	0.90	1.00	51.3
South	East: Res	olution Drive									
21b	L3	87	3.0	0.667	8.7	LOS A	5.5	39.4	0.71	0.87	51.4
22	T1	527	3.0	0.667	8.9	LOSA	5.5	39.4	0.71	0.87	53.2
Appro	ach	614	3.0	0.667	8.8	LOS A	5.5	39.4	0.71	0.87	53.0
North\	West: Gra	ndstand Roa	d								
28	T1	254	3.0	0.245	4.3	LOSA	1.5	11.1	0.19	0.47	55.3
29a	R1	445	3.0	0.245	7.9	LOSA	1.5	11.1	0.20	0.55	53.3
Appro	ach	699	3.0	0.245	6.6	LOSA	1.5	11.1	0.19	0.52	54.0
All Ve	hicles	2585	3.0	0.719	9.3	LOSA	8.4	60.4	0.67	0.84	52.4

Figure 52 – **2031 PM Peak with Golden Gateway development** Grandstand Road/Resolution Drive/Stoneham Street Intersection

Move	Movement Performance - Vehicles														
Mov ID	OD Mov	Demand l Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back ( Vehicles veh	of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h				
South:	Stoneha	m Street													
1a	L1	1237	3.0	0.796	14.2	LOS B	11.3	81.1	1.00	1.16	48.9				
3b	R3	55	3.0	0.796	20.8	LOS C	10.8	77.2	1.00	1.18	49.1				
Appro	ach	1292	3.0	0.796	14.4	LOS B	11.3	81.1	1.00	1.16	48.9				
South	East: Res	olution Drive													
21b	L3	130	3.0	0.794	11.7	LOS B	9.1	65.2	0.85	1.05	49.4				
22	T1	584	3.0	0.794	11.9	LOS B	9.1	65.2	0.85	1.05	51.0				
Appro	ach	714	3.0	0.794	11.8	LOS B	9.1	65.2	0.85	1.05	50.7				
North\	West: Gra	ndstand Roa	d												
28	T1	330	3.0	0.289	4.4	LOSA	2.0	14.1	0.23	0.45	55.3				
29a	R1	485	3.0	0.289	8.0	LOSA	2.0	14.1	0.23	0.55	53.1				
Appro	ach	815	3.0	0.289	6.5	LOSA	2.0	14.1	0.23	0.51	54.0				
All Vel	nicles	2821	3.0	0.796	11.5	LOS B	11.3	81.1	0.74	0.95	50.8				

### 7.9.4 Stoneham Street/Daly Street/Resolution Drive Intersection

Headline SIDRA movement summaries for the Stoneham Street/Daly Street/Resolution Drive Intersection are presented in Table 21 (AM Peak hour) and Table 22 (PM peak hour). The SIDRA intersection geometry is shown in Figure 53. The full SIDRA movement summaries are presented in Figure 54 and Figure 55 (AM peak hour) as well as Figure 56 and Figure 57 (PM peak hour).

Overall the Stoneham Street/Daly Street/Resolution Drive Intersection SIDRA modelling results show that this intersection would perform with LOS B or C across all approaches to the intersection during the AM peak, both in the 2031 base scenario and 2031 with Golden Gateway development scenario.





Overall the Stoneham Street/Daly Street/Resolution Drive Intersection SIDRA modelling results show that this intersection would perform with LOS B or C across all approaches to the intersection during the PM peak, both in the 2031 base scenario and 2031 with Golden Gateway development scenario.

Table 21 – AM Peak Hour (0800-0859) Stoneham Street/Daly Street/Resolution Drive Intersection Assessment

.,	Stoneham St (north)			Daly St (east)			Stoneham St (south)			Resolution Dr (west)			All Vehicles		
Year	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS
2031	0.62	13.3	В	0.05	22.7	C	0.24	11.6	В	0.26	21.2	C	0.63	13.6	В
2031 + dev	0.65	14.0	В	0.19	15.1	В	0.89	23.1	С	0.45	21.8	С	0.89	17.4	С

Table 22 – PM Peak Hour (1600-1659) Stoneham Street/Daly Street/Resolutions Drive Intersection Assessment

V	Stoneham St (north)			Daly St (east)			Stoneham St (south)			Resolution Dr (west)			All Vehicles		
Year	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS	Deg. Sat	Av. Del	LOS
2031	0.49	15.8	В	0.05	26.9	C	0.69	15.2	В	0.17	26.5	C	0.59	16.0	В
2031 + dev	0.73	18.9	В	0.09	19.4	В	0.87	29.2	С	0.22	23.8	C	0.89	25.2	С

Figure 53 – Stoneham Street/Daly Street/Resolution Drive Intersection – SIDRA geometry

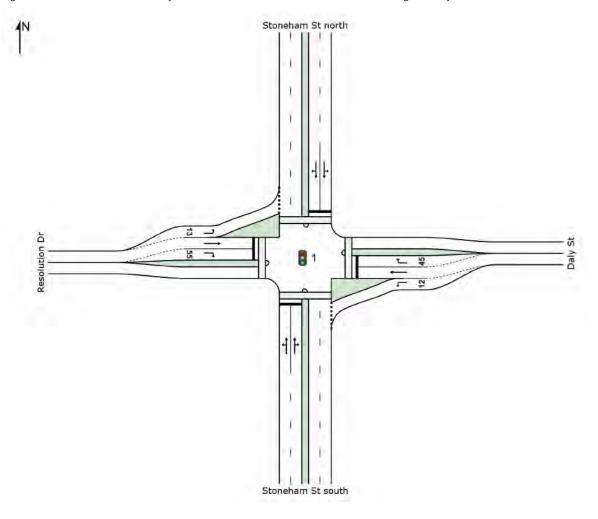






Figure 54 – 2031 AM Peak Stoneham Street/Daly Street/Resolution Drive Intersection

Move	ement Pe	rformance	- Vehic	les							
Mov ID	OD Mov	Demand Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back Vehicles veh	of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South	: Stoneha	m St south									
1	L2	32	3.0	0.244	16.0	LOS B	3.8	27.5	0.64	0.56	49.4
2	T1	363	3.0	0.244	11.1	LOS B	3.8	27.5	0.65	0.56	50.4
3	R2	9	3.0	0.244	17.3	LOS B	3.5	25.3	0.67	0.56	49.1
Appro	ach	404	3.0	0.244	11.6	LOS B	3.8	27.5	0.65	0.56	50.3
East:	Daly St										
4	L2	10	3.0	0.012	9.6	LOSA	0.1	0.7	0.45	0.61	51.1
5	T1	10	3.0	0.045	26.4	LOS C	0.3	1.9	0.87	0.59	41.9
6	R2	10	3.0	0.047	32.1	LOS C	0.3	2.0	0.91	0.67	38.4
Appro	ach	30	3.0	0.047	22.7	LOS C	0.3	2.0	0.75	0.62	43.2
North	: Stonehar	m St north									
7	L2	5	3.0	0.625	18.7	LOS B	12.5	89.5	0.81	0.71	48.1
8	T1	1049	3.0	0.625	13.2	LOS B	12.5	89.5	0.81	0.71	49.2
9	R2	28	3.0	0.625	18.8	LOS B	11.8	84.4	0.81	0.72	48.1
Appro	ach	1082	3.0	0.625	13.3	LOS B	12.5	89.5	0.81	0.71	49.2
West:	Resolutio	n Dr									
10	L2	51	3.0	0.049	7.1	LOS A	0.3	2.0	0.31	0.62	52.9
11	T1	12	3.0	0.054	26.5	LOS C	0.3	2.2	0.88	0.60	41.8
12	R2	54	3.0	0.255	33.3	LOS C	1.5	11.0	0.95	0.74	37.9
Appro	ach	117	3.0	0.255	21.2	LOSC	1.5	11.0	0.66	0.67	43.8
All Ve	hicles	1633	3.0	0.625	13.6	LOS B	12.5	89.5	0.76	0.67	48.9

Figure 55 – **2031 AM Peak with Golden Gateway development** Stoneham Street/Daly Street/Resolution Drive Intersection

Move	ement Pe	erformance	- Vehic	les							
Mov	OD	Demand		Deg.	Average	Level of	95% Back		Prop.	Effective	Average
ID	Mov	Total veh/h	HV %	Satn v/c	Delay	Service	Vehicles veh	Distance	Queued	Stop Rate	Speed km/h
South	· Stoneha	m St south	70	V/C	sec		ven	m		per veh	KIII/II
1	L2	38	3.0	0.457	17.4	LOS B	8.2	58.5	0.72	0.64	48.7
2	T1	368	3.0	0.457	11.8	LOS B	8.2	58.5	0.72	0.64	49.9
3	R2	174	3.0	0.887	48.4	LOS D	7.2	51.7	1.00	1.17	33.0
Appro	ach	580	3.0	0.887	23.1	LOS C	8.2	58.5	0.81	0.80	43.2
East:	Daly St										
4	L2	150	3.0	0.192	11.0	LOS B	1.9	13.3	0.55	0.70	50.1
5	T1	12	3.0	0.054	26.5	LOS C	0.3	2.2	0.88	0.60	41.8
6	R2	27	3.0	0.127	32.6	LOS C	0.8	5.4	0.93	0.71	38.2
Appro	ach	189	3.0	0.192	15.1	LOS B	1.9	13.3	0.62	0.69	47.4
North	: Stoneha	m St north									
7	L2	24	3.0	0.649	18.9	LOS B	13.1	94.3	0.82	0.73	47.9
8	T1	1053	3.0	0.649	13.7	LOS B	13.1	94.3	0.83	0.73	48.8
9	R2	31	3.0	0.649	19.7	LOS B	12.3	88.1	0.83	0.74	47.6
Appro	ach	1108	3.0	0.649	14.0	LOS B	13.1	94.3	0.83	0.73	48.8
West:	Resolution	on Dr									
10	L2	88	3.0	0.088	7.6	LOSA	0.6	4.3	0.35	0.63	52.5
11	T1	12	3.0	0.054	26.5	LOS C	0.3	2.2	0.88	0.60	41.8
12	R2	96	3.0	0.453	34.2	LOS C	2.8	20.3	0.97	0.77	37.6
Appro	ach	196	3.0	0.453	21.8	LOS C	2.8	20.3	0.69	0.70	43.4
All Ve	hicles	2073	3.0	0.887	17.4	LOS B	13.1	94.3	0.79	0.75	46.4





Figure 56 – 2031 PM Peak Stoneham Street/Daly Street/Resolution Drive Intersection

Move	ement Pe	rformance	- Vehic	les							
Mov ID	OD Mov	Demand Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back Vehicles veh	of Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South	: Stoneha	m St south	70	V/C	300		٧٥١١			per veri	KITI/TT
1	L2	68	3.0	0.689	20.1	LOS C	18.7	134.5	0.81	0.74	46.9
2	T1	1262	3.0	0.689	14.9	LOS B	18.7	134.5	0.81	0.73	48.1
3	R2	9	3.0	0.689	20.8	LOS C	18.3	131.4	0.82	0.73	47.0
Appro	oach	1339	3.0	0.689	15.2	LOS B	18.7	134.5	0.81	0.73	48.0
East:	Daly St										
4	L2	10	3.0	0.012	10.4	LOS B	0.1	0.9	0.43	0.61	50.6
5	T1	10	3.0	0.044	32.4	LOS C	0.3	2.3	0.87	0.59	39.2
6	R2	10	3.0	0.046	38.0	LOS D	0.3	2.4	0.91	0.67	36.1
Appro	oach	30	3.0	0.046	26.9	LOS C	0.3	2.4	0.74	0.62	41.1
North	: Stonehar	m St north									
7	L2	19	3.0	0.493	18.1	LOS B	11.6	83.0	0.69	0.62	48.4
8	T1	653	3.0	0.493	15.0	LOS B	11.6	83.0	0.73	0.65	47.9
9	R2	45	3.0	0.493	27.0	LOS C	6.8	48.5	0.84	0.73	43.0
Appro	oach	717	3.0	0.493	15.8	LOS B	11.6	83.0	0.74	0.65	47.5
West:	Resolutio	n Dr									
10	L2	34	3.0	0.054	11.1	LOS B	0.5	3.3	0.46	0.65	50.1
11	T1	11	3.0	0.048	32.4	LOS C	0.4	2.5	0.87	0.59	39.2
12	R2	37	3.0	0.170	39.0	LOS D	1.3	9.1	0.93	0.72	35.8
Appro	pach	82	3.0	0.170	26.5	LOS C	1.3	9.1	0.73	0.68	41.2
All Ve	hicles	2168	3.0	0.689	16.0	LOS B	18.7	134.5	0.78	0.70	47.4

Figure 57 – **2031 PM Peak with Golden Gateway development** Stoneham Street/Daly Street/Resolution Drive Intersection

Move	ement Pe	rformance	- Vehic	les							
Mov ID	OD Mov	Demand Total	Flows HV	Deg. Satn	Average Delay	Level of Service	95% Back Vehicles	of Queue Distance	Prop. Queued	Effective	Average
טו	IVIOV	veh/h	пv %	V/C	sec	Service	venicies	Distance	Queueu	Stop Rate per veh	Speed km/h
South	: Stoneha	m St south									
1	L2	108	3.0	0.886	33.1	LOS C	28.1	201.4	0.98	1.11	40.2
2	T1	1300	3.0	0.886	28.8	LOS C	28.1	201.4	0.99	1.12	40.5
3	R2	27	3.0	0.886	35.8	LOS D	23.7	169.9	1.00	1.13	39.4
Appro	ach	1435	3.0	0.886	29.2	LOS C	28.1	201.4	0.99	1.12	40.5
East:	Daly St										
4	L2	58	3.0	0.083	13.5	LOS B	8.0	6.0	0.61	0.68	48.5
5	T1	20	3.0	0.090	26.8	LOS C	0.5	3.8	0.88	0.62	41.7
6	R2	15	3.0	0.071	32.3	LOS C	0.4	3.0	0.92	0.68	38.3
Appro	ach	93	3.0	0.090	19.4	LOS B	0.8	6.0	0.72	0.67	45.0
North	: Stonehar	m St north									
7	L2	58	3.0	0.731	20.7	LOS C	16.2	116.4	0.87	0.81	46.6
8	T1	673	3.0	0.731	17.0	LOS B	16.2	116.4	0.89	0.82	46.5
9	R2	69	3.0	0.731	36.3	LOS D	4.8	34.7	1.00	0.91	38.2
Appro	ach	800	3.0	0.731	18.9	LOS B	16.2	116.4	0.89	0.83	45.6
West	Resolutio	n Dr									
10	L2	42	3.0	0.068	13.0	LOS B	0.6	4.2	0.59	0.67	48.8
11	T1	11	3.0	0.049	26.5	LOS C	0.3	2.1	0.88	0.59	41.8
12	R2	46	3.0	0.217	33.1	LOS C	1.3	9.3	0.94	0.73	38.0
Appro	ach	99	3.0	0.217	23.8	LOS C	1.3	9.3	0.79	0.69	42.4
All Ve	hicles	2427	3.0	0.886	25.2	LOS C	28.1	201.4	0.94	0.99	42.3





# 8. ANALYSIS OF EXTERNAL TRANSPORT NETWORKS

Impacts along Great Eastern Highway, Belgravia Street and Hardey Road are addressed in Section 7.





## 9. SAFE ROUTES TO SCHOOL

### 9.1 Schools Catchments

The Golden Gateway site is located within the following school catchments:

- Belmont Primary School (state primary school approx. 400m from the site); and
- Belmont City College (independent secondary school approx. 2.5km from the site).

Figure 58 shows the location of the above two schools in relation to the Golden Gateway site.

Figure 58 – Location of Schools in Relation to the Golden Gateway Site (source: Google Maps)



# 9.2 Walk and Cycle Routes to Schools

Belmont Primary School is located on the southwest corner of the traffic signal controlled intersection of Great Eastern Highway and Belgravia Street. The primary school is located approximately 400m from the centre of the Golden Gateway site, but a close as 50m-100m from the Belmont Trust Land portion of the site. The close proximity of Belmont Primary School to the site provides future residents with the ability to access a primary education facility via a short walk or cycle.





Belmont City College is located on Fisher Street, immediately to the south of Alexander Road and to the east of Abernethy Road (opposite Belmont Oasis Leisure Centre). The independent secondary school is located approximately 2.5km from the centre of the Golden Gateway site. The distance from the site to the secondary school means that walking and cycling trips will only be made by parents and students who are comfortable walking or cycling those distances along heavily trafficked routes.

Further details on the walk and cycle routes to Belmont Primary School and Belmont City College are provided below.

### 9.2.1 Belmont Primary School

The most direct pedestrian access route from the Golden Gateway LSP site to Belmont Primary School is via the pedestrian crossing across the western leg of the traffic signal controlled intersection of Great Eastern Highway and Belgravia Street.

Having crossed Great Eastern Highway, parents and students can walk along the footpath alongside the schools frontage with Great Eastern Highway to Lapage Street, where the pedestrian entry to the primary school is located.

It should be noted that prior to April 2016 legally in WA children aged 12 years old and under are able to ride on the footpath, in addition adults are legally able to ride on a footpath when accompanying a child under 12 years old. Since April 2016 it is now legal for anyone to ride on the footpath as long as they do so paying due care and attention to others users of the path.

As such the above pedestrian route can be legally used by parents and students to access the school. The direct pedestrian and cycle route from the LSP site to Belmont Primary School is shown in Figure 59.







### Belmont City College

The most direct pedestrian access route from the Golden Gateway LSP site to Belmont City College is via the pedestrian crossing across the western leg of the traffic signal controlled intersection of Great Eastern Highway, and then via Belgravia Street to Alexander Road, and along Alexander Road to Belmont City College located on Fisher Street.

The most direct pedestrian route between the LSP site and Belmont City College, requires crossing Great Eastern Highway via the signalised intersection with Belgravia Street, then walking along the footpath on the western side of Belgravia Street to Fairbrother Street, and then crossing Fairbrother Street to continue along the western side of Belgravia Street to Alexander Road. At Alexander Road you can walk along the footpath on the northern side of the street until you are opposite Belmont City College and then cross via the median island uncontrolled crossing to access the College site.

Similar to Belmont Primary School, students riding to Belmont City College can utilise the footpaths as it is now legal to do so. Older students that wish to ride on a shared path or on-road, could utilise the following route to access Belmont College from the LSP site; due to the dual traffic lanes in each direction along



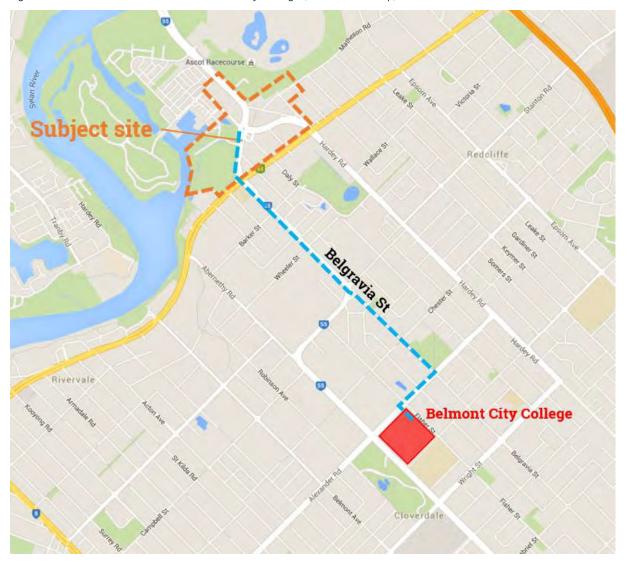


Belgravia Street between Great Eastern Highway and Fairbrother Street, the on-road cycling environment through that corridor is very hostile for inexperienced or younger cyclists.

As such it is more likely that students would cross Great Eastern Highway at the Hardey Road signalised intersection and cycle along Hardey Road to Alexander Road (note: Alexander Road has on-street bike lanes to the west of Belgravia Street intersection).

Alternatively, cyclists may use the lower traffic volume corridor of Daly Street between Great Eastern Highway and Alexander Road. The direct pedestrian and cycle route from the LSP site to Belmont Primary School is shown in Figure 60.

Figure 60 - Walk to School Route - Belmont City College (source: Nearmap)







### 10. CONCLUSIONS

### 10.1 Golden Gateway Local Structure Plan Context

The Golden Gateway Precinct is located within the City of Belmont and the LSP area is bounded by Ascot Racecourse to the north/northeast, Hardey Road to the east, Great Eastern Highway to the south, Swan River to the west and Ascot Waters to the west/northwest.

The Golden Gateway LSP is comprised of three main land uses, residential dwellings (approximately 3,000 dwellings), commercial space (approximately 5,900m² NLA) and retail space (approximately 1,200m² NLA). It is proposed that the three land uses will primarily be provided in mixed-use development sites across the Golden Gateway LSP area.

As noted in the Structure Plan Report, the LSP has been formulated around the following vision:

"The development of the Golden Gateway will transform this degraded and fragmented area into a vibrant precinct of residential and mixed use development, with strengthened connections to the Swan River and Ascot Waters, with uses, density and built form that derive best value from these attributes while respecting the area's rich culture and heritage."

The overarching objectives for the Golden Gateway Precinct as established by the project team and reinforced through stakeholder engagement include:

- Improve self-containment of facilities reduce car dependence;
- Improve peoples connection to the Swan River;
- Create accessible, quality public realm within the precinct; and
- Identify appropriate uses/densities in conjunction with infrastructure improvements.

### 10.2 Conclusions

### 10.2.1 Background Growth in Traffic

The available historic traffic count data on the road network surrounding the Golden Gateway site suggests that the road network has been subject to significant fluctuations in traffic volumes as works have been completed in the local area – most notably the Great Eastern Highway upgrades.

Based on the fluctuations in historic traffic count data and therefore the limited reliability of this data to accurately reflect recent historic trends from which to base background traffic growth in the future, and the recast ROM24 outputs suggesting that there is to be significant year on year growth around the Golden Gateway site, far in excess of levels of annual growth that would be expected to be sustained and accommodated on the surrounding road network – for the purposes of this assessment we have applied 15% growth (1% growth per annum) for the Great Eastern Highway corridor and 25% growth (1.7% growth per annum) for all other road corridors around the Golden Gateway site.

### 10.2.2 Intersection Performance

### Stoneham Street/Great Eastern Highway Intersection

The modelling results show that for the AM peak hour in the future base year 2031 scenario and 2031 with Golden Gateway development scenario, both have overall intersection LOS F. However, the modelling suggests that the additional AM peak hour traffic generated by the Golden Gateway site, that is predicted to use the Stoneham Street corridor, would place additional pressure on the Stoneham Street approach to the





Great Eastern Highway intersection and there would be a significant increase in delay from 152 seconds (2031 base scenario) to 270 seconds (2031 with development scenario).

The modelling results show that for the PM peak hour in the future base year 2031 scenario the Stoneham Street/Great Eastern Highway Intersection performs with an overall LOS E and in the 2031 plus Golden Gateway development scenario the intersection performs with an overall LOS F. The additional PM peak hour traffic generated by the Golden Gateway site, that is predicted to use the Great Eastern Highway (west) and Stoneham Street approaches to the intersection, would place additional pressure on the operation of the intersection and would see significant increases in intersection delay – Great Eastern Highway approach would increase in delay from 69 seconds (2031 base scenario) to 157 seconds (2031 with development scenario) and the Stoneham Street approach would increase in delay from 108 seconds (2031 base scenario ) to 182 seconds (2031 with development scenario).

### Resolution Drive/Great Eastern Highway Intersection

The modelling results show that for the AM peak hour in the future base year 2031 scenario and 2031 with Golden Gateway development scenario, both have an overall LOS D. The modelling results show that the additional AM peak hour traffic generated by the Golden Gateway site has a limited impact upon the operation of the Resolution Drive/Great Eastern Highway intersection.

The modelling results show that the PM peak hour in the future base year 2031 scenario the Resolution Drive/Great Eastern Highway Intersection performs with an overall LOS D. The modelling results show that the additional PM peak hour traffic generated by the Golden Gateway site has a marginal impact upon the operation of the Resolution Drive/Great Eastern Highway intersection – the average delay on each approach to the intersection is predicted to increase marginally within the inclusion of the development related traffic, however the Great Eastern Highway (west) is predicted to experience a more significant increase in average delay from 38 seconds (2031 base scenario) and 68 seconds (2031 with development scenario).

### Grandstand Road/Resolution Drive/Stoneham Street Intersection

Overall the Grandstand Road/Resolution Drive/Stoneham Street Intersection modelling results show that this intersection would perform with LOS A across all approaches to the intersection during the AM peak, both in the 2031 base scenario and 2031 with Golden Gateway development scenario.

Overall the Grandstand Road/Resolution Drive/Stoneham Street Intersection modelling results show that this intersection would perform with LOS A or B across all approaches to the intersection during the PM peak, both in the 2031 base scenario and 2031 with Golden Gateway development scenario.

### Stoneham Street/Daly Street/Resolution Drive Intersection

Overall the Stoneham Street/Daly Street/Resolution Drive Intersection modelling results show that this intersection would perform with LOS B or C across all approaches to the intersection during the AM peak, both in the 2031 base scenario and 2031 with Golden Gateway development scenario.

Overall the Stoneham Street/Daly Street/Resolution Drive Intersection modelling results show that this intersection would perform with LOS B or C across all approaches to the intersection during the PM peak, both in the 2031 base scenario and 2031 with Golden Gateway development scenario.

### 10.2.3 Pedestrian, Cycle and Public Transport Networks

The future development of the Golden Gateway Structure Plan would not only transform the pedestrian and cycle connections throughout the development site, but also provide a resident population that could be the catalyst in a step change in public transport service provision across the site/local area. It is anticipated that





there would be an uplift in public transport service and possible bus service operating through the development site.





### APPENDIX 1 - TRANSPORT IMPACT ASSESSMENT CHECKLIST





Item	Status	Comments/Proposals
Introduction/Background	✓	Section 1
Development proposal		
regional context	✓	Section 2
proposed land uses	✓	Section 2
table of land uses and quantities	✓	Section 2
major attractors/generators	✓	Section 2
Existing situation		
existing land uses within structure plan	✓	Section 3
existing land uses within 800 metres of structure plan area	✓	Section 3
existing road network within structure plan area	✓	Section 3
existing pedestrian / cycle networks within structure plan area	✓	Section 3
existing public transport services within structure plan area	✓	Section 3
existing road network within 2 (or 5) km of structure plan area	✓	Section 3
traffic flows on roads within structure plan area (PM and/or AM peak hours)	<b>√</b>	Section 3
Traffic flows on roads within 2 (or 5) km of structure plan area	✓	Section 3
existing pedestrian/cycle networks within 800m of structure plan area	✓	Section 3
existing public transport networks within 800m of structure plan area	✓	Section 3
Proposed internal transport networks		
changes/additions to existing road network or proposed new road network.	✓	Section 4
road reservation widths	✓	Section 4
road cross sections & speed limits	✓	Section 4
intersection controls	✓	Section 4
pedestrian /cycle networks and crossing facilities	✓	Section 4





Item	Status	Comments/Proposals
public transport routes	✓	Section 4
Changes to external road networks		
road network	✓	Section 5
intersection controls	✓	Section 5
pedestrian/cycle networks and crossing facilities	✓	Section 5
public transport facilities	✓	Section 5
Integration with surrounding area		
trip attractors/generators within 800m	✓	Section 6
proposed changes to land uses within 800m	✓	Section 6
travel desire lines from structure plan to these attractors/generators	✓	Section 6
adequacy of external transport networks	✓	Section 6
deficiencies in external transport networks	✓	Section 6
remedial measures to address deficiencies	✓	Section 6
Analysis of internal transport networks		
assessment years and time period	✓	Section 7
structure plan generated traffic	✓	Section 7
extraneous (through) traffic	✓	Section 7
design traffic flows (i.e. total traffic)	✓	Section 7
road cross sections	✓	Section 7
intersection controls	✓	Section 7
access strategy	✓	All sections
pedestrian/cycle networks	✓	Section 7
safe routes to schools	✓	Section 9
pedestrian permeability & efficiency	✓	Section 7
access to public transport	✓	Section 7
Analysis of external transport networks		Combined within Section 7 of the TIA
extent of analysis	✓	Section 7
Base flows for assessment years	✓	Section 7
Total traffic flows	✓	Section 7





Item	Status	Comments/Proposals
Road cross sections	✓	Section 7
Intersection layouts and controls	✓	Section 7
Pedestrian/cycle networks	✓	Wider area connections discussed in Section
		9 Safe Routes to School
Conclusions	✓	Section 10

Transport Assessors Name: Matthew Root

Company: Flyt Pty Ltd Date 17 June 2018





### Ordinary Council Meeting 26/02/19

Item 12.6 refers

**Attachment 20** 

**Local Water Management Strategy** 



## Golden Gateway Local Water Management Strategy

Prepared for City of Belmont

By Essential Environmental

June 2018



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### **CONTENTS**

1	In	troduction	1
1	1.1	Principles and design objectives	1
1	1.2	Planning background	2
1	1.3	Previous studies	2
2	Pr	oposed development	4
2	2.1	Key elements of the structure plan	4
2	2.2	Current land use	4
3	D	esign Criteria	7
3	3.1	Surface water management system	7
3	3.2	Groundwater management system	7
4	Ex	sisting site characteristics	8
4	4.1	Location and climate	8
4	4.2	Topography	
4	4.3	Geology and Soils	
4	4.4	Groundwater Resources	
4	4.5	Surface Water Resource	
4	4.6	Environmental and social	
4	4.7	Summary	19
5	W	ater management strategy	
į	5.1	Water sustainability initiatives	
į	5.2	Water servicing	
į	5.3	Stormwater management	
	5.4	Groundwater management	
į	5.5	Water quality management	
Į	5.6	Management of disease vectors and nuisance insects	28
6	In	nplementation	
(	5.1	Urban Water Management Plans	
	5.2	Pre-development monitoring	
	5.3	Construction	
(	5.4	Roles and Responsibilities	30
7	Re	eferences	32
Αp	pen	dix 1 – LWMS checklist	34
Αp	pen	dix 2 – Water Balance Modelling	36
Αp	pen	dix 3 – UNDO Modelling (Proposed Redevelopment)	39
Αp	pen	dix 4 – UNDO Modelling (Existing Conditions)	45



### **TABLES**

Table 1: Groundwater resource allocation and availability (as of February 2017)	12
Table 2: Annual summary water demands	21
Table 3: Typical on-site retention volumes	
Table 4: Summary of roles and responsibilities	31
FIGURES	
Figure 1: Integrating drainage planning with the land planning process (WAPC, 2008a)	
Figure 2: Location plan	
Figure 3: Proposed local structure plan	5
Figure 4: Previous land use	<i>6</i>
Figure 5: Climate summary data (Perth Airport, BoM, 2016)	8
Figure 6: Topography and soils	10
Figure 7: Acid sulfate soils	11
Figure 8: Groundwater Mapping	13
Figure 9: Surface water resources	
Figure 10: Environmental and social features	18
Figure 11: Typical roadside tree pit or raingarden	24
Figure 12: Stormwater management system	
Figure 13: Typical median swale or biofilter	



- ii -

### 1 INTRODUCTION

Essential Environmental has been commissioned by the City of Belmont to prepare this Local Water Management Strategy (LWMS) in support of a Local Structure Plan for the Golden Gateway Precinct located in Ascot. The study area is approximately 32 hectares in size and is located in the vicinity of Great Eastern Highway, Resolution Drive, Grandstand Road and Stoneham Street.

The site currently comprises of a mixture of commercial lots, the heritage listed Ascot Brick Works, places of public assembly (racecourse), public open space (including the Trust Land), and areas set aside for water supply, sewerage and drainage. The City of Belmont recognises the site as a promising opportunity for development due to its proximity to the Swan River and existing public open space. The proposed development will optimise the potential of the Golden Gateway Precinct by introducing high density commercial and residential areas, while maintaining the social and heritage values of the public open space and the Ascot Brick Works.

### 1.1 Principles and design objectives

Consistent with State Planning Policy 2.9: Water Resources (WAPC, 2006) and Better urban water management (WAPC, 2008a), a local water management strategy is required to be submitted to support any rezoning of land in a Local Planning Scheme or adoption of a Local Structure Plan to ensure that appropriate water management strategies are identified.

The position of this strategy within the state government planning framework is defined in Better urban water management (WAPC, 2008a) and Planning bulletin no. 92, urban water management (WAPC, 2008b) and is outlined in Figure 1.

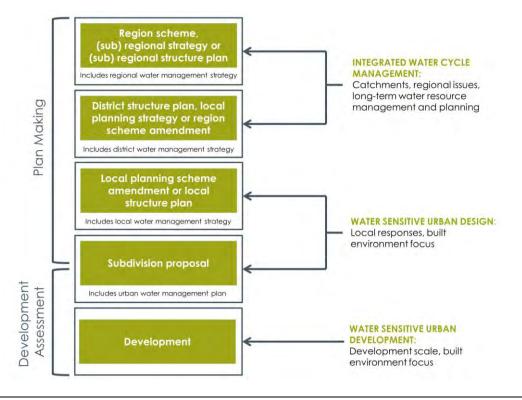


Figure 1: Integrating drainage planning with the land planning process (WAPC, 2008a)



- 1 - June 2018

In accordance with Better urban water management (WAPC, 2008a) preparation of a local structure plan is to be supported by a local water management strategy (LWMS) prepared in accordance with the Department of Water and Environmental Regulation's (DWER) Interim: Developing a local water management strategy (DoW, 2008a). The LWMS should demonstrate to the satisfaction of the WAPC in accordance with this plan and any approved DWMS:

- how the key principles and strategies of this plan have been addressed;
- how the urban structure will address water use and management;
- existing and required water management infrastructure; and
- detailed land requirements for water management.

The principles and strategies contained within section 4 of this Local water management strategy will be implemented as part of detailed land use planning and development requirements and are consistent with the framework and requirements in Better urban water management (WAPC, 2008a).

### 1.2 Planning background

The majority of the study area is zoned 'Urban' under the Metropolitan Region Scheme, with a small portion zoned 'Parks & recreation' and 'Mixed use' & 'Place of Public Assembly' under the City of Belmont Local Planning Scheme No. 15.

The City of Belmont Local Planning Scheme No. 15, adopted in December 2011, provides a district level framework to guide more detailed planning for the City. It requires local structure plans to be prepared to provide the level of detailed planning required to facilitate subdivision and development within the scheme area. The Western Australian Planning Commission and the City of Belmont are preparing a local structure plan to guide land use and development outcomes for the broader Golden Gateway precinct.

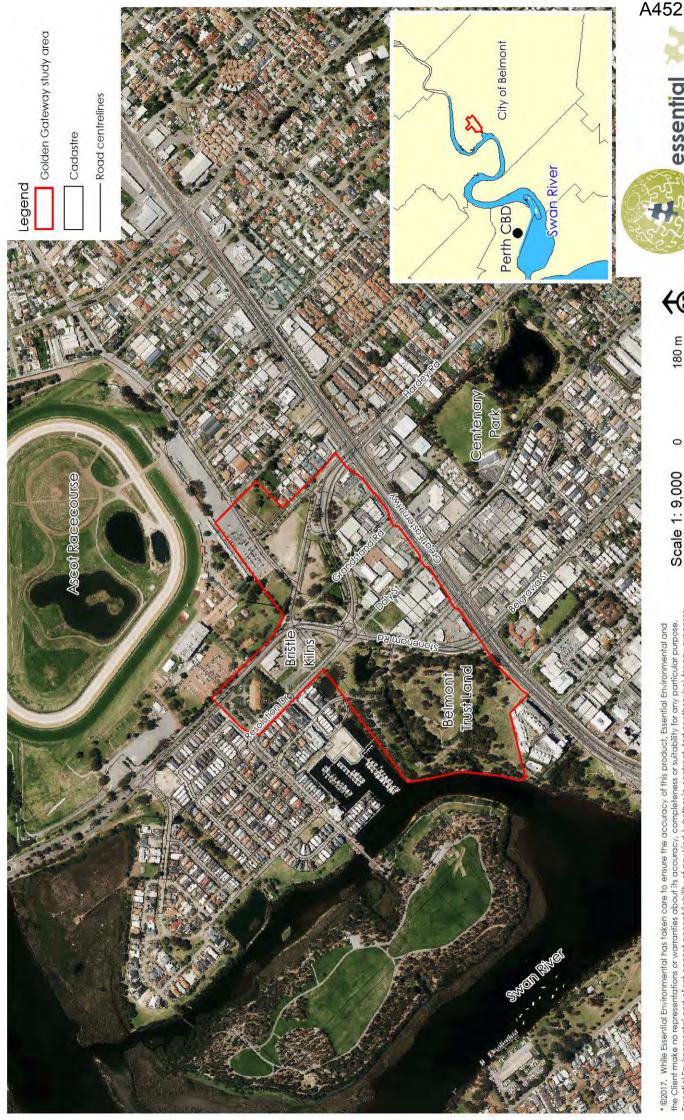
Figure 2 shows the Golden Gateway Precinct. A large portion of the Precinct is zoned 'Mixed use' in the Local Planning Scheme (LPS). The historical Ascot's beehive kilns and chimney stacks are located within this zone. A portion of the Precinct is zoned 'Place of Public Assembly (Racecourse)' accounting for portions of the Ascot Racecourse carpark and facilities. A small section of the precinct, approximately 6370 m², is zoned 'Parks and Recreation' under the Metropolitan Region Scheme. The waterway area in the Precinct is zoned 'water supply, sewerage and drainage' in the LPS. Water Corp owns the Lots in this zone and maintains the existing channels and piped drainage network system.

### 1.3 Previous studies

A number of previous studies have been undertaken that are relevant to this strategy area. In addition to *State Planning Policy 2.9*, these documents inform the strategies and management principles contained within this Local Water Management Strategy:

- City of Belmont Policy Manual (CoB, 2015);
- Developing a Local Water Management Strategy (DoW, 2008a);
- Stormwater Management Manual for Western Australia (DoW, 2004-07);
- Better Urban Water Management (WAPC, 2008a); and
- Decision Process for Stormwater Management in Western Australia (DWER, 2017a).





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Scale 1: 9,000

180 m

essential environmental

### 2 PROPOSED DEVELOPMENT

A brief summary of the current land use and proposed layout of the structure plan are provided below.

### 2.1 Key elements of the structure plan

The local structure plan will guide the subdivision and development of approximately 32 hectares in the Golden Gateway Precinct. The structure plan (Figure 3) contains:

- Predominantly a mixture of medium to high density residential areas with development between 2 and 15 storeys;
- Non-residential development at ground level in Mixed Use zones;
- Realignment of Stoneham Street, Resolution Drive and Grandstand Road;
- Upgrade of existing local roads to reflect an inner urban street character;
- Implementation of an east to west green spine replicating existing drainage; and,
- Retention of foreshore reserve Public Open Space (POS) surrounding the Swan River.

During the preparation of this report, the design of the local structure plan has been modified to incorporate water quality treatment and flood detention areas into the plan, based on the natural hydrology of the study area in order to minimise any potential environmental impacts resulting from redevelopment. Importantly, the existing drainage corridors are to be largely retained, rehabilitated and incorporated into a feature of the public open space.

Planning for the Belmont Trust land (Figure 3) will be undertaken separate to this Structure Plan. This area has been considered in this Local Water Management Strategy to facilitate future planning and ensure links between that land, the Golden Gateway area and the river are maintained.

### 2.2 Current land use

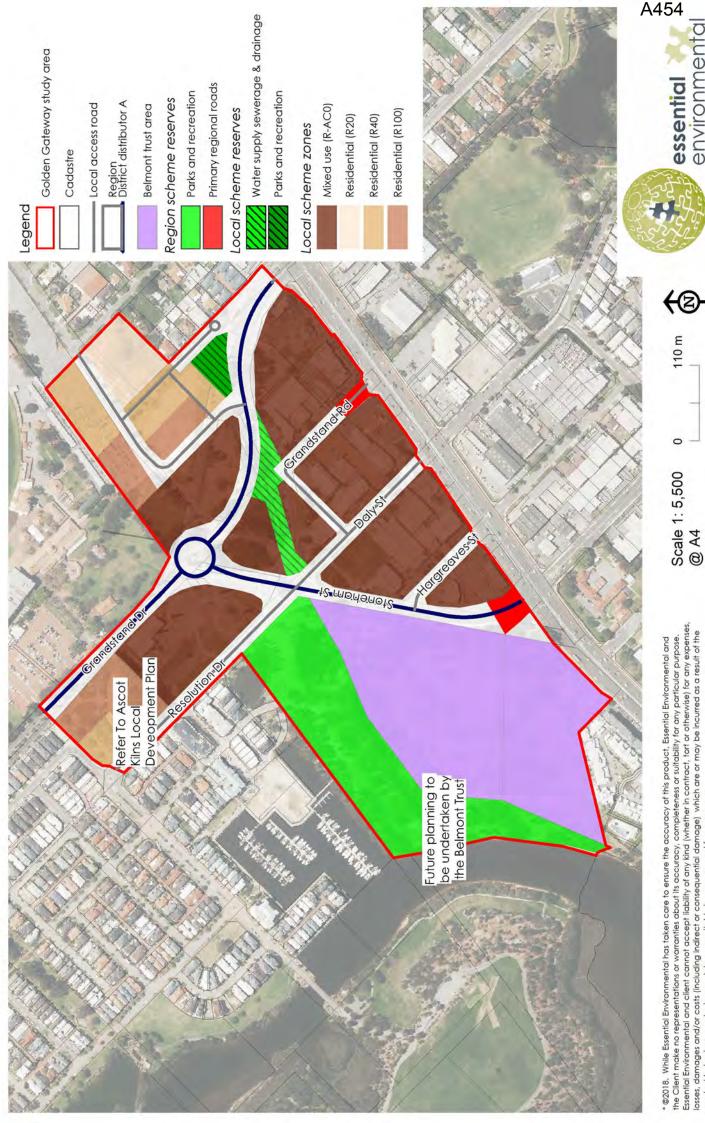
Commercial properties are dominant along Great Eastern Highway south of Resolution Drive and Stoneham Street. The north and east of Resolution Dr contains a parcel of land approximately 5 hectares in size that is largely vacant, with the exception of a few mature trees, used as overflow parking servicing the Ascot Racecourse. This portion of land, as shown in Figure 3, also accommodates a 150 m Central Belmont Main Drain, which discharges via piped drainage under the Stoneham Street / Resolution Drive roundabout into the Ascot Waters compensation basin on the north-western boundary of the study area. North of the Ascot Waters Compensation Basin is a second compensation basin that services the Ascot Waters development. This compensation basin is herein referred to as 'Northern Drainage Lake'. The northern portion of the site contains the Perth Racing Administration Offices.

Historical aerial photography from Landgate suggests the land has been used for commercial purposes for over 50 years, with the majority of lots being approximately 1/3 hectare, accommodating warehouse facilities and such, predominantly adjacent to Great Eastern Hwy. The northern portion of the study area contains the Bristle kilns and Brick Works, which were established in 1929 and ceased operation in 1982 (Heritage Council, WA). The western portion of the study area, over the now Belmont Trust Land, was historically used for sporting purposes such baseball fields (Clark, 1952), and more recently as a temporary worksite for development in the area, such as the widening of the Great Eastern Hwy.

- 4 -



City of Belmont: Golden Gateway - Local Water Management Strategy Figure 3: Proposed structure plan



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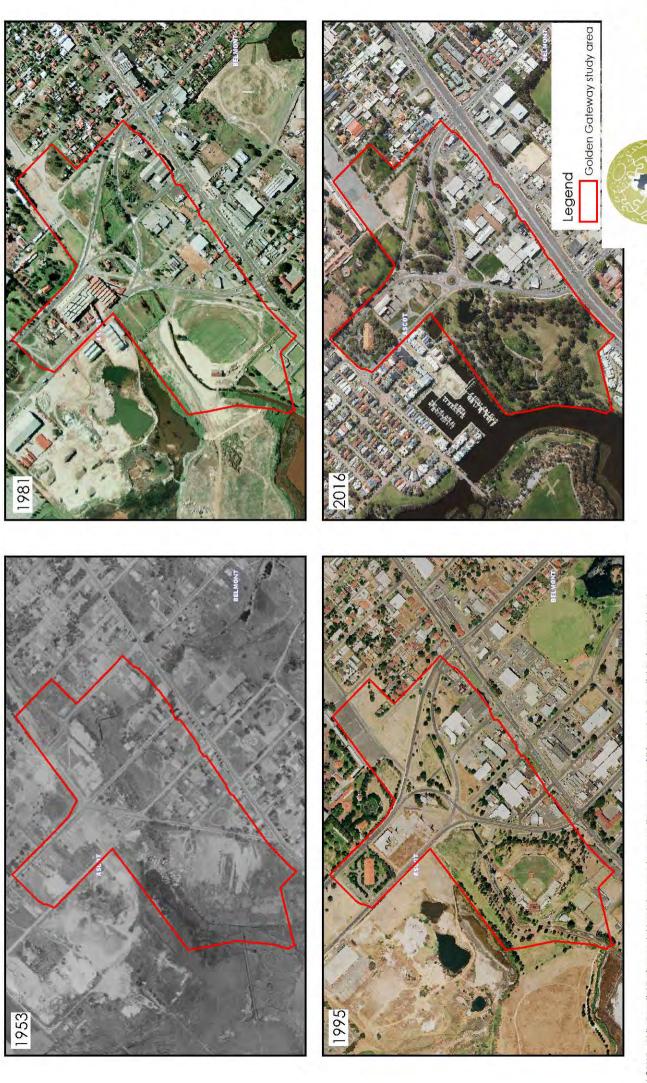
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200 m

Scale 1: 10,000 @ A4

# City of Belmont: Golden Gateway - Local Water Management Strategy Figure 4: Historical land use



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Data source: Co8, Landgate, Created by: A1. Projection: MGA: zone 50.

### 3 DESIGN CRITERIA

Any development within the Golden Gateway Precinct in Ascot should aim to meet the following specified design criteria using appropriate best management practices. Additional design criteria may need to be met, particularly where they are specific to particular precincts. These will be outlined in the relevant Detailed Area Plan or associated development guidelines.

### 3.1 Surface water management system

- The first 15mm of rainfall is to be retained within all lots through a combination of raingardens, water tanks and soakwell systems.
- Raingardens and tree-pits are to be installed in all new or upgraded streets to provide infiltration of the first 15mm of rainfall.
- Minor event runoff from events larger than 15mm total depth are to be managed in accordance with serviceability requirements of the City of Belmont.
- Roads and public open spaces are to be designed to cater for the surface overflow for more severe storm events with habitable floors at least 0.3 m above the 1% AEP flood or storage level at any location.
- Habitable floors are to be constructed at least 0.5m above the 1% AEP flood level in the Swan River adjacent to the development area.
- Water quality treatment systems and stormwater management structures should be designed in accordance with the Stormwater Management Manual for Western Australia (DoW, 2004-07) and Australian Runoff Quality: A guide to water sensitive urban design (Engineers Australia, 2006).

### 3.2 Groundwater management system

- Groundwater management systems are to be designed as free-discharging under normal operating conditions.
- Flows from groundwater management systems are to be treated prior to discharge.



### 4 EXISTING SITE CHARACTERISTICS

A summary of the existing environmental conditions in the study area are provided in this section, including determination of the opportunities and constraints for water management. The information presented incorporates data from the following reports:

- Lot 5, Resolution Drive Ascot Site Management Plan (GHD, 2013);
- Belmont Foreshore Precinct Plan (City of Belmont, 2014);
- Environmental Report: Golden Gateway (Essential Environmental, 2018a); and
- Central Belmont Main Drain Stage 2 Upgrade Review (Water Corporation, 2009).

### 4.1 Location and climate

The climate for Golden Gateway study area is typically Mediterranean having hot summers and mild winters with low annual rainfall. Average annual rainfall recorded at the nearest Bureau of Meteorology (BOM) weather station (Perth Airport (no. 9021) approximately 4 km east of the site) since 1944 is 768 mm but has declined in recent years to an average of 670 mm since 2002. The minimum recorded annual rainfall was in 2006 at just 480 mm and the maximum recorded was in 1955 at 1,165 mm.

The majority of rainfall is experienced in the winter between May and September with the driest months being December to February (Figure 5).

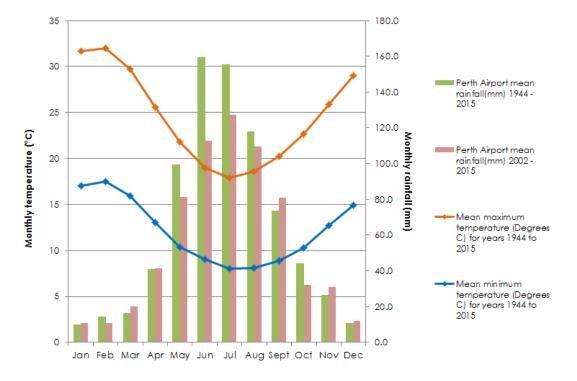


Figure 5: Climate summary data (Perth Airport, BoM, 2016)

Temperature recorded at the Airport (BOM station 9021) shows that the average maximum temperatures range between 17.9°C in July and 32°C in February, while average minimum temperatures range between 8°C in July and 17.5°C in February.



### 4.2 Topography

The study area grades down from Great Eastern Highway (between  $6-8\,$  m AHD) towards the Swan River or the Water Corporation drain and basin (approximately 1 m AHD), located along the north-western boundary. There are a few local depressions located adjacent to the Ascot Racecourse. The topography of the study area is shown in Figure 6.

### 4.3 Geology and Soils

The soils of the study area influence the ability to retain and infiltrate stormwater.

### 4.3.1 Surface geology and soils

The surface geology is described broadly as Guildford formation: Alluvial sand and clay with shallow-marine and estuarine lenses and local basal conglomerate (WA surface geology 1:250,000 scale geological maps, Geological Survey of WA, and Geoscience Australia).

As shown in Figure 6, two-thirds of the north-western portion of the study area is classified as Ms2 – Sandy Silt, and the eastern third as S8 – Sand, described as follows:

- Ms2 Sandy Silt: strong brown to mild grey, mottled, blocky, disseminated fine sand, hard when dry, variable clay content. This soil type is historically resourced for clay bricks and tile manufacture. It has a low permeability and low potential for erosion. Sandy Silt has a low shrink swell potential, however is prone to flooding.
- S8 Sand: very light grey at surface, yellow at depth, fine to medium grained, sub
  rounded quartz, moderately well sorted. Sand of aeolian origin is used for construction
  purposes with a high permeability and low erosion potential. Well drained given a low
  water table.

Geotechnical investigations will need to be undertaken to provide site specific information to inform the redevelopment of individual sites within the structure plan area.

### 4.3.2 Acid sulfate soils

A review of Department of Water and Environment Regulation acid sulfate soils (ASS) risk mapping (DWER, 2017e) identifies two-thirds of the study area, predominantly coinciding with surface geology Ms2-Sandy Silt as containing a Class I 'high to moderate' risk of ASS and the remainder, coinciding with S8-Sand, classified as Class II 'moderate to low' risk occurring within 3 m of the natural soil surface. The ASS mapping is provided in Figure 7.

### 4.4 Groundwater Resources

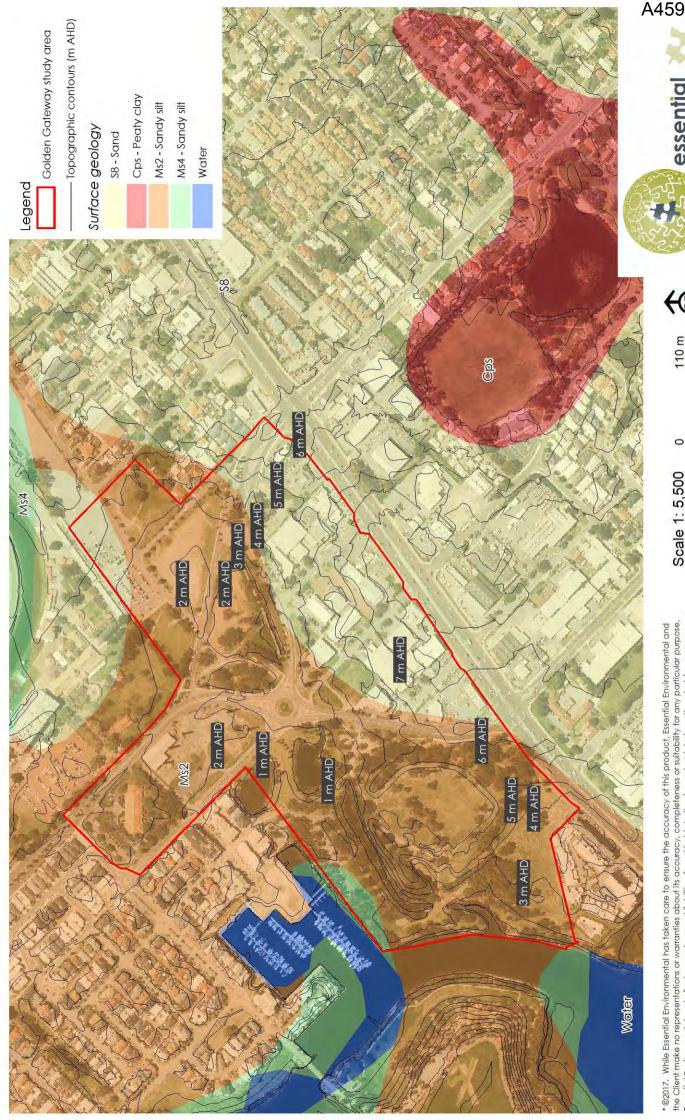
Determining the groundwater depth and quality are crucial for forming total cycle water management strategies. These components are outlined below.

### 4.4.1 Public Drinking Water Source Area

There are no Public Drinking Water Source Areas within the study area.



City of Belmont: Golden Gateway - Local Water Management Strategy Figure 6: Topography and surface geology



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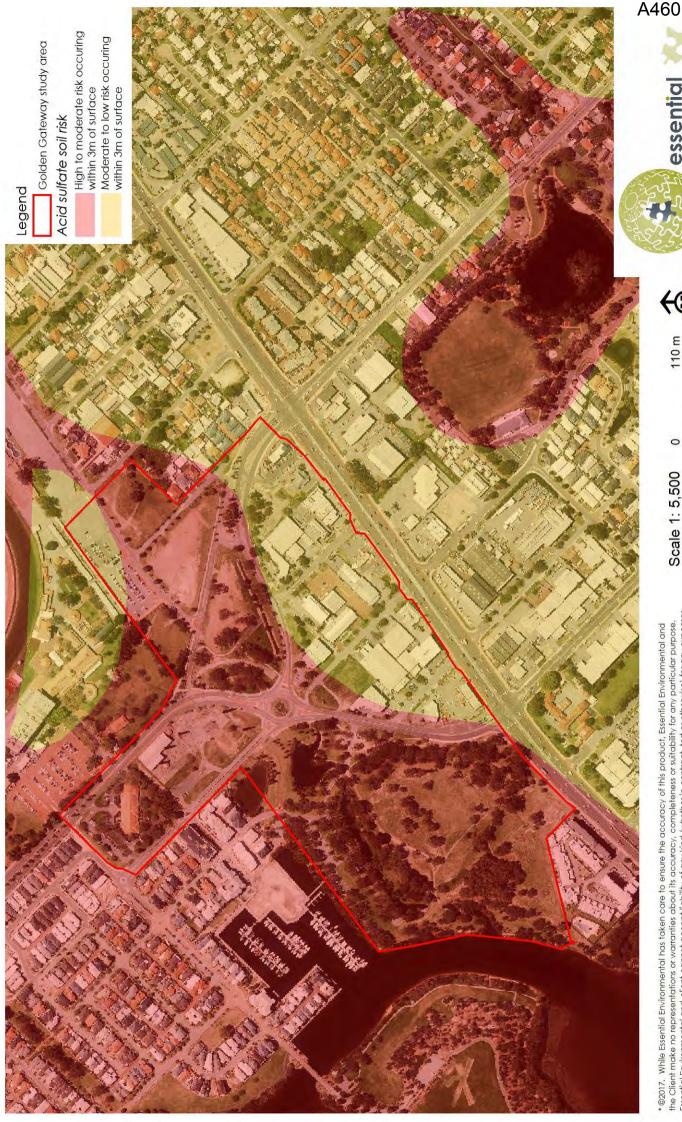
110 m

Scale 1: 5,500 @ A4

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City of Belmont: Golden Gateway - Local Water Management Strategy Figure 7: Acid sulfate soil risk



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Scale 1: 5,500 @ A4

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### 4.4.2 Allocation Information

The Study area is located within the Perth Groundwater Area and City of Belmont Subarea. The Department of Water and Environmental Regulation's Water Register (2017b) shows no further allocation available within the study area, as shown in Table 1.

Table 1: Groundwater resource allocation and availability (as of February 2017)

Management Area	Management Sub Area	Resource	Allocation Limit	Allocated Volume	Remaining Volume
Perth	City of Belmont	Perth – Superficial Swan	1,497,000	1,836,634	-339,634
Perth	Perth South Confined	Perth – Leederville	4,500,000	5,860,333	-1,409,233
Perth	Perth South Confined	Perth – Yarragadee North	400,000	800,000	-400,000

There are no current groundwater licences within the study area. The City of Belmont currently has a groundwater licence allocation of 1,171,200 kL (licence no. 157042) with a draw point located along the Swan River just south-west of the study area for POS irrigation throughout the City. Within the study area, irrigation of POS is provided from the Ascot Water Compensating Basin, which is unmetered, but estimated to be approximately 56,000 kL/yr. It is understood from the City of Belmont that local groundwater conditions are saline and future abstraction of groundwater from the superficial aquifer for irrigation will not be possible.

### 4.4.3 Groundwater Levels

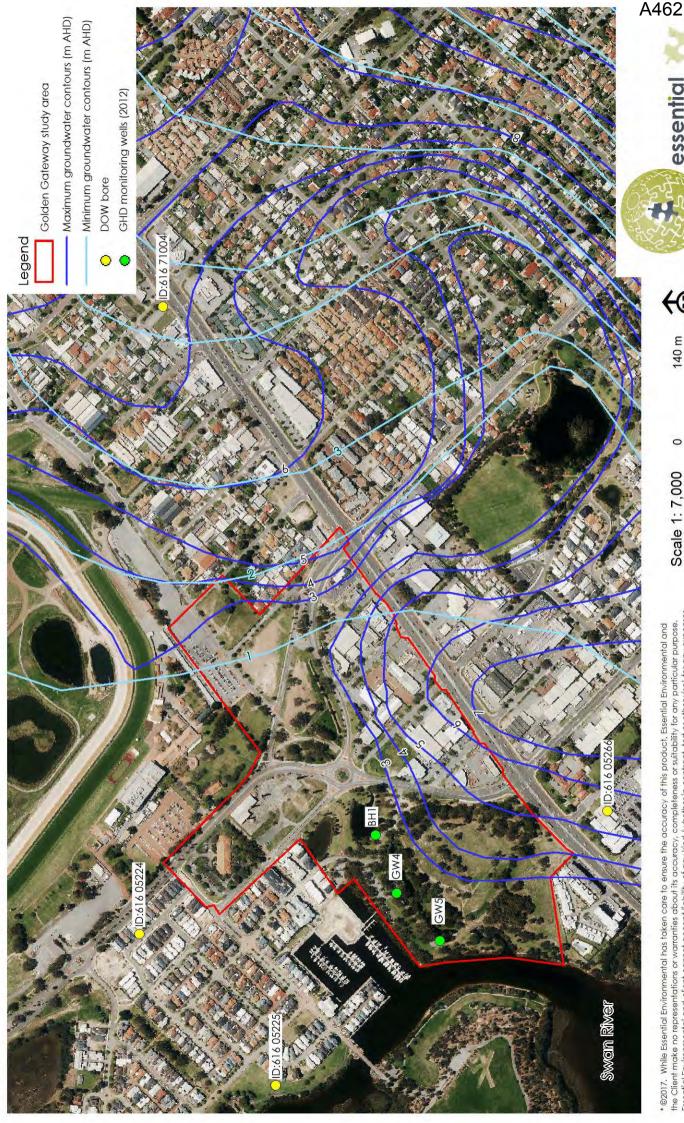
The Department of Water and Environmental Regulation's Perth Groundwater Atlas (DWER, 2017c) provides groundwater level contours across the study area based on maximum level data from 1996, as shown in Figure 8. Maximum groundwater level contours generally follow the topography contours, with levels ranging from 6 m AHD near Great Eastern Highway to 1 m AHD in the northwest corner of the Site. These contours indicate a minimum depth of approximately 1-2 m across the Site in the existing commercial areas, and potentially shallower groundwater west of Stoneham Street. Minimum groundwater levels from the Perth Groundwater Atlas (DWER, 2017c) are also presented in Figure 8 and are based on summer groundwater levels from 2003. These contours demonstrate that the groundwater flow direction is general west towards the Swan River.

These contours are extrapolated from the DWER network of bores as provided in the DoW Water Information Network (2017d). No long term DWER monitoring bores are located within the study area however several other bores are located within the vicinity, though they have not been subject to ongoing monitoring. These bores provide a snapshot of groundwater levels that are neither maximum nor minimum levels but would be expected to be within the natural seasonal variation. The most recently sampled bore was in 2011 (ID: 61671004) situated 500 m east and hydrologically upstream of the study area showing a groundwater level 4.5 m below ground level (BGL). Considering this information together with another bore close to the study area (ID: 61605266) which last recorded data from 1999 of 4 m BGL, which indicates that the groundwater level may be lower than the mapped minimum groundwater atlas levels.

Groundwater levels from two bores located north of the study area (ID: 61605225 and ID: 61605224) were measured in 1996 at approximately 3 m BGL. These bores are part of the Ascot Water development, which topographically sits approximately 2 m higher than the northern section of the study area and has been built-up for the purposes of the development. Therefore it is reasonable to conclude that the groundwater level of these bores is less likely to be representative of levels within the study area than the surrounding locations.



City of Belmont: Golden Gateway - Local Water Management Strategy Figure 8: Groundwater levels



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140 m

Scale 1: 7,000 @ A4

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Data source: WC, DEC, Landgate. Created by: AT. Projection: MGA: zone 50.

As part of a contamination investigation of Lot 5 Resolution Drive, adjacent to the Water Corporation drain in the west of the study area, water levels in groundwater monitoring bores were measured. Groundwater was identified as between 1.56 m BGL (BH1) and 1.76 m BGL (GW5), which is consistent with mapping from the Perth Groundwater Atlas (DWER, 2017c).

### 4.4.4 Groundwater Quality

There have been limited assessments of groundwater quality within the study area. As part of the contamination investigations of Lot 5 Resolution Drive (GHD, 2013) (outlined in Section 4.6.4) three groundwater monitoring bores (Figure 8) were sampled for water quality. Results from this sampling indicated the following exceedances of Australian and New Zealand Environment and Conservation Council (ANZECC) guideline values:

- Concentrations of iron (3.9, 20 and 1.5 mg/L) were above ANZECC Fresh Water (0.3 mg/L) and Marine Water (1.0 mg/L) criteria;
- Concentrations of zinc (0.026, 0.009 and 0.017 mg/L) and were above ANZECC Fresh Water (0.008 mg/L) criteria in all bores and Marine Water (0.015 mg/L) criteria in bores GW4 and BH1;
- Concentrations of ammonia were above ANZECC Fresh Water (0.9 mg/L) and Marine Water (0.91 mg/L) criteria in GW4 (3.8 mg/L) and GW5 (4.9 mg/L);
- Concentrations of nitrate were above ANZECC Marine Water (0.005 mg/L) criteria in all bores and above Fresh Water (0.01 mg/L) criteria in GW4 (0.019 mg/L) and GW5 (0.019 mg/L); and
- Concentrations of total nitrogen exceeded ANZECC Fresh Water (2 mg/L) criteria in GW4 (5 mg/L) and GW5 (5.9 mg/L).

These exceedances were considered by GHD to be characteristic of the winter conditions in the Swan River and natural soils in the area rather than impact associated with fill material on this site. Note that the site management plan was not implemented.

The City of Belmont has identified that groundwater in the area is saline and therefore not suitable for irrigation of landscaped areas.

### 4.5 Surface Water Resource

Existing surface water features within the study area may require protection from development or provide opportunities for modification to deliver the community an asset with social and ecological benefits. The surface water resources are outlined in below.

### 4.5.1 Natural Water Resources

The Swan River is adjacent to the western portion of the study area. The Swan River holds significant ecological value because it provides habitat for local and migratory birds and other fauna, with the majority of the River being identified as a conservation category wetland and environmentally protected area. Furthermore, the Swan River provides important social value for visual amenity, recreation on the river and its reserves. The Swan River also holds Aboriginal and European heritage significant values. The Department of Water and Environmental Regulation Floodway mapping indicates that a large area in the northern portion of the study area lies within the Swan River 100 year average reoccurrence interval (ARI) flood fringe (Figure 9).

A portion of the site is located within the Swan River Trust Development Control area (Figure 9). Land use planning and development within the Development Control Area is subject to



approval of the Department of Biodiversity, Conservation and Attractions under Part 5 of the Swan and Canning Rivers Management Act 2006 and the Swan and Canning Rivers Management Regulations 2007. This area includes the waterways of the Swan and Canning rivers and the adjoining parks and recreation reserves.

Water management strategies for the study area will need to ensure protection of the Swan River, particularly with regards to water quality improvement. The catchments of the Swan Canning River system are the subject of the Swan Canning Water Quality Improvement Plan (SRT, 2008) which contains catchment management measures and control actions. It was developed as a part of the Coastal Catchments Initiative, with the following aim of reducing nitrogen and phosphorus inputs to the Swan-Canning river system.

The Belmont Foreshore Precinct Plan (City of Belmont, 2014) was prepared to guide development and landuse within the river setting and ensure that the landscape values of the river system are conserved or enhanced. The study area, particularly Trust Land and POS contains areas identified as parkland within the precinct plan, characterised by open lawns surrounding large individual trees. The precinct plan outlines strategic recommendations that will need to be incorporated into future planning of the Trust Land.

On the basis of predictive modelling, the Swan Canning Water Quality Improvement Plan reports the maximum acceptable load to the Swan and Canning rivers per year as 130 tonnes of total nitrogen (TN) and 14 tonnes of total phosphorus (TP). To meet these objectives the Swan Canning water quality improvement plan aims to:

- reduce the nitrogen load by 120 tonnes per year (49%); and
- reduce the phosphorus load by 12 tonnes per year (46%).

### 4.5.2 Drainage

Drainage throughout the study area is part of the Water Corporation's Central Belmont Main Drain system and consists of a combination of piped drainage, open channels and constructed compensation basins. The study area is at the downstream end of a larger catchment within the City of Belmont that extends south and east covering are area of approximately 350 ha. This wider catchment includes the Centenary Park compensation basin.

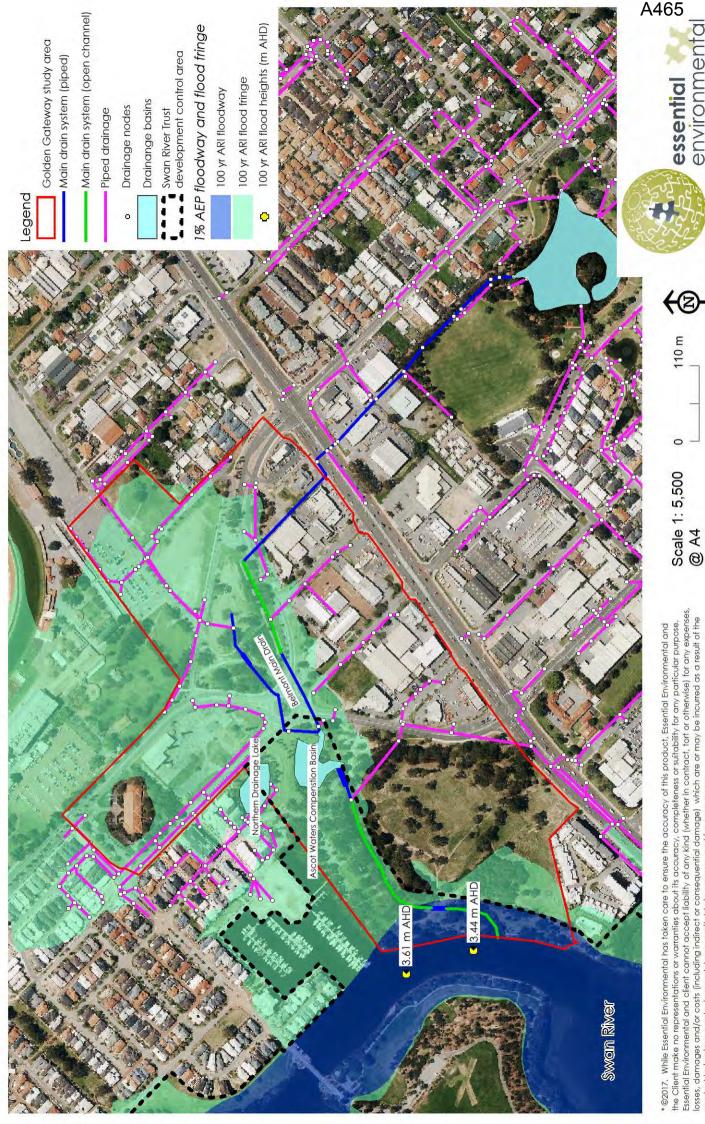
The Belmont Main Drain system enters the study area under Great Eastern Highway through a 1500 mm pipe that flows from Centenary Park to north of Resolution Drive. This pipe discharges to an open drain that flows approximately 150 m to the west. This drain has a trapezoidal shape and straight alignment that was modified most recently in the 1980s (Figure 9).

It is understood that the Water Corporation planning for the system includes a number of capital works to modify the system upstream of the study area (Water Corporation, 2009). Within the study area, the open section of Belmont Main Drain between Stoneham Street and Resolution Drive is recommended to be piped for safety reasons. No other modifications to the Main Drain were recommended by the Water Corporation (Water Corporation, 2009) since it was found to operate satisfactorily in accordance with its design requirements.

Belmont Main Drain discharges under the Stoneham Street / Resolution Drive roundabout to the Ascot Waters Compensation Basin before extending another 350 m in an open drain towards the Swan River (Figure 9). The Ascot Water Compensation Basin controls flow rates and allows sediment settlement prior to entering the Swan River. A contaminated sites investigation was conducted by GHD and a Site Management Plan was subsequently developed in 2013 for the expansion of this compensation basin, though it was not implemented. The investigation identified issues of leachable metals, PAH and TPH fractions and asbestos (see section 4.6.4).



City of Belmont: Golden Gateway - Local Water Management Strategy Figure 9: Surface water resources



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product being inaccurate, incomplete or unsuitable in any way and for any reason. Data source: WC, DEC, Landgate. Created by: AI. Projection: MGA: zone 50. The remainder of the drainage, as shown in Figure 9, consists of traditional side entry pits and piped drainage along existing roadways. Inflows into the Ascot Waters Compensation Basin have been monitored by the City of Belmont with data available publically on the Water Information Reporting (WIR) Database. The data indicated that mean values of Total Nitrogen (0.96 mg/L) were below ANZECC guidelines values (1.2 mg/L), whilst Total Phosphorus (0.11 mg/L) was above (0.07 mg/L).

North of the Ascot Waters Compensation Basin and outside the study area is a second compensation basin servicing the Ascot Water development, the Northern Drainage Lake. The Northern Drainage Lake has experienced water quality issues in the past with two fish kill incidents occurring during July and September 2012. The first incident involved approximately 300 fish deaths and the latter 100-150 fish deaths. No incidents have occurred since 2012. Investigations were undertaken by the Swan River Trust in 2012 in response to the fish kills. Water quality testing indicated low concentrations of algae, and higher concentrations of organic matter resulting in oxygen-depleted water. In addition, it was identified that fish often become trapped in backwaters such as this lake.

### 4.5.4 Wetlands

Geomorphic wetland mapping (DCBA, 2017) demonstrates that there are no wetlands within the study area (Figure 10). The nearest significant mapped wetland is the Swan River to the west and downstream of the study area which is identified as a conservation category wetland.

### 4.6 Environmental and social

Several environmental and social factors are either influence or are dependent on management of the water cycle with the study area. A summary of these factors are provided below.

### 4.6.1 Conservation Area

An environmentally sensitive area, as mapped by the Department of Water and Environment Regulation surrounds the Bush Forever site as described above. This area is described as 'Temperate Saltmarsh' and listed as 'vulnerable' under the Environment Protection and Biodiversity Conservation Act 1999 (EPBC Act). Temperate salt marshes are an important habitat for local and migratory bird species (Department of Environment, 2015).

### 4.6.2 Vegetation

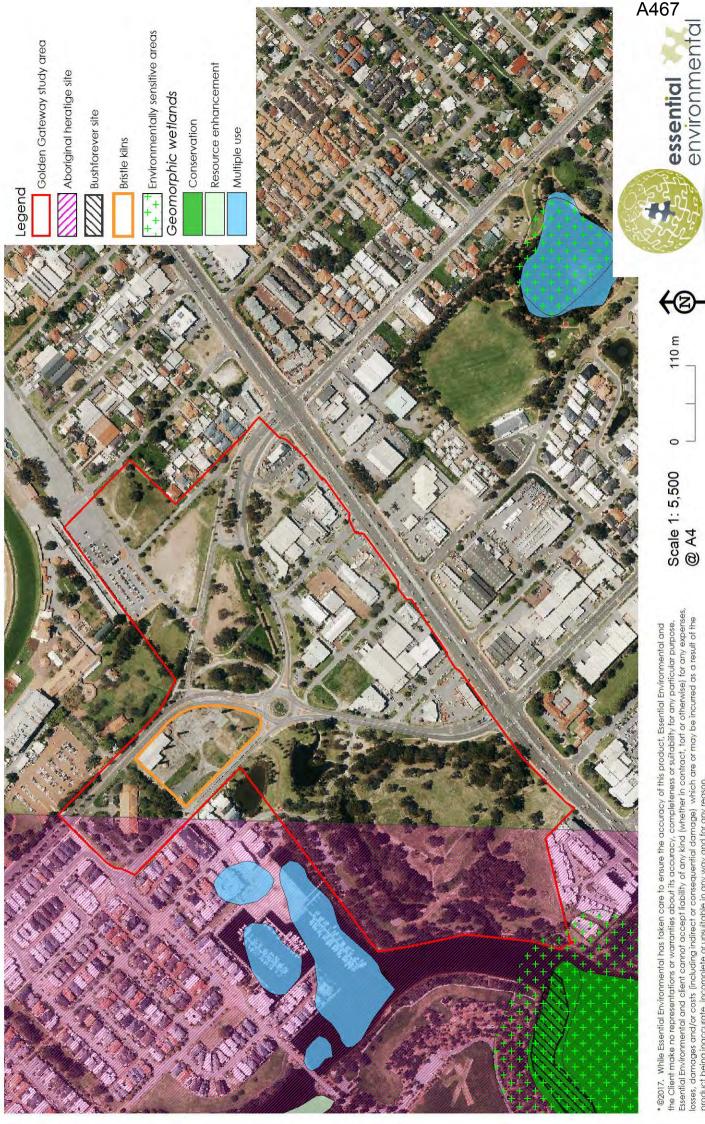
There are no Bush Forever sites within the study area, reflecting the existing commercial land use through the study area. Notably there is no significant vegetation along the Belmont Main Drain upstream of the Stoneham Street / Resolution Drive roundabout.

Bush Forever site 313, Swan River Salt Marshes, exists to the north and west of the study area, as shown on Figure 10. The closest proximity of the Bush Forever site to the study area is adjacent to Trust Land at the south-western boundary. Apart from this point, the study area is largely disconnected from the Bush Forever site.

A portion of the vegetation along the banks of the Swan River has been identified as a Bush Fire Prone Area. A Bushfire Management Plan (Essential Environmental, 2018b) has been prepared for management of these areas.



City of Belmont: Golden Gateway - Local Water Management Strategy Figure 10: Environmental and heritage features



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Scale 1: 5,500 @ A4

### 4.6.3 Heritage

A search of the Department of Planning, Lands and Heritage aboriginal heritage enquiry system showed one site overlaying the study area (Figure 10):

• Site ID 3753 – Registered site, Name: Perth, Type: Historical, mythological, hunting place, named place, natural feature.

One other site is adjacent to the study area, however not within the boundary, site ID 3536 - Registered site, Name: Swan River, Type: mythological.

The significant European Heritage feature of the study area is the Bristle Kilns; beehive and tunnel kilns, with associated chimney and floor ducts, located at 197 Grandstand Rd Ascot. The Kilns were first built in 1930, manufacturing terracotta, stoneware and steel products. Production ceased in 1982 (Heritage Council, 2012). The Kilns and chimneys remain and were placed on the State Heritage List in 2003.

### 4.6.4 Contamination

A search of the Department of Water and Environmental Regulation Contaminated Sites database found no contaminated sites within the study area. Lot 5 Resolution Drive (160 Stoneham Street) is listed as "Possibly Contaminated, Investigation Required".

The Ascot Waters Compensation Basin was assessed by Douglas Partners (2009) to determine the occurrence of acid sulphate soils and waste classification in support of expanding the basin. A Preliminary and Detailed Site Investigation was developed by GHD along with a Site Management Plan for this area (GHD, 2013). A summary of the contamination issues identified through these investigations include:

- Soil (Inorganic): Exceedances of metals (As, Ca, Mn, Hg, Ni, Pb, Zn, Al, Fe) above Ecological Investigation Levels and Health Investigation Levels were minimal and asbestos was considered to be low risk (though further investigations are required);
- Soil (Organic): Hydrocarbons were localised and not considered to pose a risk to ecological or human receptors;
- Groundwater (Inorganic): Three groundwater bores were sampled to test for Fe, Zn, Ni, NO<sub>3</sub>, NO<sub>2</sub>, Total Nitrogen and Total Phosphorus with results generally consistent with winter conditions in the Swan River and natural soils in the area; and,
- Groundwater (Organic): Samples were analysed for BTEX, Total Recoverable Hydrocarbons (TRH) and Polycyclic Aromatic Hydrocarbons (PAH) with all reported below the DER Domestic Non-Potable Water Criteria (GHD, 2013).

Further details are provided in the Golden Gateway Local Structure Plan Environmental Report (Essential Environmental, 2018a).

### 4.7 Summary

Based on the review of the geological, hydrological and environmental information for the study area, the key considerations for water management are as follows:

- No additional groundwater allocation (beyond existing licences) available for irrigation of public open space;
- Saline groundwater conditions unsuitable for irrigation;



- Groundwater is generally shallow across the study area, approximately 1-2m below the surface in some areas;
- Potentially high risk of acid sulfate soils present in parts of the site with sandy silt soils;
- Former land uses may have contributed to legacy nutrients within groundwater;
- Limited water sensitive urban design features throughout the existing development areas;
- Water Corporation planning includes replacing the open Central Belmont Main Drain between Resolution River and Stoneham Street with a piped system for safety concerns; and,
- The Swan River which presents a number of considerations including the 100yr ARI flood fringe, conservation category wetland mapping and the Swan River Trust Development Control Area in the west of the study area.



### 5 WATER MANAGEMENT STRATEGY

Water management strategies for the study area have been prepared in accordance with the guiding documents, policies and strategies (Section1), the intended redevelopment (Section 2) and the site considerations (Section 4). Strategies for water sustainability, stormwater, groundwater, and water quality improvement are outlined in this Section.

### 5.1 Water sustainability initiatives

- Ensure the efficient use of all water resources in the redeveloped urban form and aim to achieve highest value use of fit-for-purpose water;
- maintain opportunities for future generations by using water more efficiently.

A broad scale water demand model has been developed, using Essential Environmental's Water Balance Tool, to identify the various post-development water demands from residential and non-residential areas. Summary results of this water balance modelling are presented in Table 2 and details are provided in Appendix 2. The modelling is based on yield estimates for the Golden Gateway area, excluding the Belmont Trust land. A separate demand model will be required for that area as planning progresses.

The analysis is based on the development comprising 46 townhouses and 3,412 apartments, with an approximate population of 6,250 residents. Analysis assumes the use of standard water efficiency measures within households and irrigation of up-to 25% of townhouse lot areas and 5% of apartment lot areas consistent with current State Government policies (watering for 2 days per week through summer). These demands are consistent with a target of less than 100 kilolitres per person per year.

The most significant public non-drinking water demand for the redevelopment will be public open space irrigation. As discussed in Section 4.4.4, the saline groundwater conditions prevent the local abstraction of groundwater for irrigation and therefore an alternative source is required. The City may consider stormwater capture, reuse and harvesting options during further detailed design of the redevelopment and public spaces, for example using paving design to collect local runoff. To further reduce demand, landscaping within the redevelopment will use hydrozoning and smart meters.

Other non-drinking water demands include toilets and washing machines in homes and garden irrigation. Although there is no proposal to provide for these demands through a non-drinking water scheme there remains potential for significant reductions in scheme water use to be achieved through water efficiency measures within individual apartment buildings potentially including stormwater harvesting and or greywater reuse.

Table 2: Annual summary water demands

Scenario	Drinking	Non-drinking	Total
Residential (kL)	213,707	176,314	390,021
Commercial (kL)	900	600	1,500
Public Open Space (kL)		10,058	10,058
Total (kL)	214,607	186,972	401,579
Estimated Household Consumption (kL/person/year)	34	28	62



# 5.2 Water servicing

The strategy area is located in an area served by the Water Corporation's integrated water supply scheme. All dwellings will be connected reticulated drinking water distribution network. Similarly, the strategy area is located in an area served by the Water Corporation's integrated sewerage scheme and will be connected to a reticulated sewerage network.

# 5.3 Stormwater management

The key objectives for surface water management are:

- Protection of wetlands and waterways (receiving environments) from the impacts of urban runoff; and,
- Protection of infrastructure and assets from flooding and inundation;

The following planning measures are adopted to achieve the above objectives:

- Residential, industrial or commercial premises in existing or proposed areas must have their floor levels elevated 500 mm above the 1% AEP flood level (100yr ARI) in the Swan River and 300 mm above the 1% AEP flood level in the local drainage system;
- Runoff from events greater than the 100% AEP (1yr ARI) interval event and up to the 20% AEP (5yr ARI) event in residential areas and 10% AEP (10yr ARI) event in commercial/industrial areas are to be managed in accordance with the serviceability requirements of Australian Rainfall and Runoff (Engineers Australia, 2001) minor/major system;
- The design of the redeveloped urban areas should incorporate current best practice in water-sensitive urban design to mitigate the potential impacts on regional water quantity and quality from redevelopment and the legacy conditions within the catchment;
- Retrofitting of stormwater management systems to achieve improved water quality outcomes should be maximised through the installation of biofilters (raingardens), amended soils and the use of structural controls to address litter, sediment and vegetative materials at source;
- Modification of the existing Central Belmont Main Drain and local drainage systems to suit the urban form whilst maintaining drainage capacity and peak flow rates; and,
- Water-sensitive urban design and best management practices promoting on-site retention of the first 15 mm of rainfall form the basis of the surface water quantity management strategy for minor events.

#### 5.3.1 Small event management

The development will retain no less than the first 15 mm of rainfall on-site within lots and streets. It is understood that Water Corporation system has sufficient capacity for redevelopment of the study area, however retention of the 15mm event will allow water quality objectives to be achieved (Section 5.5).

Retention of runoff within lots (multi-story mixed use development) will be achieved through a combination of raingardens, water tanks and soakwell systems. Rainfall captured from roof areas is suitable for non-potable reuse in-house (toilets, laundry) and ex-house (garden areas), and will assist in achieving the water sustainability objectives outlined in Section 5.1. The suitability of a roof runoff capture and reuse system will be determined during detailed design of each building. Where these systems are not viable, roof runoff will be captured and infiltrated in underground infiltration systems within the lot boundaries. Developments will be



required to provide on-site retention within raingardens for runoff from other impervious areas on lots (pavement, carparks).

Upgrading of local roads to deliver an inner urban street character will provide the opportunity to incorporate water sensitive urban designs. Raingardens and tree-pits (e.g. see Figure 11) with the streetscape will be installed to provide infiltration of the first 15mm of rainfall. Similarly, the realignment and upgrade of the distributor roads (Resolution Drive, Grandstand Road and Stoneham Street) to four lane roads with a central median will require retention and infiltration of the first 15mm. This will be achieved through the installation of bio-retention swales within the median.

Raingardens, swales and tree-pits will be connected to the downstream environment via subsoil drainage discharging to the road drainage system to prevent local groundwater levels from rising and becoming a nuisance to the future community.

Within the Belmont Trust land, the retention and treatment of the first 15mm of rainfall will occur outside of the Swan River Trust Development Control Area. Vegetated buffer zones/verges should be installed in POS between waterways and turf area to help prevent herbicides, fertilisers and grass clippings entering waterways where practical.

Typical volumes required for retention and infiltration of the first 15mm of rainfall within lots and road reserves are provided in Table 3.

Table 3: Typical on-site retention volumes

Area type	System type (assumed)	Volume to be provided
Road reserve	Raingardens or tree-pits	0.9 m³ per 100 m²
Single residential lot	Soakwells	0.5 m <sup>3</sup> per 100 m <sup>2</sup>
Multi-residential lots	Underground infiltration system	9.4 m³ per 1000 m² lot
Apartment building	Underground infiltration system	9.5 m³ per 1000 m² lot
Mixed use building	Underground infiltration system	9.5 m³ per 1000 m² lot

#### 5.3.2 Minor event management

Stormwater in excess of the capacity of on-site retention systems will be conveyed through the existing drainage system consisting of local road drainage, Central Belmont Main Drain Basin and compensating basin. Owing to the current commercial land use within the study area, redevelopment to mixed Commercial/Residential towers will have minimal impact on the impervious areas and runoff characteristics within the catchment. Therefore the existing system is considered sufficient for future land uses.

The significant modification to the system will be the conversion of the open drain between Resolution Drive and Stoneham Street, as recommended in the Water Corporation (2009) review of the system. It was recommended that this drain is replaced with a 1500mm pipe for safety reasons. Replacing the drain with a pipe will also allow for realignment of the system consistent with other services and future POS alignments.

Any changes to the Water Corporation drainage system will need to be undertaken in consultation with the Water Corporation and will require further detailed design, justification and agreement. This includes consideration of the modifications outlined above to ensure that



the capacity of the main drain is sufficient to meet the conditions of the Water Corporation's operating license.

Downstream of Stoneham Road, within the Swan River Trust Development Control Area, the existing compensation basin and drain that discharges to the Swan River will not be modified.

Potential raingarden and median swale locations are provided in Figure 12 and an example of a median swale or biofilter is provided in Figure 13.

### 5.3.3 Floodplain (major event) management

Major flood runoff (1% AEP) will be conveyed via overland flow within the road reserve to the compensating basin and drain prior to discharging to the Swan River.

To ensure there is adequate protection from major flood events in the Swan River, the habitable flood level of any buildings within the study area will be a minimum of 500mm above the 1% AEP (100yr ARI) flood levels as shown in Figure 9. This is most relevant for the northern portions of the study area that are located within the Swan River flood fringe. The nearest 1% AEP flood level for the study area, located at the outlet of the Central Belmont Main Drain system is 3.44 m AHD and therefore the minimum habitable floor level throughout the study area would be 3.94 m AHD.

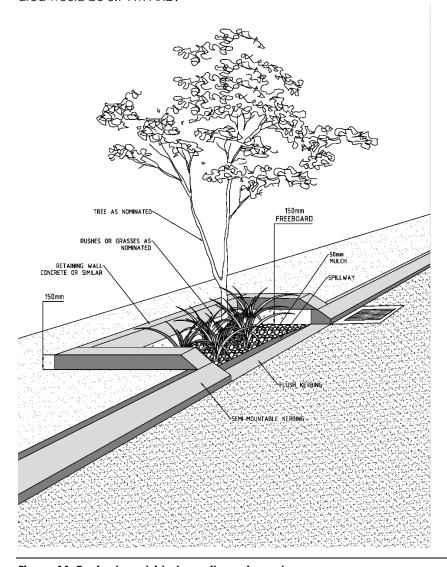
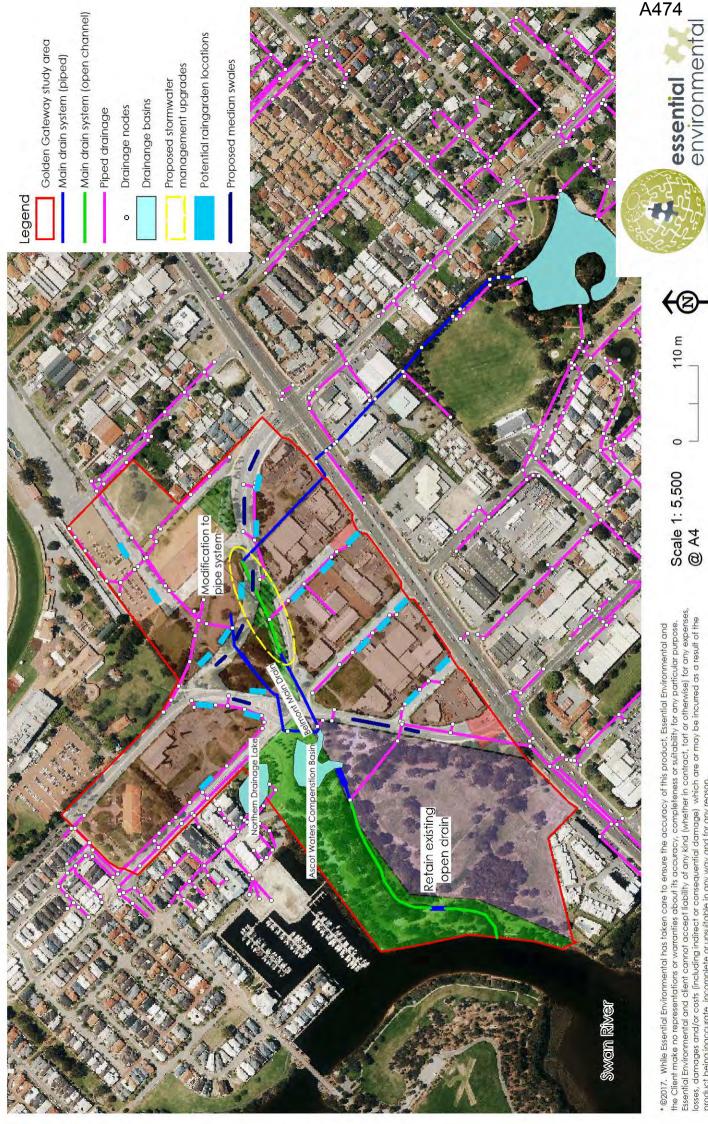


Figure 11: Typical roadside tree pit or raingarden



City of Belmont: Golden Gateway - Local Water Management Strategy Figure 12: Stormwater management system



Scale 1: 5,500 @ A4

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product being inaccurate, incomplete or unsuitable in any way and for any reason. Data source: WC, DEC, Landgate. Created by: AI. Projection: MGA: zone 50.

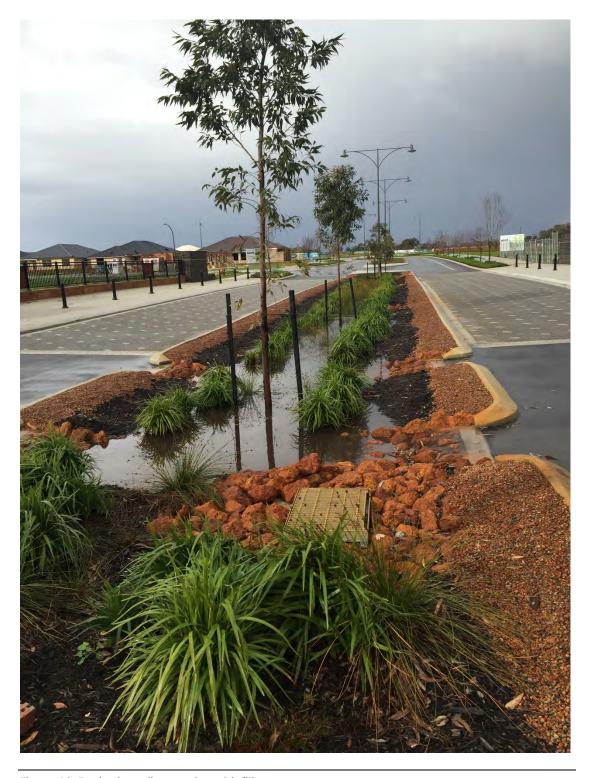


Figure 13: Typical median swale or biofilter



# 5.4 Groundwater management

The key objectives for groundwater management are:

- Protecting infrastructure and assets from flooding and inundation by high seasonal groundwater levels, perching and/or soil moisture;
- Protecting groundwater dependent ecosystems from the impacts of urban runoff; and,
- Managing and minimising changes in groundwater levels and groundwater quality following redevelopment.

The following planning measures are adopted to achieve the above objectives:

- Retain existing surface levels as a minimum to ensure adequate separation;
- Limit basements in areas of shallow groundwater; and,
- Use of subsoil drainage below bio-retention areas, raingardens and tree pits to minimise local groundwater rise.

Groundwater levels throughout the study area have been derived from regional contouring information and snapshot measurements. The results indicated that there is potential clearance for basements to be installed, with two storey basements possible closer to Great Eastern Highway. Detailed designs of any infrastructure below the existing surface level (such as basements) will require further onsite monitoring to determine local groundwater levels and separation. Where levels are shallow, design will need to account for these conditions and may include tanking or other forms of damp-proofing.

Any temporary lowering groundwater for construction, either for basements or sewer, may require dewatering licences from the Department of Water and Environmental Regulation (discussed further in Section 6.3.1) and ASS Management Plans.

# 5.5 Water quality management

Site specific targets have been proposed for estuarine catchments of the Swan Coastal Plain as a part of background work undertaken by the Department of Water and Environmental Regulation during the development of the UNDO water quality modelling tool. The targets were developed based on consideration of the sensitivity of the receiving water body and the proximity of the development site. Applying this approach, the study area would lie within the suggested 'proximity zones' of the estuarine portion (<1000 m) owing to the proximity to the Swan River. This would indicate that the following targets could be applied:

- 2.7 kg/ha/yr of Nitrogen
- 0.15 kg/ha/yr of Phosphorous

UNDO (Urban Nutrient Decision Outcomes) is a simple empirical decision support model with a flexible framework that evaluates nutrient reduction decisions for new urban developments on the Swan Coastal Plain. It has been developed by the Department of Water and Environmental Regulation to provide urban development proponents with an easy to use tool for assessment by local and state government authorities.

An UNDO model has been developed for the site with two sub-regions identified (1: Area outside of the Swan River Trust Development Control Area and 2: within the Swan River Trust Development Control Area). These sub-regions have been defined as recommended by the type of drainage strategy proposed for each. The model was run for existing conditions and the proposed redevelopment scenarios. For both scenarios, the area outside of the Swan River



Trust Development Control Area is assumed to be served by piped drainage, while the smaller area within the Control Area features open drains.

Results of UNDO modelling, in the form of a report which is generated by the software containing details of all assumptions and inputs, are provided in Appendix 3 (Proposed Redevelopment) and Appendix 4 (Existing Conditions).

The outcomes of UNDO modelling indicate that on-site retention of the first 15 mm of rainfall within soakwells and bio-retention systems on lots and raingardens in streets will provide discharge loads of:

- 0.39 kg/year/hectare of total nitrogen
- 0.01 kg/year/hectare of total phosphorous

These loads are well within the recommended targets for developments within estuarine catchments of the Swan Coastal Plain that have been developed for a discussion paper as a part of supporting information for the UNDO modelling tool by the Department of Water and Environmental Regulation.

For comparison, the existing conditions scenario returned discharge loads of:

- 0.80 kg/year/hectare of total nitrogen
- 0.10 kg/year/hectare of total phosphorous

The existing loads are within targets, but they demonstrate that improvement that can be achieved through redevelopment of the study area and implementation of water sensitive urban design.

### 5.6 Management of disease vectors and nuisance insects

The presence of permanent or seasonal water bodies close to residential areas provides the potential for Mosquitos and Chironomid Midges to become a nuisance to the resident population. This strategy does not propose to construct any new permanent or semi-permanent water bodies. However, the construction of above ground water quality treatment systems is proposed. These systems will need to be designed to minimise detention times throughout the year.

Additionally, there are constructed and natural water courses and water bodies in surrounding land where there is potential for mosquitoes and midges to breed. This strategy cannot influence the design or management of water systems in surrounding land areas but is required to recognise their presence and propose strategies that can assist with managing their potential impacts on the developing land and future community.

Physical, chemical and biological control methods can be used to manage mosquito populations. Methods which are likely to be employed (and their order of priority) include:

- Engineering and Landscape design and construction will aim not to create any new permanent or semi-permanent water bodies;
- Improved water quality, minimising nutrient loads and thereby reducing potential for algal blooms and fish kills; and,
- Should Mosquitos and Chironomid Midges become a nuisance, pesticides (larvicides and/or adulticides) will be used as required to kill mosquito larvae in breeding sites.



#### **6 IMPLEMENTATION**

The success of the water management strategies outlined in this document is dependent on their implementation through further planning, detailed design, construction and maintenance.

# **6.1 Urban Water Management Plans**

Urban Water Management Plans (UWMPs) are the final water management documents within the state government planning framework outlined in Section 1.1. These documents are prepared as a condition of subdivision (in support of local development plans) to demonstrate that designs achieve the objectives, strategies and design criteria outlined in this LWMS. Where subdivision is proposed in an area that is not covered by a local development plan, the City of Belmont and/or the Department of Water and Environmental Regulation may request preparation of a UWMP if additional information is required to demonstrate compliance with this strategy.

Urban water management plans should be prepared in consultation with the City of Belmont and the Department of Water and Environmental Regulation and be based on local site investigations appropriate to the proposal and level of risk to water resources. The UWMPs should be consistent with the requirements of the Department of Water and Environmental Regulation's *Urban water management plans: Guidelines for preparing plans and for complying with subdivision conditions* (DoW, 2008b), recognising that the area is a redevelopment area rather than a greenfield site. Specifically, these documents should include detailed engineering and landscaping designs, design of bio-retention systems and measures to manage impacts from construction.

Where an urban water management plan is not requested by the City of Belmont and/or the Department of Water and Environmental Regulation, development should be undertaken consistent with the objectives, strategies and design criteria in this local water management strategy.

### 6.2 Pre-development monitoring

The requirement for any further groundwater monitoring is dependent on proposed building designs and any proposals for lowering of the existing surface. This is particularly relevant for any proposed basements. Onsite monitoring should be undertaken, preferably over winter months, to determine groundwater levels. These levels should then be correlated with long term Department of Water and Environmental Regulation groundwater data to calculate maximum groundwater levels and any design considerations for the site.

Results of this groundwater monitoring should be presented in the corresponding urban water management plan for assessment by the City of Belmont and Department of Water and Environmental Regulation.

#### 6.3 Construction

Construction activities have the potential to directly and indirectly impact local water resources and water management measures are required.



#### 6.3.1 Licencing

Water will be required for construction activities such as dust suppression. Although the superficial aquifer is fully allocated, as discussed in Section 4.4.2, temporary groundwater abstraction licences should be sought for construction activities to reduce the demand on potable resources.

Where dewatering will be undertaken for construction, for example with basements and sewer installation, a dewatering licence should be submitted to the Department of Water and Environmental Regulation. Furthermore, where there is a risk of ASS, the licence application should be supported by an ASS and Dewater Management Plan to ensure risks are mitigated.

The Department of Water and Environmental Regulation issues licences to take water from water sources under section 5C of the Rights in Water and Irrigation Act 1914. Such licences may be granted for dewatering activities for construction purposes. The DWER does not issue licences or approvals for the disposal of dewatering effluent. It is the licensee's responsibility to ensure that it obtains all necessary approvals for the disposal of dewatering effluent from relevant authorities and that the dewatering effluent does not cause injury or damage to any persons or property. Relevant approval authorities include the City of Belmont and the Department of Biodiversity, Conservation and Attractions.

#### 6.3.2 Construction Management

To ensure downstream waterways are protected, developers, builders and landscapers must implement best management practices to control erosion and sedimentation. Contractors and staff should be notified of specific construction management requirements including appropriate disposal of waste material, erosion control and dust suppression.

## 6.4 Roles and Responsibilities

Key tasks, roles and responsibilities relating to delivery of urban water management objectives are outlined in Table 4.

All development is to be in accordance with the objectives, strategies and design criteria in this Local Water Management Strategy. Additional design criteria may need to be met, particularly where they are specific to particular precincts. These will be outlined in the relevant Local development plan, urban water management plan or associated development guideline.

Engineering and building drawings submitted to Council for development approval are to be supported by clear and auditable documentation, providing details outlining the water management requirements including any proposed staging, and demonstrating compliance with design criteria.

Where required, the City of Belmont may seek the advice of the Department of Water and Environmental Regulation regarding water management measures outlined in any development application.



Table 4: Summary of roles and responsibilities

Planning action	Water planning requirement	Timing and responsibility	Additional comments
Golden Gateway Structure Plan	Supported by LWMS which includes objectives, strategies and criteria to be met as part of planning and development	LWMS accompanies the structure plan City of Belmont	LWMS may be revised as detailed planning progresses or information comes to light
Local Development Plan (LDP)	Supported by an urban water management plan (UWMP) which demonstrates how the proposed development meets the objectives, strategies and criteria in the LWMS.	Required prior to any subdivision or development occurring Proponent	Where development is proposed to include amendments to Water Corporation drainage infrastructure the UWMP will need to include detailed designs for the modified drainage infrastructure agreed by the Water Corporation.  Any proposed excavation will need to be supported by
			groundwater level monitoring and appropriate licence applications.
Subdivision	Must meet the requirements of the relevant UWMP (and LWMS)	Should not occur until LDP and UWMP completed Proponent	Unlikely that a UWMP will be required due to the small scale nature of likely subdivision
Development	Must meet the requirements of the relevant UWMP (and LWMS)	Should not occur until LDP and LWMS completed Proponent	Should be consistent with relevant design guidelines



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# **APPENDIX 1 – LWMS CHECKLIST**

Local water management strategy Item	Deliverable	$oldsymbol{\square}$	Comments
Executive summary			
Summary of the development design strategy,	Table 1: Design	V	
outlining how the design objectives are proposed	elements &		
to be met	requirements for BMPs		
	and critical control		
	points		
Introduction			
Total water cycle management – principles &			
objectives			
Planning background			
Previous studies			
Proposed development	Cita a substitution	L7I	
Structure plan, zoning and land use.	Site context plan	<u> </u>	
Key landscape features Previous land use	Structure plan		
Landscape – proposed POS areas, POS credits,	Landscape Plan	V	
water source, bore(s), lake details (if applicable)	Lanascape Han		
Design criteria			
Agreed design objectives		V	
Pre-development environment		_	
Existing information and more detailed assessments		V	
(monitoring). How do the site characteristics affect			
the design?			
Site Conditions – existing topography/ contours,	Site condition plan	Ø	
aerial photo underlay, major physical features	· ·		
Geotechnical – topography, soils including acid	Geotechnical plan	Ø	
sulphate soils and infiltration capacity, test pit			
locations			
Environmental – areas of significant vegetation,	Environmental Plan	☑	
wetlands and buffers, waterways and buffers,	plus supporting data		
contaminated sites	where appropriate		
Surface Water – topography, 100 year floodways	Surface Water Plan	☑	
and flood fringe areas, water quality of flows			
entering and leaving (if applicable)  Groundwater – topography, pre development	Groundwater Plan	V	
groundwater levels and water quality, test bore	plus details of		
locations	groundwater		
i codinorio	monitoring and testing		
Water sustainability initiatives			
Water supply & efficiency measures – private and		V	
public open spaces			
Fit-for-purpose strategy and agreed actions. If non-		V	
potable supply, support with water balance			
Wastewater management			
Stormwater management strategy			
Flood protection – peak flow rates, volumes and	major event Plan	Ø	
top water levels at control points, 100 year flow	Long section of critical	☑	
paths and 100 year detentions storage areas	points		
Manage serviceability – storage and retention	minor event Plan	$\square$	
required for the critical 5 year ARI storm events			
Minor roads should be passable in the 5 year ARI			
event			<u>I</u>

Local water management strategy Item	Deliverable	$oldsymbol{ abla}$	Comments
Protect ecology – detention areas for the 1 yr 1 hr ARI event, areas for water quality treatment and types of (including indicative locations for) agreed structural and non-structural best management practices and treatment trains. Protection of waterways, wetlands (and their buffers), remnant vegetation and ecological linkages	small event Plan Typical cross sections	<u>n</u>	
Groundwater management strategy			
Post development groundwater levels and fill requirements (including existing and likely final surface levels), outlet controls, and any subsoils	Groundwater/subsoil Plan	Ĭ I	
Actions to address acid sulfate soils or contamination		V	
The next stage – subdivision and urban water management plans			
Content and coverage of future urban water management plans to be completed at subdivision. Include areas where further investigations are required prior to detailed design.		Ø	
Monitoring			
Recommended future monitoring plan including timing, frequency, locations and parameters, together with arrangements for ongoing actions		V	
Implementation			
Developer commitments		V	
Roles, responsibilities, funding for implementation		Ø	
Review		V	

# **APPENDIX 2 - WATER BALANCE MODELLING**





# Site Water Balance City of Belmont: Golden Gateway Sheet 1: Water Demands and Waste Generation

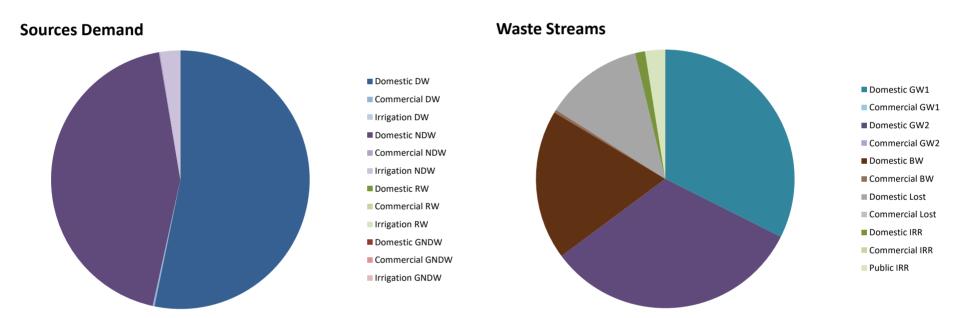
Domestic Uses																			
Domestic Uses  Dwelling Type		Lifestyl	e		Traditio	nal		Cottage			Terrace			Apart	ment		Other		
No. households		0			0			0			46			3412			0		
Population / household		2.736			2.736			1.814			2.4			1.8			1.552		
Occupancy %		100			100			100			100			100			100		
Effective Population	0	0			0			0			110.4			6141.	6		0		
			•															_	
Use Domestic Individual Usage	Base Rate	Rate	Source	Waste	Rate	Source	Waste	Rate	Source	Waste	Rate	Source	waste	Rate	Source	Waste	Rate	source	Waste
<u>Domestic Individual Usage</u> Shower	kL/pp/day 0.05	0.05	DW	GW1	0.05	DW	GW1	0.05	DW	GW1	0.05	DW	GW1	0.05	DW	GW1	0.05	DW	GW1
Kitchen sink	0.008	0.008	DW	GW1	0.008	DW	GW1	0.008	DW	GW1	0.008	DW	GW1	0.008	DW	GW1	0.008	DW	GW1
Bathroom basin	0.006	0.006	DW	GW1	0.006	DW	GW1	0.006	DW	GW1	0.006	DW	GW1	0.006	DW	GW1	0.006	DW	GW1
Dishwasher	0.003	0.003	DW	GW2	0.003	DW	GW2	0.003	DW	GW2	0.003	DW	GW2	0.003	DW	GW2	0.003	DW	GW2
Bath	0.001	0.001	DW	GW1	0.001	DW	GW1	0.001	DW	GW1	0.001	DW	GW1	0.001	DW	GW1	0.001	DW	GW1
Laundry trough	0.004	0.004	DW	GW2	0.004	DW	GW2	0.004	DW	GW2	0.004	DW	GW2	0.004	DW	GW2	0.004	DW	GW2
Toilet	0.033	0.033	NDW	BW	0.033	RW	BW	0.033	RW	BW	0.033	NDW	BW	0.033	NDW	BW	0.033	DW	BW
Washing machine	0.042	0.042	NDW	GW2	0.042	RW	GW2	0.042	RW	GW2	0.042	NDW	GW2	0.042	NDW	GW2	0.042	NDW	GW2
Household Usage	kL/househol	d/day																	
Leaks	0.029	0.029	DW	N/A	0.029	DW	N/A	0.029	DW	N/A	0.029	DW	N/A	0.029	DW	N/A	0.029	DW	N/A
Car washing	0.002	0.002	DW	N/A	0.002	DW	N/A	0.002	DW	N/A	0.002	DW	N/A	0	DW	N/A	0.002	DW	N/A
Evaporative cooling	0.006	0.006	DW	N/A	0.006	DW	N/A	0.006	DW	N/A	0.006	DW	N/A	0.006	DW	N/A	0.006	DW	N/A
Other	0.004	0.004	DW	N/A	0.004	DW	N/A	0.004	DW	N/A	0.004	DW	N/A	0.004	DW	N/A	0.004	DW	N/A
Other	kL/each/yea	l r																	
Household / Communal Pools	90	0	DW	N/A	0	DW	N/A	0	DW	N/A	0	DW	N/A	0	DW	N/A	0	DW	N/A
<u>Domestic Irrigation</u>														40					
Number Lots	2	0			0			0			46			12	c		0		
Average Lot Area Irrigated area	m2 %	1500 12			386 25			245 25			393.2 25			6228. 5	U		100 50		
Irrigation event depth	mm	10			10			10			10			5			10		
Frequency (days/week)	days	2			2			2			2			2			2		
Season length	months	9			9			9			9			9			9		
Source		GNDW			NDW			NDW			NDW			NDW			GNDW		
No. Irrigation Events	L1 A	79.0			79.0			79.0			79.0			79.0	2		79.0		
Irrigation Demand	kL/year	0.0			0.0			0.0			3572.2			1476.	<b>L</b>		0.0		
Domestic Waste Streams	kL/year																		
Greywater Type 1 (GW1)		0			0			0			2298			12786			0		
Greywater Type 2 (GW2)		0			0			0			2298			12786			0		
Black Water (BW)		0			0			0			1331			74026			0		
Lost (N/A) Irrigation (IRR)		0			0			0			689 3572			48603 1476	5		0		
iiiigatioii (ink)		J						U			3372			14/6			U		
Overall Domestic Demand																			
Drinking Water (DW)	kL/year	0			0			0			3592			21013			0		
Non-Drinking Water (NDW)		0			0			0			6596			1697	18		0		
Rain Water (RW)		0			0			0			0			0			0		
Groundwater (GND) Total Demand		0			0			0			0 10189			0 37983	13		0		
Total per capita demand	kL/pp/yr	#DIV/0	!		#DIV/0!			#DIV/0!			92			62	,5		#DIV/0!		
Per capita DW demand	·/ FF/ J'	#DIV/0			#DIV/0!			#DIV/0!			33			34			#DIV/0!		
Public Irrigation			200			200			C	N1				-			_ <del>_</del>		
Total Area	m2	Passive 49014	POS		Active F	205		Verge / 8458	Garden E	seds	School 0			Other 0			-		
Intal Area Irrigated area	m2 %	49014 25			75			25			50			25					
Irrigation event depth	mm	10			16			10			12			10					
Frequency (days/week)	days	2			2			2			2			2					
Season length	months	8			10			8			10			8					
Source		NDW			GNDW			NDW			GNDW			GND\	V				
No. Irrigation Events	kl ///	70.0			87.0			70.0			87.0			70.0					
Irrigation Demand	kL/year	8577.5			0.0			1480.2			0.0			0.0			I		
Commercial Uses																			
	Base rate	Qty	Rate		Source	Waste	Total		Comr	ment									
	kL/year		kL/ye	ear			kL/ye												
Shopping centre DW	0.6	1500	1		DW	BW		900		ssumptio									
shopping centre NDW	0.4	1500	0		NDW	BW		600	WC a	ssumptio	on								
							1												



# Site Water Balance City of Belmont: Golden Gateway Sheet 1: Water Demands and Waste Generation

Demand Based Water Balance (kL/year)					
	Domestic	Commercial	Public Irr'	Total	
	kL / year	kL / year	kL / year	kL / year	%
Total Water Use	390021	1500	10058	401579	100.0%
Source Demand					
Drinking Water (DW)	213707	900	0	214607	53.4%
Non-Drinking Water (NDW)	176314	600	10058	186972	46.6%
Rain Water (RW)	0	0	0	0	0.0%
Groundwater (GND)	0	0	0	0	0.0%
					100.0%
<u>Waste</u>					
Greywater Type 1 (GW1)	130162	0		130162	32.4%
Greywater Type 2 (GW2)	130162	0		130162	32.4%
Black Water (BW)	75357	1500		76857	19.1%
Lost (N/A)	49292	0		49292	12.3%
Irrigation (IRR)	5048	0	10058	15106	3.8%
					100.0%

Performance		
Population	6252	people
POS Area	5	ha
Total Public Space Area	6	ha
Per Capita Domestic Total Water	62	kL/person/year
Per Capita Domestic Drinking Water	34	kL/person/year
POS Irrigation Rate	0.18	kL/m2/year
Total Public Space Irrigation Rate	0.18	kL/m2/year



# APPENDIX 3 – UNDO MODELLING (PROPOSED REDEVELOPMENT)





Project: Golden Gateway Date: 02-Mar-17

Version: Version 1.1.0.16333

			Inpu	ıt load	Total area (ha)	Total percent (%
Landuse	Percent (%)	Area (ha)	Nitrogen (kg)	Phosphorus (kg)		
Residential	74	20.13	29.18	2.05	27.20	87
Industrial, commercial & schools	0	0.00	0.00	0.00	Nitrogen input (kg/ha/yr)	Phosphorus input (kg/ha/yr)
Rural living	0	0.00	0.00	0.00		
Public open space	3	0.82	0.00	0.00	6.30	0.23
Road reserve	23	6.26	0.00	0.00		
					Nitrogen export (kg/ha/yr)	Phosphorus (kg/ha/yr)
					0.41	0.01

			Inpu	rt load		
Size	Percent	Area	Nitrogen	Phosphorus	Description of the last of the	The same of the
(m²)	(%)	(ha)	(kg)	(kg)	Total area (ha)	Total percent (%
<400	0	0.00	0.00	0.00	20.125188	74
400-500 m²	0	0.00	0.00	0.00	201122300	
501-600 m²	0	0.00	0.00	0.00	Nitrogen input (kg)	Phosphorus inpu (kg)
601-730 m <sup>2</sup>	0	0.00	0.00	0.00		
>730 m²	0	0.00	0.00	0.00	29.18	2.05
Iultiple dwellings	100	20,13	29,18	2.05		

#### Public Open Space (POS) Landuse Percent Area (%) (ha) Native gardens 0 0.00 Total area (ha) Total percent (%) Non-native gardens 0.00 Not fertilised 100 0.82 0.82 3 0 Nature 0.00 0.00 Sport 0 Nitrogen input Phosphorus input (kg) (kg) 0.00 Recreation 0 Golf course 0 0.00 0.00 0.00 Bowling green 0 0.00 Impervious 0 0.00 Water body 0 0.00

### Road reserve

Landuse	Percent	Area		
	(%)	(ha)	Total area (ha)	Total percent (%)
Roads	80	5.00	3,000,000	100
Road reserve - impervious	0	0.00	6.255126	23
Road reserve - native garden	0	0.00	Nitrogen input	Phosphorus input
Road reserve - non-native garden	0	0.00	(kg)	(kg)
Road reserve - turf	0	0.00	0.00	0.00
Road reserve - not fertilised	20	1.25		

Time of destroys				
Type of drainage	Piped drainage	Does it contain importe	d fill? Yes	
Soil type	Bassendean	Type of fill imported	Yellow sand (Spearwood)	
Depth to groundwater (m)	2	Fill depth (m)	0.3	
Groundwater slope (%)	1	Approximate PRI of impor	ted fill 11	
Soil PRI	5.9	Does subregion contain	onsite sewage diposal system?	1

Note: Please attach the results of soil tests to this report when submitting.

#### Subregion name: SRT DC Area Input load Total percent (%) Total area (ha) Landuse Percent Area Nitrogen Phosphorus (%) (ha) (kg) (kg) 4.06 13 Residential 0 0.00 0.00 0.00 Industrial, commercial & schools 0.00 0 0.00 0.00 Nitrogen input Phosphorus input (kg/ha/yr) (kg/ha/yr) Rural living 0 0.00 0.00 0.00 5.23 0.15 Public open space 100 4.06 0.00 0.00 Road reserve 0.00 0 0.00 0.00 Nitrogen export Phosphorus (kg/ha/yr) (kg/ha/yr) 0.01 0.32

Public Open Space (P	os)			
Landuse	Percent	Area		
	(%)	(ha)		
Native gardens	0	0.00		
Non-native gardens	0	0.00	Total area (ha)	Total percent (%
Not fertilised	100	4.06	4.06	100
Nature	0	0.00		
Sport	0	0.00	Nitrogen input	Phosphorus inpu
Recreation	0	0.00	(kg)	(kg)
Golf course	0	0.00	0.00	0.00
Bowling green	0	0.00		
mpervious	0	0.00		
Water body	0	0.00		

# Soil and drainage information

Type of drainage
Open channel drains
Does it contain imported fill? No
Soil type
Bassendean
Does subregion contain onsite sewage diposal system?

Depth to groundwater (m) 1

Groundwater slope (%)

Soil PRI 5.0

Note: Please attach the results of soil tests to this report when submitting.

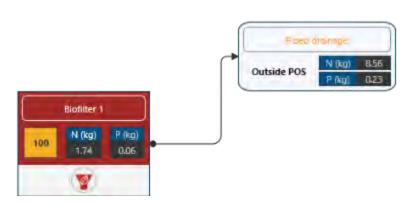
Treatment	Name	Size	Treated area	Treating	N removed	P removed
		(m²)	(ha)		(kg/ha/yr)	(kg/ha/yr)
Biofilter	Biofilter 1	16.00	0.27	Sandy soils – Runoff only (infiltration on lots)	0.07	0.00
Load remove	d				0.06	0.00
Net export					0.33	0.01

Summary: Nutrient load exports							
Region	Area	P export	N export				
	(ha)	(kg/ha/yr)	(kg/ha/yr)				
Outside POS	27.20	0.01	0.41				
SRT DC Area	4.06	0.01	0.32				

PRE-TREATMENT LOAD (kg/ha/yr)		LOAD REMOVED (kg/ha/yr)		NET LOAD EXPORT (kg/ha/yr)	
PHOSPHORUS	NITROGEN	PHOSPHORUS	NITROGEN	PHOSPHORUS	
0.01	0.06	0.00	0.33	0.01	
	PHOSPHORUS	PHOSPHORUS NITROGEN	PHOSPHORUS NITROGEN PHOSPHORUS	PHOSPHORUS NITROGEN PHOSPHORUS NITROGEN	

# Treatment diagram





# APPENDIX 4 – UNDO MODELLING (EXISTING CONDITIONS)







Project: Golden Gateway Existing Revised Date: 02-Mar-17

Version: Version 1.1.0.16333

		Input load			Total area (ha)	Total percent (%)
Landuse	Percent (%)	Area (ha)	Nitrogen (kg)	Phosphorus (kg)	Total area (na)	Total percent (78
Residential	0	0.00	0.00	0.00	26.97	87
Industrial, commercial & schools	35	9.44	226.36	53.81	Nitrogen input (kg/ha/yr)	Phosphorus input (kg/ha/yr)
Rural living	0	0.00	0.00	0.00	(kg/na/yt/	(kg/na/yt/
Public open space	45	12.14	0.00	0.00	13.62	2.15
Road reserve	20	5.39	0.00	0.00	Nitrogen export (kg/ha/yr)	Phosphorus (kg/ha/yr)

Landuse	Percent	Area	Total area (ha)	Total percent (%
	(%)	(ha)		
Light industrial	20	1.89	9.44	35
Heavy industrial	0	0.00		
Commercial / Offices	80	7.55	Nitrogen input (kg)	Phosphorus input (kg)
Schools	0	0.00	and he	Les of
Public buildings	0	0.00	226.36	53.81

#### Public Open Space (POS) Landuse Percent Area (%) (ha) Native gardens 0 0.00 Total area (ha) Total percent (%) Non-native gardens 0 0.00 Not fertilised 100 12,14 12.14 45 0 Nature 0.00 Sport 0 0.00 Nitrogen input Phosphorus input (kg) (kg) Recreation 0 0.00 Golf course 0 0.00 0.00 0.00 Bowling green 0 0.00 Impervious 0 0.00 Water body 0 0.00

### Road reserve

Landuse	Percent	Area		
	(%)	(ha)	Total area (ha)	Total percent (%)
Roads	80	4.32		
Road reserve - impervious	0	0.00	5.394	20
Road reserve - native garden	0	0.00	Nitrogen input	Phosphorus input
Road reserve - non-native garden	0	0.00	(kg)	(kg)
Road reserve - turf	0	0.00	0.00	0.00
Road reserve - not fertilised	20	1.08		

# Soil and drainage information

Type of drainage	Piped drainage	Does it contain imported fill? No	
Soil type	Bassendean	Does subregion contain onsite sewage diposal system?	No
Depth to groundwater (m)	2		
Groundwater slope (%)	1		
Soil PRI	5.0		

Note: Please attach the results of soil tests to this report when submitting.

#### Subregion name: SRT DC Area Input load Total area (ha) Total percent (%) Landuse Percent Area Nitrogen Phosphorus (%) (ha) (kg) (kg) 4.03 13 Residential 0.00 0 0.00 0.00 Industrial, commercial & schools 0.00 0 0.00 0.00 Nitrogen input Phosphorus input (kg/ha/yr) (kg/ha/yr) Rural living 0 0.00 0.00 0.00 5.23 0.15 Public open space 100 4.03 0.00 0.00 Road reserve 0.00 0 0.00 0.00 Nitrogen export Phosphorus (kg/ha/yr) (kg/ha/yr) 0.01 0.32

Public Open Space (P	os)			
Landuse	Percent	Area		
	(%)	(ha)		
Native gardens	0	0.00		
Non-native gardens	0	0.00	Total area (ha)	Total percent (%
Not fertilised	100	4.03	4.03	100
Nature	0	0.00		
Sport	0	0.00	Nitrogen input	Phosphorus input
Recreation	0	0.00	(kg)	(kg)
Golf course	0	0.00	0.00	0.00
Bowling green	0	0.00		
Impervious	0	0.00		
Water body	0	0.00		

# Soil and drainage information

Type of drainage Open channel drains Does it contain imported fill? No

Soil type Bassendean Does subregion contain onsite sewage diposal system?

Depth to groundwater (m) 1

Groundwater slope (%)

Soil PRI 5.0

Note: Please attach the results of soil tests to this report when submitting.

Treatment Na	Name	Size	Treated area Treating	N removed	P removed
		(m²)	(ha)	(kg/ha/yr)	(kg/ha/yr
Load removed	1			0.00	0.00
Load removed Net export	1			0.00	0.00

Summary: Nutrient load exports							
Region	Area	P export	N export				
	(ha)	(kg/ha/yr)	(kg/ha/yr)				
Outside POS	26.97	0.11	0.88				
SRT DC Area	4.03	0.01	0.32				

PRE-TREATMENT LOAD (kg/ha/yr)		(kg/ha/yr)	NET LOAD EXPORT (kg/ha/yr)		
PHOSPHORUS	NITROGEN	PHOSPHORUS	NITROGEN	PHOSPHORUS	
0.10	0.00	0.00	0.80	0.10	
	PHOSPHORUS	PHOSPHORUS NITROGEN	PHOSPHORUS NITROGEN PHOSPHORUS	PHOSPHORUS NITROGEN PHOSPHORUS NITROGEN	



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# Ordinary Council Meeting 26/02/19

Item 12.6 refers

**Attachment 21** 

Infrastructure Assessment Report



# Infrastructure Assessment Report

Golden GatewayPrecinct

Prepared for City of Belmont

5 May 2017







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# **Executive Summary**

The Golden Gateway Structure Plan provides a framework for the redevelopment of the Golden Gateway Precinct area into a "major growth area by 2031".

Through close liaison with the relevant service providers, Cardno has researched and reported on the current capacity of the infrastructure and services within the Golden Gateway area. Cardno has also provided detailed findings and recommendations regarding the future infrastructure and servicing requirements that are needed to accommodate the redevelopment of the area as proposed by the Structure Plan.

In summary, Cardno's assessment of the Golden Gateway Precinct in terms of required infrastructure for the Golden Gateway Structure Plan area is as follows:

- The Golden Gateway Precinct area faces a shortage in wastewater infrastructure to service the proposed increase in residential and commercial activity.
- There is currently capacity in the existing HV feeders to supply the proposed development with power. However, Western Power advise power capacity cannot be reserved, and that subject to other developments in the area, a new HV feeder may be required to fully support the development.
- Upgrades other than the required major infrastructure upgrades as outlined in this report infrastructure will be rolled out in response to new development within the subject area.
- It is recommended that a working group between the City of Belmont and Water Corporation is set up in order to help plan and coordinate precinct development and staging with any Water Corporation trunk infrastructure capital works.
- National Broadband Network (NBN) Co. has not yet rolled their infrastructure across the Golden Gateway Precinct. It is recommended that the City of Belmont liaise with NBN Co. as per the Best practice guide for Councils when initially dealing with NBN Co document.

In conclusion, based on advice received by Cardno from the relevant service authorities, there should be no reason from a servicing point of view that the Golden Gateway Precinct Structure Plan could not be implemented with the proposed infrastructure upgrades outlined in this report.



# **Table of Contents**

1	Background				
	1.1	Introduction			
	1.2 Location				
2	Water			3	
	2.1	Existing Infrastructure	3		
	2.2 Required Infrastructure				
	Wastewater			5	
	3.1	Existing Infrastructure	5 5		
	3.2	Future Demand			
	3.3	3.2.2 Service Capacity  Required Infrastructure	6		
		0	7		
	Power Supply				
	<ul><li>4.1 Existing Infrastructure</li><li>4.2 Required Infrastructure</li></ul>				
	·				
	<b>Gas</b> 5.1	Existing Infrastructure	10	10	
	5.2	Required Infrastructure	10		
		nunications		11	
	6.1	Existing Infrastructure	11	•••	
	6.2	Required Infrastructure	11		
		6.2.1 Telstra	11		
		6.2.2 NBN	12		
Δnn	endi	CAS			
APP	Ci iui	003			
Appe	ndix A	Existing Infrastructure			
Appe	ndix B	Western Power Feasibility Study			
Toh	loc				
Tab	162				
Table	3-1 L	Local Scheme Zone Sewer Demand		6	
Table 3-2 Pump Station Service Capacity		Pump Station Service Capacity		6	
Table 4-1 Estimated Maximum Power Demand				8	
<b>—:</b>					
Figu	ıres				
Figure	:1-1 (	Golden Gateway Subject Area		2	
Figure		Existing Sewer Infrastructure		5	
Figure		Existing Power Network Capacity		7	
Figure		Forecast Power Network Capacity 2036		8	
Figure		Proposed Western Power Feeder Route		9	
Figure		Felstra Mobile Network Coverage		11	
9 4 1 0	- '	. s.s.sos.s . totto ago			



# 1 Background

#### 1.1 Introduction

Cardno was engaged to assist the City of Belmont, in conjunction with the Department of Planning to undertake an infrastructure and services strategy for the Golden Gateway precinct, Ascot. The strategy will help guide the preparation of a Local Structure Plan over the area.

The scope of works includes:

- · Review of planned growth area;
- · Provide analysis of existing services infrastructure, including;
  - Water:
  - Sewer;
  - Power;
  - Gas: and
  - Telecommunications
- Identification of future service demands;
- · Liaison and engagement of services providers; and
- Development of reports.

Cardno assessed the infrastructure to inform the City on decisions around the long-term provision of electrical energy, natural gas, potable water, wastewater disposal, along with high speed data /telecommunications for the growth areas.

The findings and advice presented in this report is based on Cardno's observations, experience from similar projects and responses from various service providers and stakeholders.

The investigations and preparation of this report have largely been based on preliminary advice from the various Service Authorities. The information is current as of April 2017 and is subject to change as development proceeds.

#### 1.2 Location

The subject area is located in Ascot, and is generally bounded by Great Eastern Highway, Stoneham Street, Grandstand Road and Resolution Drive. It includes the Belmont Trust Land, a portion of the Ascot Racecourse site as well as the Western Australian Turf Club headquarters and Ascot Kilns. The extent of the subject area is shown in **Figure 1-1**.



Figure 1-1 Golden Gateway Subject Area





### 2 Water

Water Corporation Western Australia is the state authority regulating the distribution infrastructure for water reticulation in the area.

### 2.1 Existing Infrastructure

The Serpentine Trunk Main runs along Grandstand Road and Daly Street. There is also a 915 steel distribution main running along Grandstand Road through the subject area. The existing lots are well serviced with a mixture of 100, 150 and 200 dia reticulation pipes made of asbestos cement, cast iron, PVC and steel.

Cardno Drawing *CW942300-CI-SK2* in **Appendix A** shows the location of the existing power infrastructure within and adjacent to the subject area.

### 2.2 Required Infrastructure

The Golden Gateway Precinct is located in the Supply Scheme area. It is difficult to ascertain exactly what capacity the current infrastructure network has without full water network modelling carried out by Water Corporation. However, Water Corporation does not foresee any issues with servicing the proposed scheme with potable water at the time of this report.

Exact water infrastructure upgrades will be determined when Water Corporation carries out full water network modelling. Water Corporation has advised that water reticulation planning and modelling will be done after Structure Plan and rezoning is confirmed, effectively at development application phase. The Water Corporation provided initial advice to Cardno and in their advice; they offered the following key points.

- Water Corporation will upgrade the headwork's, pipe equal to or greater than 300mm diameter and pump stations, as and when required. However, headwork's charges will be charged to the developer. Minor reticulation works, typically pipework less than 300mm diameter, are to be funded directly by the developer.
- All temporary works associated with any development within the Golden Gateway Precinct is to be funded directly by the developer.
- Redevelopment areas within the Golden Gateway Precinct need to integrate water efficiency technology and design approaches into the area and buildings in line with Water Corporation's 'Water Forever 2009" document. This will require a local water management strategy that includes local scale water balancing and identifying water efficiency measures such as; rainwater reuse, appropriate fittings, irrigation smart systems, planting and soil types and drainage collection and reuse.
- Water Corporation advises that a Development Area Plan be commissioned to support development in the Golden Gateway Precinct and submitted to Water Corporation once the Structure Plan has been finalised. This should include a plan identifying the proposed development, densities and likely staging and timeframe. Accompanying this should be a water management strategy outlining how water efficiencies are to be met along with engineering plans detailing proposed works and estimates. The water efficiency targets are to be determined by the City of Belmont in consultation with Water Corporation. Water Corporation runs a Waterwise Development Program that enables developments that have applied water efficient principles to be recognised and endorsed by Water Corporation.
- Water Corp recommends a consolidated approach to the requesting and programming of works to minimise disruptions and maximise cost efficiencies. Water Corporation recommends any reticulation reinforcement or new work should be managed by the City of Belmont due to the fractured land ownership within the area. It is recommended that a working group between the City of Belmont and Water Corporation is set up in order to help plan and coordinate precinct development and staging with any Water Corporation trunk infrastructure capital works.



Identification of required infrastructure upgrades requires detailed water modelling and more specific demand inputs. Water reticulation planning will be done after Structure Plan and rezoning is confirmed.



### 3 Wastewater

The Water Corporation (WC) of Western Australia is the main service provider regulating the distribution, storage and disposal infrastructure for wastewater in the Ascot area.

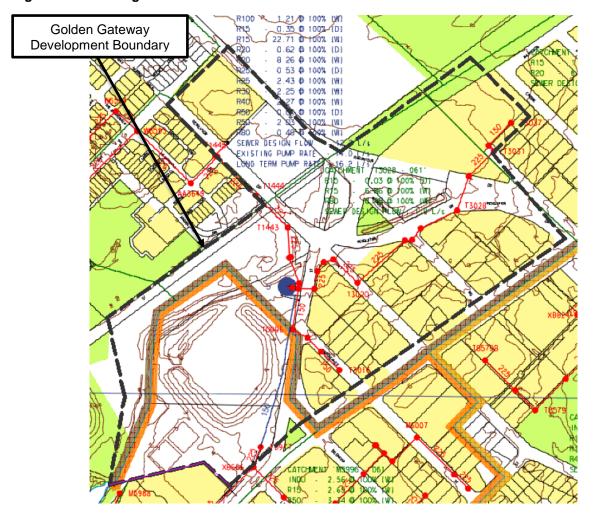
Wastewater infrastructure general to Ascot area is serviced by gravity style wastewater drainage infrastructure. A mixture of concrete and plastic arterial pipes on grade service all areas to local pump stations throughout the City of Belmont.

### 3.1 Existing Infrastructure

Lots within and surrounding the proposed Golden Gateway Structure plan are serviced by two main arterial sewer routes; a 225mm collector flowing north to south and a 225mm collector flowing east to west. Both collectors flow to the Redcliffe Pump Station 5 located on Stoneham Street. The Redcliffe P.S 5 collects all sewerage west of the Ascot raceway within the Ascot Suburb and discharges it to the Redcliffe Pump Station 2 located on Abernethy Road.

Refer to Cardno Drawing *CW942300-CI-SK1* in **Appendix A** for further details and drawings for the wastewater infrastructure in this area.

Figure 3-1 Existing Sewer Infrastructure



### 3.2 Future Demand

**Table 3-1** identifies the additional sewer demand estimated for the proposed Golden Gateway re-zoning structure plan.



Table 3-1 Local Scheme Zone Sewer Demand

Local Scheme Zones	Area (ha)	Additional Dwellings (No.)*	Additional Sewer Demand (L/s)**
Mixed-Use (R-AC0)	10.3	1648	9.15
Residential (R20)	0.88	18	0.19
Residential (R40)	1.73	70	0.61
Residential (R100)	1.57	157	1.34
Total		1893	11.29

<sup>\*</sup> Refer Table 4.4 of DS 50 for Design & Construction Requirements for Gravity Sewers DN150 to DN600

### 3.2.2 Service Capacity

Service Capacity has been analysed for Redcliffe P.S 5 and Redcliffe P.S 2 to determine if the stations have adequate capacity to service the proposed Golden Gateway development

Table 3-2 Pump Station Service Capacity

Pumping Station	Additional Flow (L/s)	P.S Existing Sewer Flow (L/s)	Long Term P.S. Capacity (L/s)	Future Capacity / [Shortfall] (L/s)
Redcliffe PS 5	11.29	14.0	16.2	[9.09 L/s]
Redcliffe PS 2	11.29	20.1	37.0	5.61 L/s

As per **Table 3-2** the proposed development will have significant impacts to the current wastewater infrastructure. It is not envisaged the existing Redcliffe PS5 will have sufficient capacity with a shortfall of 9.09 L/s to service the proposed development and will require a significant upgrade. Redcliffe PS 2 will likely have capacity however further planning should be co-ordinated with the Water Corporation to ascertain other timing of other developments in the area.

### 3.3 Required Infrastructure

As highlighted in the previous section the Redcliffe Pumping Station 5 has a shortfall of 9.09 litres per second in the case of the planned development. This will require the upgrade of the existing pumping station to a larger type 40. A type 40 pumping station is a station capable of a 40 L/s service consisting of two pump-sets located in a common wet-well constructed from 2500mm internal diameter precast concrete pipes.

Based on the above requirements Cardno engaged the Water Corporation to request further advice on the required upgrade, with no further feedback received at time of writing this report.

<sup>\*\*</sup>Capacity based on Water Corporation DS50 Table 4.1.

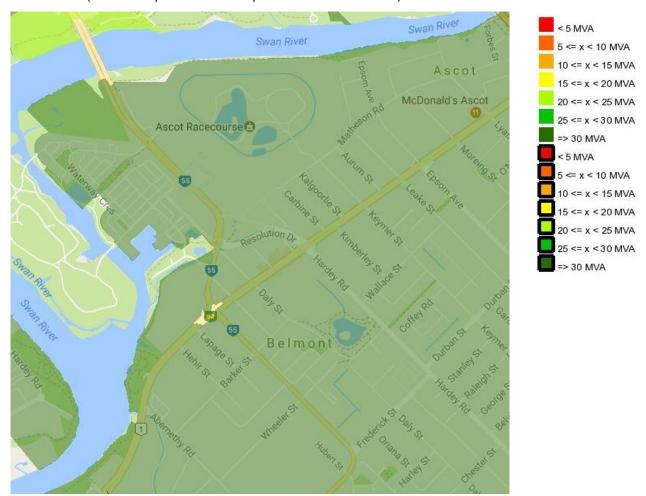


### 4 Power Supply

### 4.1 Existing Infrastructure

Power distribution and production is managed by Western Power. Data obtained from the Western Power *Network Mapping Tool* indicates that the area is serviced by the Belmont Substation and the forecast network capacity for 2015 is >30MVA, as shown in **Figure 4-1**. There are High and Low Voltage power lines in the vicinity of the site.

Figure 4-1 Existing Power Network Capacity
(Source: http://ncmt.westernpower.com.au/index.cfm)



Cardno Drawing *CW942300-CI-SK6* in **Appendix A** shows the location of the existing power infrastructure within and adjacent to the subject area.

### 4.2 Required Infrastructure

Maximum power requirement for the development has been calculated using Western Power's online Design Maximum Demand calculator. The estimated demand for the development is shown in **Table 4-1** below.

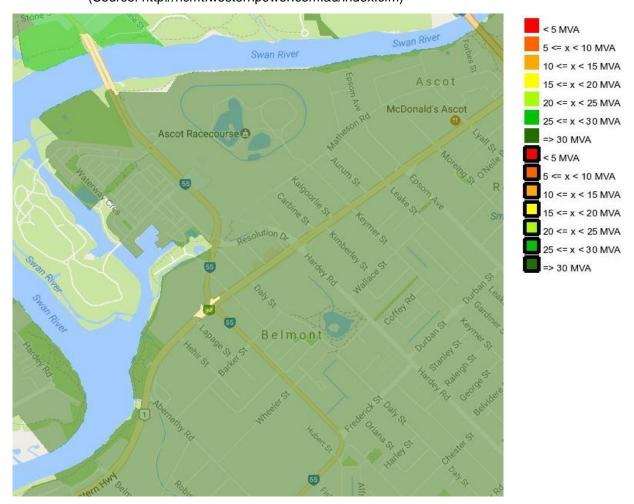


Table 4-1 Estimated Maximum Power Demand

Proposed Lot Use	Number of Units/Dwellings	Max. Demand/Unit (kVA)	Approx. Estimated Demand (kVA)
Single Dwelling Units	18	4.7	84.6
Grouped Residential (5-10 Units)	70	3.5	245
Grouped Residential (Over 10 Units)	1805	3.1	5,596
Mixed Use Commercial	1	2,400kVA	2,400
Total Development			8,325.6

Belmont substation falls under the Cannington load area. Western Power's *Annual Planning Report 2015/16* states "no substation capacity shortfall is forecast in the Cannington load area over the next five years." This takes into account committed and most likely to occur network expansion plans for the area. The Western Power *Network Mapping Tool* indicates that there is >30MVA spare capacity in the network until at least 2036 based on current and forecast demand (see **Figure 4-2**).

Figure 4-2 Forecast Power Network Capacity 2036 (Source: http://ncmt.westernpower.com.au/index.cfm)



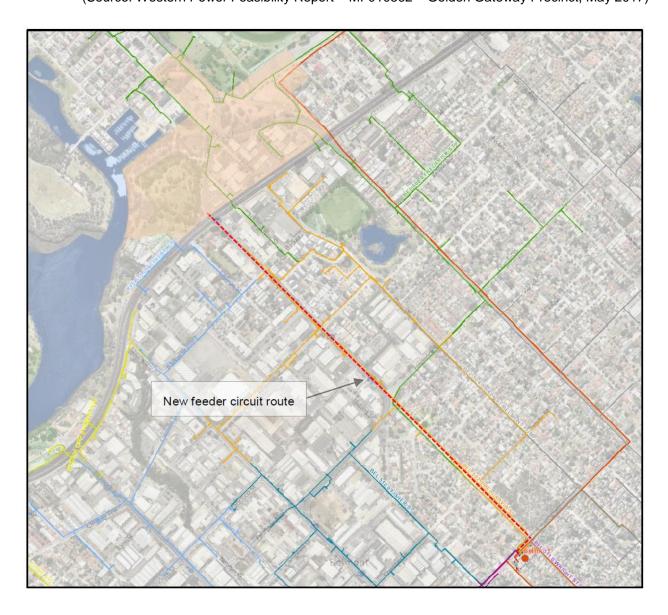
Western Power has completed a feasibility report for the proposed development, which is attached in Appendix B. Western Power has advised the following:



"Network analysis has identified that there sufficient capacity on the present configured network, and new feeder circuit would not be required as there are adequate spare capacity available on the BEL508 and surrounding feeders (BEL502 & RVE526) to fully accommodate the 8.325MVA total load. However, as the load growth to the redevelopment area is not expected till 2031, it is deemed reasonable that the provision to install a new 2km long feeder from BEL to entirely supply the 8.325MVA load may be required."

The proposed route of a new feeder from the Belmont Substation is shown in **Figure 4-3**.

Figure 4-3 Proposed Western Power Feeder Route
(Source: Western Power Feasibility Report – MF010862 – Golden Gateway Precinct, May 2017)





### 5 Gas

### 5.1 Existing Infrastructure

Gas infrastructure and distribution in Western Australia is managed by ATCO Gas Australia.

Correspondence from Atco Gas identifies Medium Low Pressure (MLP) gas mains (pressure indicated at 70kPa) along most roads within the subject site.

Cardno Drawing *CW942300-CI- SK4* in **Appendix A** contains information on gas infrastructure in the vicinity of the area.

### 5.2 Required Infrastructure

Correspondence received from Atco Gas advised that the existing infrastructure can support the proposed development as outlined in the Structure Plan.



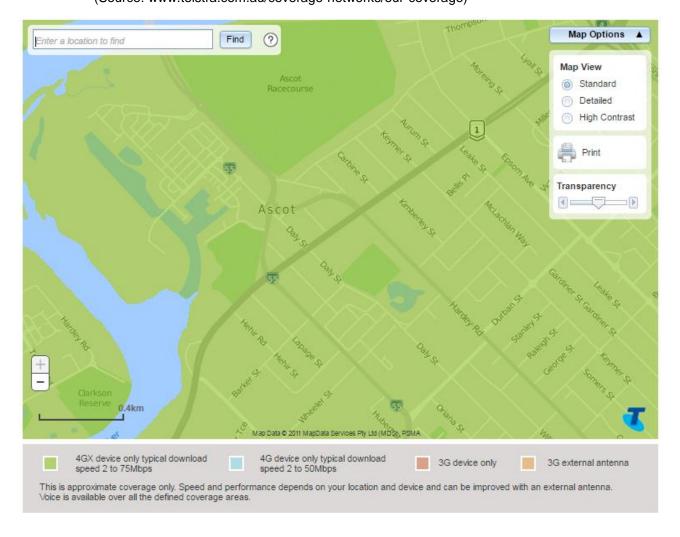
### 6 Communications

### 6.1 Existing Infrastructure

The subject area is well serviced by telecommunications infrastructure with optical fibre running in or adjacent to all precincts. This infrastructure is owned by various telecommunications providers including Telstra, Optus and others.

Refer to Cardno Drawing CW942300-CI-SK5 in Appendix A for a detailed plan of the fibre optic cable locations.

Figure 6-1 Telstra Mobile Network Coverage
(Source: www.telstra.com.au/coverage-networks/our-coverage)



Mobile network coverage in the area is well serviced with 4G covering the entire subject area under the Telstra network (as shown in **Figure 6-1**); other network providers may vary.

The National Broadband Network (NBN) has yet to be rolled out in the subject area. However, NBN Co have advised that fibre to the node (FTTN) technology rollout has been planned for October-December 2017.

### 6.2 Required Infrastructure

### 6.2.1 Telstra

Should a developer wish to register a development with Telstra smart communities; this must be done twelve weeks prior to construction.



The infrastructure within a development will be installed by the developer. Alternatively, Telstra can be engaged to install infrastructure within a development at the developer's expense.

Telstra's commercial pit and pipe service will generally not be offered in developments where NBN Co has confirmed agreement to install NBN Co fibre within a development stage.

### 6.2.2 NBN

As NBN is still in the planning phase, it is recommended that the City of Belmont liaise with NBN Co as per the Best practice guide for Councils when initially dealing with NBN Co document published by the Australian Local Government Association and NBN Co.

In line with the new *Telecommunications Infrastructure in New Developments* policy, NBN is required to recover part of the cost of deploying network infrastructure by applying a deployment contribution charge. These deployment charges only apply to developers and builders.

- A charge of \$400 per premises in multi dwelling units (MDUs).
- A charge of \$600 per premises within a single dwelling unit (SDU).

A backhaul contribution charge may also apply to the development, NBN will clarify this requirement when the developer submits his application.

APPENDIX



**EXISTING INFRASTRUCTURE** 



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APPENDIX

B

WESTERN POWER FEASIBILITY STUDY





### Feasibility Report

MF010862 - Golden Gateway Precinct

Large Mixed-Use Development – 8.325MVA Supply Options

westernpower

### **Document release information**

Client	Cardno (WA)
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Distribution Network Design Officer

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### **Table of contents**

1	Intro	oductio	n	1
	1.1	Backg	ground	1
	1.2	Purpo	se	1
	1.3	Scope	e of Study	1
2	Stud	dy Activ	rities	2
	2.1	Activit	ty 1 – Network Configuration Assessment	2
	2.2	Activit	ty 2 – Network Impact Assessment	2
		2.2.1 2.2.2	Transmission Distribution	2
3	Tec	hnical E	Evaluation	5
	3.1	Suppl	y Options	5
		3.1.1 3.1.2 3.1.3 3.1.4 3.1.5	Western Power Scope of Works	5 5 6 6
4	Con	clusion	s and Recommendations	7



### 1 Introduction

### 1.1 Background

Cardno has submitted a feasibility study on behalf of the City of Belmont, in conjunction with the Department of Planning to undertake an infrastructure and services strategy for the Golden Gateway Precinct in Ascot. The strategy will help guide the preparation for the Local Structure Plan over the area. Cardno is seeking information on the available network capacity to supply the Golden Gateway Precinct.

### 1.2 Purpose

The proposed outcomes from the feasibility study are;

- Desktop network assessment on the nearby distribution HV networks to determine the available capacity from these networks.
- Network planning capacity assessment (Distribution & Transmission) to determine available capacity from zone substations within proximity to the development.
- High level scope of works for the transmission and distribution works required to provide up to 8.325MVA of capacity (if reinforcement or extension is required).

### **1.3** Scope of Study

The activities that will be undertaken to achieve the specified outcomes are;

- 1. Network Configuration Assessment
- 2. Network Impact Assessment
- 3. Western Power Scope of Works

### 2 Study Activities

### 2.1 Activity 1 - Network Configuration Assessment

The proposed development are is set amongst the BEL508 22kV feeder network emanating from the Belmont zone substation (BEL) located approximately 2.0km south. The BEL508 22kV feeder along with three other HV feeder networks (BEL502, RVE511 & RVE526) are the only networks within close proximity to the redevelopment (figure 1).



Figure 1 - Existing Distribution HV Network

### 2.2 Activity 2 – Network Impact Assessment

### 2.2.1 Transmission

Network analysis was carried out on the closet zone substation to the redevelopment area. It has been identified that there is sufficient spare NCR capacity available from BEL to cater for this 8.325MVA undiversified load. The load forecast chart for BEL is provided in figure 2.

Currently, BEL is supplied from the Cannington Terminal via BEL-KDL 81 line and BEL-RVE/WE 81 line (with the pre-contingency being BEL-NT/EP 81 line open). In connecting this load, it is not expected for the affected 132 kV transmission lines to experience the issue relating to the thermal over-loading or under-voltage, during the N-1 contingency. As well, connection of this customer load is not expected to trigger any voltage instability issues in the load area, hence this load is cleared to connect to the Western Power BEL network.

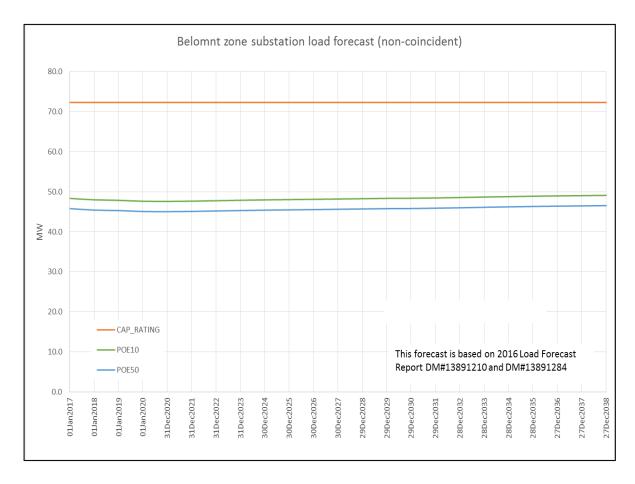


Figure 2 - BEL Zone Substation Forecast

### 2.2.2 Distribution

The entire redevelopment area is currently supplied by the BEL508 Frederick St feeder, including three other feeder networks (BEL502, RVE511 and RVE526) within close proximity. Based on the BEL508 feeder load readings (figure 3), there is approx. 3MVA of spare capacity available at this point of time that can be directly connected into. Additional network capacity can also be made available by network reconfiguration or extension, provided that there are significant spare capacity available on the nearby feeders at the time of connection. Hence, it is likely that the first few stages of development area can via supplied without any major network extension or reconfiguration.

Due to the expected timing of the power uptake, there is no certainty what spare capacity will still be available on the BEL508 feeder and other feeders around the proposed redevelopment area. Hence, it is not feasible to estimate what network extension will be required to create sufficient network capacity to supply the 8.325MVA load. An alternative option is to install of a new feeder from BEL to the proposed development boundary, near the intersection between Great Eastern Hwy and Stoneham St, to supply the entire 8.325MVA load. The new feeder circuit is likely to be installed along Belgravia St which will require approximately 2km of 400mm2 Al XLPE 22kV cable. Although there is no spare feeder circuits available at BEL, arrangement can be made (such as double feeders termination) to allow new feeder connections to the BEL.

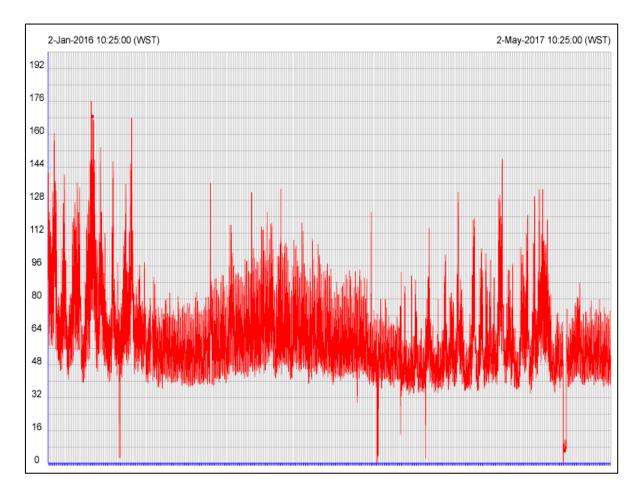


Figure 3 - BEL508 Feeder Utilisation

### 3 Technical Evaluation

### 3.1 Supply Options

### 3.1.1 Overview

As stated in section 2.2.2 of this report, there is approximately 3MVA of spare capacity available on the BEL508 Frederick St feeder with the opportunity to utilise the surrounding feeders (BEL502 & RVE526) to fully accommodate the total 8.325MVA load until either exhausted by the customer's development or other competing applications. A new feeder circuit can be provided for further capacity beyond the existing HV networks capacity limitations.

Considering the above information, there are two design options which have been identified to meet the customers' requirements;

- 1. Utilise the remaining capacity available on the BEL508 feeder and other nearby HV networks until exhausted. The scope of works for this option cannot be defined due to the unknown load uptake and location of connections to the redevelopment area.
- 2. Once depleted, install approximately 2.0km of new underground cable from the BEL to the redevelopment area expected along Belgravia St.

### **3.1.2** Site Map

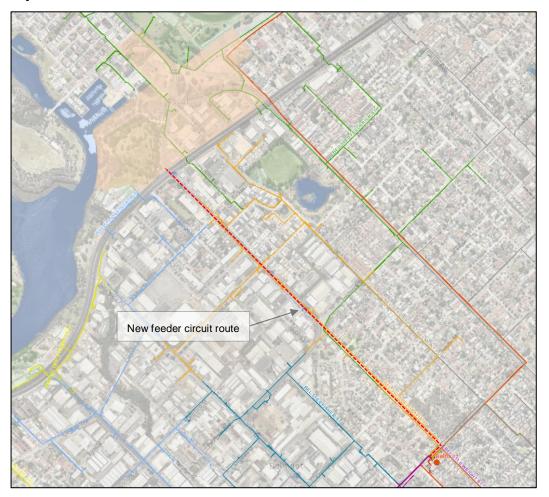


Figure 4 - Proposed Western Power Scope of Works

### 3.1.3 Western Power Scope of Works

With reference to the site map in section 3.1.2 of the document, the Western Power scope of works for the new feeder extension is as follows:

- The connection of the new circuit into the BEL zone substation.
- Cable installation by a combination of open trenching and directional drilling from the BEL to the corner of Great Eastern Hwy and Stoneham St.
- Cable jointing, including testing and commissioning.

### 3.1.4 Third Party Approvals

If any of the surrounding HV feeders are to be extended or a new feeder circuit is installed from BEL then it is likely that the proposed cable route will need to cross under the Great Eastern Fwy. This instalment of new cable will require the approval from Main Roads. The underground cable route will be determined when a formal application has been received and detailed planning studies have been conducted.

### 3.1.5 Assumptions

The customer contribution and scope of works are dependent on the following assumptions;

- No other connection requests and changes to network conditions prior to the formal application for this connection.
- The proposed design solution, estimated cost (non-binding) is based on the desktop information only & is subject to detailed design investigation.
- All new underground cables are assumed to be installed in at the Western Power standard depth (i.e. 850mm deep from finished level) and in the Western Power standard alignment (0-500m from property boundaries) apart from road crossings.
- Drilling depth of electrical cables under roadways must be between 1000 and 1500mm of ground level.
- Allowance of polypipe included for the proposed cable route where cable is crossing under roadway or deemed rock ground conditions.
- Main Roads approval is granted for works associated on Great Eastern Hwy
- The proposed works receive no objection from all involved parties (which may include local authorities, private land owners and/or other utilities).
- The interconnection works required within the development site boundary are not considered in the study.
- The load assessment on the submission of the formal application will support the customers load request.
- Environmental studies have not been undertaken for the purpose of this report.
- Detailed Load Flow and Power Quality studies have not been undertaken for this study.

### 4 Conclusions and Recommendations

Network analysis has identified that there sufficient capacity on the present configured network, and new feeder circuit would not be required as there are adequate spare capacity available on the BEL508 and surrounding feeders (BEL502 & RVE526) to fully accommodate the 8.325MVA total load. However, as the load growth to the redevelopment area is not expected till 2031, it is deemed reasonable that the provision to install a new 2km long feeder from BEL to entirely supply the 8.325MVA load may be required.

Applicants need to be aware that the information herein is provided in good faith and is accurate at the time of issue. Power systems are dynamic in nature, due to the connection of new users and changes in consumer behaviour. As such, Western Power's distribution electricity networks will change over time - this may have a bearing on the amount of reinforcement required to accommodate new developments.

As capacity cannot be reserved, it is possible that requirements will also be altered resulting in a variation in power infrastructure requirements. There may be other competing applications for new loads or upgrades which may use the available spare capacity.

Please be aware that Western Power's response may become out-of-date, resulting in a significant variation in power infrastructure requirements. To provide a firm connection proposal and cost, a formal application to Western Power will need to be made, in accordance with current connection policies.

### **About Cardno**

Cardno is a professional infrastructure and environmental services company, with expertise in the development and improvement of physical and social infrastructure for communities around the world. Cardno's team includes leading professionals who plan, design, manage and deliver sustainable projects and community programs. Cardno is an international company listed on the Australian Securities Exchange [ASX:CDD].

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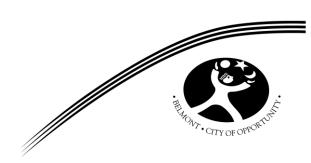


### Ordinary Council Meeting 26/02/19

Item 12.6 refers

**Attachment 22** 

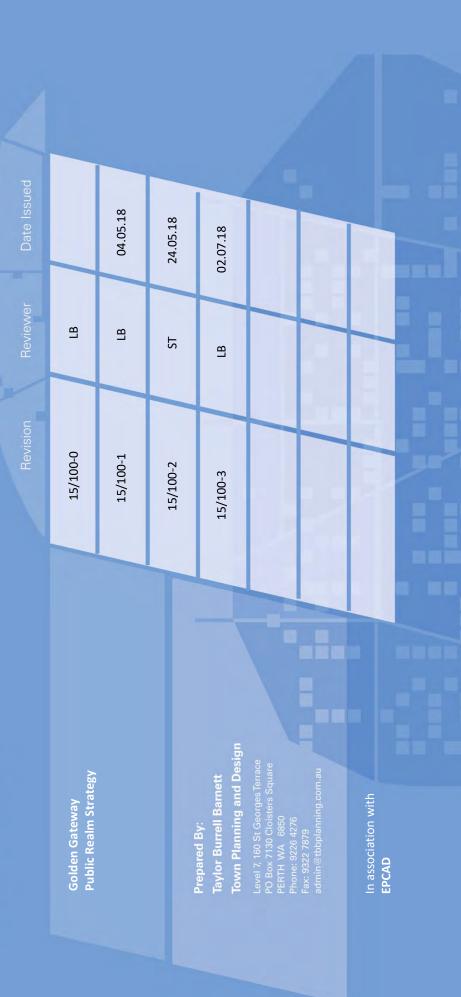
**Public Realm Strategy** 







### DOCUMENT HISTORY AND STATUS



## EXECUTIVE SUMMARY

This Public Realm Strategy has been prepared as part of the suite of detailed strategies and studies supporting the Local Structure Plan (LSP) for the Golden Gateway precinct in

elements of the public realm as articulated in this overarching framework will inform quality public realm. The graphical representation is indicative only and demonstrates how The purpose of this Strategy is to develop a clear vision, principles and objectives to inform development of the public realm. The design intent and functional requirements for developed as a component of this Strategy to outline the underlying public realm design objectives and guide the design and development of an integrated and functional high further detailed planning, design and management. A landscape masterplan has also been the three key public space areas may be developed. The strategy creates an approach to the public realm that will create one very distinctive urban character. The public realm will accommodate pedestrians and vehicles in a safe uncluttered manner and the streets and spaces will be shaded by trees that will form a strong visual landscape framework.

Existing local streetscapes are predominantly reflective of the commercial environment, particularly within the commercial 'triangle'. The standard of verge maintenance ranges from good quality reticulated lawns through to poorly maintained verges damaged by random, uncontrolled, overflow parking.

the site is of a standard commensurate with the nature of existing development across the subject land (i.e. primarily light industrial/commercial unit style development). Each of the major road corridors running through the precinct (Grandstand Road, Resolution Drive and Stoneham Street) include footpaths along one side of the street. The extent and The extent and quality of the existing pedestrian infrastructure within, and surrounding, quality of the existing cycling infrastructure within and surrounding the site is of a high standard, partly as a result of the Great Eastern Highway upgrades.

circulation, accommodates vehicles in a safe and logical manner and is an environment that presents a desirable destination to live, work and recreate. Placemaking should The Strategy sets out to provide a high quality urban framework that promotes pedestrian inform the detailed design of spaces throughout the precinct. The spaces need to be able to facilitate and accommodate diverse uses that may emerge from community social investment.

Places across the site will achieve a successful balance between physical attributes, the vehicle circulation and dynamic social, cultural and economic vitality. Its inherent qualities are strongly related to its proximity to the Swan River and its heritage related to the Ascot Kilns.

It is the intention that distinctive physical spaces will be encouraged to evolve, responding to community, social and commercial opportunities. Spaces will consolidate a strong identity and character that is easily recognised by local users and visitors. In accordance with best practice, the public realm should be designed to maximise universal access for all members of the community. Designs will need to comply with prevailing legislation but should also strive to safely accommodate ease of safe use encouraging full accessibility through all areas. The strategy for the site comprises a number of different public realm space types ranging from the strong east-west Linear Park ("Greenlink"), boulevard high-use roads to small streets. A cohesive approach across the public realm will consist of an urban landscape that reinforces a fluid and flowing spatial arrangement starting from the river parklands and extending this character throughout the subject land.

character that is then extended through the locality by complementary paving designs and parklands and creates an open space core to the Golden Gateway precinct, is the primary tree canopies. Daly Street is structured to encourage and accommodate street commercial opportunities as buildings have trading frontages. This street becomes the The creation of a strong east west aligned central space that links to the Swan River structuring component of this area. The design of this space establishes the "high street"

Golden Gateway | Public Realm Strategy

In terms of implementation, under normal circumstances, the development of the public realm is typically undertaken by a private developer/s as part of their private land subdivision process; however, given that several areas of the public realm already exist in the form of Crown Reserves (e.g. existing road reserves) and the private land is under fragmented ownership, the City of Belmont will need to assume responsibility for implementing the Public Realm Strategy. The cost of this work and any mechanism to recover cost from private landowners through a Developer Contribution Plan or alternative funding mechanism to be determined by the City will require further consideration.

It is not anticipated that the entire landscape masterplan be implemented at once, rather it should be progressively rolled out commensurately with the delivery of other key infrastructure, particularly the various road realignments and subdivision works that are required to create the environment for private redevelopment. Priority should be given to establishing the road network and lots that frame the POS spine and the primary focus should then be the implementation of this public infrastructure to set the stage for the precinct's credentials as a high quality development opportunity.

# TABLE OF CONTENTS

X	ECUTIV!	EXECUTIVE SUMMARY	_
<u>.</u>	INTRO	1. INTRODUCTION	4
	<u></u>	PURPOSE	4
	1.2	SITE CONTEXT	9
2.	SITE AI	SITE ANALYSIS	7
	2.1	URBAN FORM	7
	2.2	STREETSCAPE	7
	2.3	MOVEMENT AND ACCESS	0
	2.3.1	VEHICLE MOVEMENT	0
	2.3.2	PEDESTRIAN NETWORK	0
	2.3.3	CYCLING	o
ω.	DESIG	DESIGN OBJECTIVES	10
	3.1	AN URBAN LANDSCAPE	10
	3.2	IDENTIFIABLE CHARACTER	10
	3.3	VALUABLE LANDSCAPES	10
	3.4	ENVIRONMENTAL/SUSTAINABILITY	10
4	PUBLIC	PUBLIC REALM OVERALL APPROACH	12
5.	<b>PARKS</b>	13	
	5.1	FORESHORE RESERVE	14
	5.2	LINEAR PARK "GREENLINK"	14
	5.3	EAST PARK	15
9.	ROADS	ROADS AND STREET TREATMENTS	16

	6.1	GENERAL	16
	6.2	ROAD TREATMENTS	16
	6.3	RESOLUTION DRIVE	17
	6.4	STONEHAM STREET	$\infty$
	6.5	DALY STREET	19
	9.9	CENTRAL STREETS	20
	6.7	LOCAL STREETS PRECINCT (NORTH-EAST)	21
	8.9	GATEWAYS	21
7.	INTEG	INTEGRATED DRAINAGE MANAGEMENT	22
œ.	STREE	STREET FURNISHING	24
0	PUBLIC ART	SART	25
10.	GOLD	10. GOLDEN GATEWAY TREE SPECIES	26
	10.1	PARK AND CIVIC SPACE SPECIES	26
	10.2	STONEHAM STREET AND RESOLUTION DRIVE 26	26
	10.3	DALY STREET	26
	10.4	CENTRAL STREETS	27
	10.5	LOCAL STREETS PRECINCT	27
	10.6	HIGHLIGHT SPECIES (GATEWAYS)	27
<u></u>	IMPLE	11. IMPLEMENTATION	28
		LANDSCAPE CONSTRUCTION AND MANAGEMENT	28
	11.2	WATER MANAGEMENT	28
	11.3	STAGING	28

## 1. INTRODUCTION

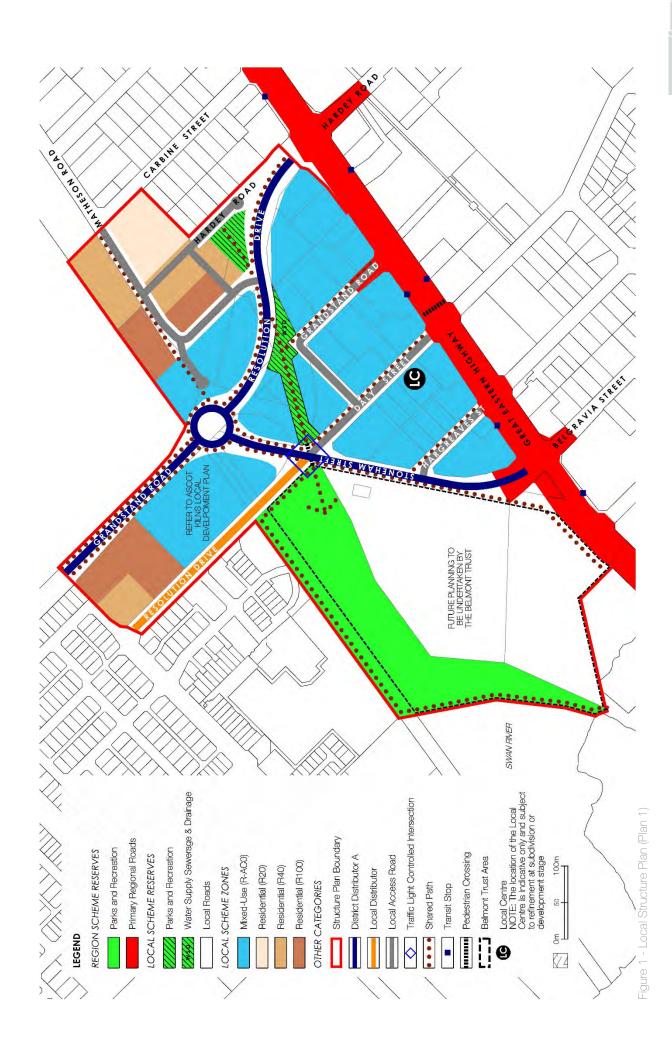
### 1.1 PURPOSE

This Public Realm Strategy has been prepared as part of the suite of detailed strategies and studies supporting the Local Structure Plan (LSP) for the Golden Gateway precinct in Belmont (refer **Figure 1**).

The creation of a high quality and functional public realm, in the streets and open spaces, is a pivotal element in planning for a more intensified urban environment to create a liveable and well connected community.

The Public Realm Strategy has been developed in conjunction with the Golden Gateway Development Concept Plan that ultimately formed the cornerstone of the Golden Gateway LSP. This has meant that the resultant urban form envisaged in the landscape masterplan has been developed around a thoroughly considered framework of public spaces.

This document summarises the main issues/opportunities and design outcomes for the creation of a public realm, similar to the concept of an urban village. The purpose of this report is to inform the LSP and the further design (Design Guidelines) of this area through the planning process. This report is a companion document to the LSP and should be read in conjunction with it.



### SITE CONTEXT

The subject land is located approximately 5 kilometres (km) north east of the Perth Central Business District (CBD), 3 km north of Belmont Forum and 5 km north east of Victoria Park entertainment precinct (refer **Figure 2**). It is close to the Swan River and Ascot Racecourse and forms a triangular land parcel that is well connected to the regional roads. Further details on the planning context and background can be found in the LSP Part Two, Section 1 Planning Background.

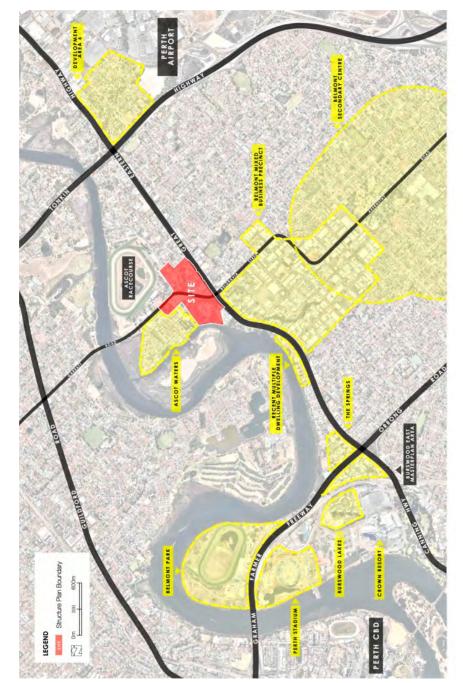


Figure 2 - Site Context Plan

## Golden Gateway | Public Realm Strategy

## 2. SITE ANALYSIS

### .1 URBAN FORM

The existing urban form of the site is very much influenced by its strategic location at the axis of a number of key movement corridors, dominated by Great Eastern Highway, Stoneham Street and Resolution Drive. The 'triangle' of land bounded by these roads contains a mix of office and commercial uses, including some more intensive retail/food and beverage outlets towards the eastern edge at Resolution Drive and Great Eastern Highway.

Outside of the 'triangle', the remainder of the LSP area consists of a number of different sub-precincts with very diverse functions and characteristics. These include a mix of land uses, including the administration headquarters of the WA Turf Club (WATC), the Ascot Kilns, overflow parking for the Ascot Racecourse, a substantial riverfront area held by the Belmont Trust, and a patchwork of residual government landholdings created by the past realignment of Resolution Drive and Stoneham Street.

While the existing urban form is largely unremarkable, the key features that are notable, in terms of future planning, include:

- . The Ascot Kilns, in particular the chimneys, which present an important visual and historical reference point in the precinct (refer **Figure 3**); and
- 2. The Belmont Trust land, which presents an opportunity for a strong public link to the Swan River, albeit presently isolated by Stoneham Street (refer **Figure 4**).

### 2.2 STREETSCAPE

Existing local streetscapes are predominantly reflective of the commercial environment, particularly within the commercial 'triangle' (refer **Figure 5**). The existing road reserves are typically 20m wide with wide carriageways to accommodate commercial vehicle movement as well as on-street parking. The standard of verge maintenance ranges from good quality reticulated lawns through to poorly maintained verges damaged by random, uncontrolled, overflow parking.



igure 3 - The Ascot Kilns Chimneys



igure 4 - Belmont Trust Land



Figure 5 - Typical 'Commercial' Streetscape

Great Eastern Highway, the most exposed edge of the precinct, has recently been widened/upgraded to improve regional traffic movement. The result is a heavily engineered, highly efficient arterial road, with four lanes of through-traffic, increasing to 6-7 lanes in places where there are long turning pockets and bus/cycle lanes at the intersections.

The footpath is approximately 3m wide and occupies the whole verge from kerb to boundary, with no street trees or other landscaping, as illustrated in **Figure 6**. This combined with the significant traffic activity immediately adjacent, presents an unappealing environment for pedestrians.

Resolution Drive and Stoneham Street are also heavily engineered arterial roads that offer little attraction to the pedestrian, although the Stoneham Street environment is somewhat softened by its interface with heavy vegetation along the periphery of the Belmont Trust land and the landscaped drainage area to the north.



Figure 6 - Great Eastern Highway

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## 2.3 MOVEMENT AND ACCESS

## 2.3.1 VEHICLE MOVEMENT

The LSP report provides a detailed analysis of the existing and proposed vehicle movement network. From a public realm perspective the key factors are as follows:

- The regional road system, comprising Great Eastern Highway, Stoneham Street, Resolution Drive and Grandstand Road, offer excellent connections in all directions; however, they also serve to segregate parts of the precinct, and isolate the site from the most attractive existing public realm asset, being the Swan River foreshore.
- The local road system, particularly through the commercial 'triangle', provide a high level of access and permeability for both vehicles and pedestrians, and offers an effective framework for future development of the site; and
- The local road system features wide (20m) road reserves, which, if retained, offer opportunities to design high standard streetscapes, with generous space available to devote to landscaping, pedestrians, street parking etc.
- Local access streets (Hargreaves Street, Daly Street and Grandstand Road (southern section) providing access in a northerly direction from Great Eastern Highway with poor pedestrian amenity and no existing footpaths present.

## 2.3.2 PEDESTRIAN NETWORK

The extent and quality of the existing pedestrian infrastructure within, and surrounding, the site (with the exception of Great Eastern Highway) is poor and of a standard commensurate with the nature of existing development across the subject land (i.e. primarily light industrial/commercial unit style development).

However, Great Eastern Highway bordering the site to the south features good quality footpaths on both sides of the corridor, although as previously mentioned, it is not a particularly appealing environment for pedestrians.

Within the vicinity of the site, the safe crossing of Great Eastern Highway by pedestrians is facilitated via traffic signal controlled intersections at both Stoneham Street/Belgravia Street and Resolution Drive/Hardey Road intersections with Great Eastern Highway.

Each of the major road corridors running through the precinct (Grandstand Road, Resolution Drive and Stoneham Street) include footpaths along one side of the street – Grandstand Road along the eastern side adjacent to the Ascot Racecourse, Raconteur Drive along the northern side to connect to Grandstand Road, Resolution Drive along the eastern side adjacent to the Ascot Waters development and Stoneham Street along the western side adjacent to the Belmont Trust land.

### 2.3.3 CYCLING

The extent and quality of the existing cycling infrastructure within and surrounding the site is of a high standard, partly as a result of the Great Eastern Highway upgrades.

A number of existing shared paths and cycling connections are located along primary routes, including Stoneham Street, Raconteur Drive and Grandstand Road providing local connections. There is demand to upgrade facilities on Stoneham Street and Resolution Drive. Protected bicycle lanes and a shared path on Resolution Drive is essential.

A number of shared paths are also located within the Ascot Waters development directly to the north-west of the site. The Graham Farmer Freeway Principal Shared Path (PSP) provides regional cycling connections and can be accessed via the shared path along the southern side of the Swan River.

## 3. DESIGN OBJECTIVES

## .1 AN URBAN LANDSCAPE

The site forms an important gateway announcing the City of Belmont when approached from the south-west and north-east. The site is traversed with major roads and as discussed, its triangular form presents challenges in vehicular circulation and pedestrian accessibility. This location currently presents as a transient place that is passed through, however the design of the public realm will result in the creation of a cohesive network of spaces enabling the locality to be an identifiable place.

As a busy location, the public realm offers the opportunity to be transformative, linking uses and people to the nearby valued Swan River, its parklands and the heritage and interest of the Ascot Kilns.

The public realm spaces made up of streets and a linear park, combine to be a defining element of this location, that importantly the users, employees and residents will experience and define the qualities of the public realm.

The overall landscape design objectives for the public spaces are set out below:

## 3.2 IDENTIFIABLE CHARACTER

- Create a contemporary urban environment that promotes safe and easy pedestrian experiences.
- Create new diverse urban landscapes that reflect the subject land's unique characteristics and close links to the river parklands.
- Create spaces that encourage and accommodate local community use and engagement.

- Establish an aesthetic that promotes positive development and investment in the location.
- Celebrate the heritage significance of the Ascot Kilns.
- Respect the social and recreational values of the Ascot Racecourse.
- Establish considered connections to the Ascot Kilns and the Ascot Racecourse in terms of tourism opportunities and amenity.

## 3.3 VALUABLE LANDSCAPES

- Create a microclimate in public realm spaces and streets which encourages use and enjoyment.
- Provide visual connections to the river park through public open spaces (as shown on Figure 7).
  - Provide key views and relationships that assist in orientation and legibility.
- Create highly utilised and valued public realm streets and spaces.

## 3.4 ENVIRONMENTAL/SUSTAINABILITY

- Create a durable urban landscape.
- Reduce urban heat sink characteristics.
- Create urban tree canopy (in compliance with The City of Belmont's Urban Forest Strategy 2014).
- Retain vegetation wherever practical.
- Promote the use of low water demand plants.
- Pursue water harvesting, passive irrigation and integrated urban water management.



Figure 7 - Landscape Masterplan

# 4. PUBLIC REALM OVERALL APPROACH

The site comprises a number of different public realm space types ranging from the strong east-west linear park ("Greenlink"), boulevard high-use roads to local streets as detailed on the landscape masterplan (refer **Figure 7**).

A cohesive approach across the public realm will consist of an urban landscape that reinforces a fluid and flowing spatial arrangement starting from the river parklands and extending this character throughout the subject land. The creation of smaller pockets of activity and open space will be defined by street trees, tree groups and sinuous tree lines. Pedestrian spaces will be sheltered by a substantial tree canopy and vehicular routes flanked by boulevard plantings. A unified paving design and materials for pedestrian areas will extend throughout the subject land extending down streets and through the central Linear Park. This will both unify and delineate the different pedestrian and vehicular spaces.

Placemaking should inform the detailed design of spaces throughout the precinct. The spaces need to be able to facilitate and accommodate diverse activities that may emerge from community social investment. The location and development of the public spaces will be achieved through the successful balance between physical attributes, the vehicle circulation and dynamic social, cultural and economic vitality. The site's inherent qualities are strongly related to its proximity to the Swan River and its heritage related to the Ascot Kilns. It is the intention that distinctive physical spaces will be encouraged to evolve beyond the design, responding to the growing community and social and commercial opportunities. Spaces will consolidate a strong identity and character that is easily recognised by local users and visitors.

In accordance with best practice, the public realm should be designed to maximise universal access for all members of the community. Designs will need to comply with prevailing legislation but should also strive to safely accommodate ease of safe use encouraging full accessibility through all areas.

To reduce maintenance and water consumption, where possible, consideration should be made as to the use of hard surfaces or low water alternatives instead of turf. Water harvesting of hard surfaces is also exploited where possible using swales, channels and ground amendments to reduce the need for overall water consumption.

### 5. PARKS

Public Open Space (POS) is to be provided generally in accordance with Figure 8 and should be vested in the Crown and managed by the local government. The development of land included within the Swan River Trust Development Control Area will be subject to the approval of the Department of Biodiversity, Conservation and Attractions (DBCA). The POS is to provide for both informal active and passive recreation uses. These uses will not utilise large spaces for sports but provide activities for the community that may include, children's play areas, health and fitness trails, small scale ball kick-a-bout areas and one-on-one basketball spaces. The POS areas may accommodate stormwater generated from the proposed development of the site and this will be designed in such a manner that its function as local open space is not compromised.



Figure 8 - Public Open Space Provision

Structure Plan Boundary

Foreshore Reserve

LEGEND

## .1 FORESHORE RESERVE

The 'Foreshore Reserve' creates a valued open space adjacent to the Swan River. The nature of the space, its future and development, is controlled largely by the Belmont Trust and is not the subject of this Public Realm Strategy but will be addressed by a separate study.

## .2 LINEAR PARK "GREENLINK"

The Linear Park ("Greenlink") is a valuable community asset which has great potential to be the focus of community use, a meeting place and the primary pedestrian movement corridor linking the subject land with the Swan River and associate parklands to the west (refer **Figure 9**). The linear park is an urban park relying on tree canopies to provide shade and "softness" to the urban space. The size of paved areas should be able to accommodate potentially large numbers of users including cyclists, skaters, and pedestrians all within a network of footpaths linking into the surrounding road network and building entrances. The space will create a seamless and comprehensive character that embraces built form and is unaffected by the rigidity of traditional street infrastructure.

The informality of parklands will envelope development forming a new dynamic setting that is capable of providing informal meeting spaces, alfresco spill-out, community meeting places and potentially facilities such as active recreational spaces, outdoor fitness trail equipment and interactive sculptural elements as public art. The space will facilitate clear passive surveillance from the lower levels of buildings and will be well lit at night. The open and broad nature of spaces providing safe pedestrian circulation and sight lines, lighting and activity will enable the space to accommodate the needs of a growing local population.

Importantly, this space and its robust form diminishes the role of vehicular traffic aesthetically creating a dominance of pedestrian orientated space.

The design of this central linear park will create the setting or venue for community use. The emerging community values and enjoyment of a place for its special social and physical attributes is enhanced when the community can contribute to, and influence, decision-making about that space. Ongoing public and stakeholder engagement in regards to the use of spaces, their evolution and resultant design detail should therefore be an integral part of spatial management and design delivery. As the community grows and matures, places can respond to changing needs and opportunities. Management of this place therefore needs to include stakeholder engagement as well as consider the pragmatics of maintenance. The place may accommodate community or food gardens, established as an urban element within the overall design character. This can provide for activities that may not be possible within multiple dwelling and grouped developments, however such proposals should be derived from further consideration of the evolving community's needs and desires.



-igure 9 - Linear Park Green Link

### 5.3 EAST PARK

At the eastern-most extent of the POS network, a large simple space of grass is located (refer **Figure 10**). This forms a transitional space between mixed-use and multiple dwelling development and the lower density existing residential and stables area that is located to the east of the site. The park creates a soft, semi-active open space suitable for informal ball games and passive pursuits.



Figure 10 - East Park

# 6. ROADS AND STREET TREATMENTS

### .1 GENERAL

Road hierarchies and overall legibility of the subject land will be reinforced by the type of tree planting associated with the scale of the road. The paving treatments within all streets and roads will be consistent with the material palette of the Linear Park, reinforcing a distinctive character of this place.

The scale and robust nature of proposed street tree species relate to the potential scale and height of built form. Street trees have an important role in the urban environment, improving microclimate and urban heat sink characteristics, reducing storm runoff rates and contributing to the character and qualities of neighbourhoods. The detailed design of roads will need to ensure the provision of adequate soil volumes within road reserves to ensure sufficient root development for street trees.

## .2 ROAD TREATMENTS

Road hierarchies and overall legibility of the precinct can be enhanced with the use of varied road and footpath paving treatments, in keeping with the material palette of the Linear Park. Consideration should be given to the use of block pavers at road junctions or to create varying precincts within the development. For example, emphasising Daly Street and the extension of Grandstand Road where it forms the edge to the Linear Park.

The selected paving treatments of local streets should emphasise the overall precinct character. All paving detailing at junctions and associated with pedestrian circulation should address both the need to reduce traffic speeds, manage drainage and create a distinctive character. Raised tables can be used to provide traffic calming and to add texture to the urban streetscape reinforcing a character that promotes pedestrian safety.

Cycle lanes throughout the site will be red asphalt except where they are incorporated into areas of feature pedestrian paving where colour differentials will relate to paving patterns, and if necessary, lanes defined by studs. Paving material changes will be used to accentuate areas such as major pedestrian road crossings, civic areas and hazards. Parking bays should be differentiated from the road reserve through the use of alternative paving treatments as shown in **Figure 11**.

The materials used for road pavement can assist with drainage management within the area. This may include the use of permeable paving and/or porous brick paving and/or porous asphalt. These materials can play a significant role in managing drainage in a water sensitive manner and where 'soft' open space is not an extensive feature of this location.



Figure 11 - Material Palette (Illustration of indicative paving material palette, colour, type)

## 6.3 RESOLUTION DRIVE

Whilst Resolution Drive will be largely vehicle dominated, the landscape aesthetic will be dominated by tree planting of larger species, creating a canopy boulevard along its length. Verge and median planting will create a formalised sinuous corridor of canopy trees that are recognisably different to the scale and nature of other landscapes in the area (refer **Figure 12**). Like street trees will be planted to create a boulevard aesthetic the length of the street, aiding in wayfinding (refer to section 10.2 for proposed tree species).



Figure 12 - Resolution Drive (Plan Extract and Indicative Section)

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## 6.4 STONEHAM STREET

Stoneham Street will be identified by a boulevard of planting comprising species related to the adjoining Belmont Trust land such as a mix of natives and introduced species emphasised at junctions and the key pedestrian crossing points (refer **Figure 13**). The boulevard will accommodate a key pedestrian connection that extends through to Matheson Road via the Linear Park. Street trees will be selected to form a large canopied street, adding to the boulevard aesthetic. (Refer to section 10.2 for proposed tree species).



Figure 13 - Stoneham Street (Plan Extract and Indicative Section)

### 6.5 DALY STREET

Daly Street is proposed to function as the 'main street' and as such, the public realm has been configured to respond to retail uses (refer **Figure 14**). The pedestrian pavement will be configured to minimise clutter and encourage possibilities for alfresco seating. Importantly the paving design character established within the linear park extends through the street extending to the Linear Park. Tree groups will be used and located to define potential smaller public realm areas such as alfresco seating and informal gathering spaces (refer to section 10.3 for proposed tree species). Car parking is configured at right angles to optimise numbers in support of retail and food and beverage uses within the 'main street'.



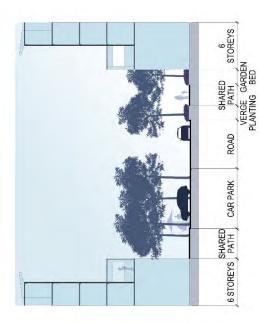


Figure 14 - Daly Street (Plan Extract and Indicative Section)

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## 6.6 CENTRAL STREETS

Hargreaves Street and Grandstand Road will comprise street tree planting that is not a monoculture but uses a mix of street trees (refer **Figure 15**) in varying combinations, to provide a dynamic and varied street tree canopy (refer to section 10.4 for proposed tree species). The mix will create a character that is related to, but distinguished from, Daly Street, emphasising the different nature of the space (refer to section 10.4 for proposed tree species). These streets will extend the overall public realm character established within the Linear Park and central portion of the site but in a simpler manner. Street tree planting is proposed to create a canopied streetscape and to be positioned abutting the parallel parking embayments.



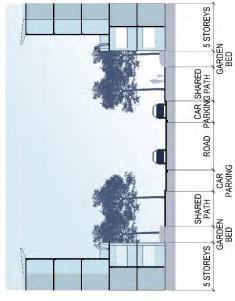


Figure 15 - Central Streets (Plan Extract and Indicative Section)

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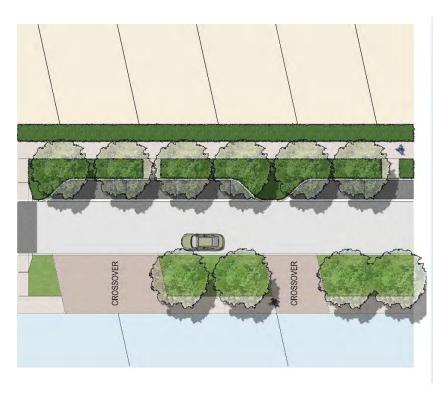
## .7 LOCAL STREETS PRECINCT (NORTH-EAST)

The streetscapes of the areas to the north and east of Resolution Drive will have a character that is dominated by street tree planting creating a heavy canopy. Street tree planting will consist of a variety of species that attain modest height but develop a broad canopy (refer **Figure 16**).

Raised paved tables can be used to provide traffic calming and to add texture to the urban streetscape reinforcing a character that promotes pedestrian safety. The selected paving treatments of local streets will change the character of streets especially in locations where separated pedestrian access is limited. All paving detail at junctions and associated with pedestrian circulation should address both the need to reduce traffic speeds, manage drainage and create a distinctive character.

### 6.8 GATEWAYS

In key locations within the streetscape and public realm, highlight tree species will be used to create a visual accent. This can aid in creating distinctive spaces, and provide physical cues within a legible street network. These highlight species will be used to create gateways, focal points or to emphasise uses. Refer to section 10.6 for proposed tree species.



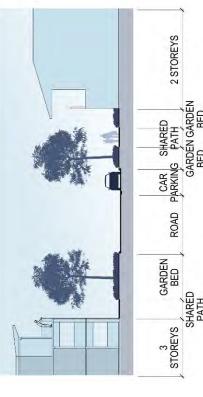


Figure 16 - Local Street (Plan Extract and Indicative Section)

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# 7. INTEGRATED DRAINAGE MANAGEMENT

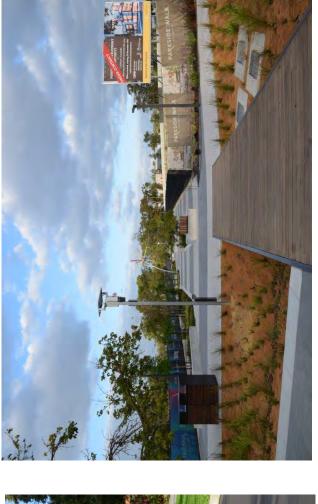
The use and promotion of Water Sensitive Urban Design (WSUD) techniques and approaches are to be utilised wherever possible throughout the site. The space for nutrient stripping is limited. As the urban area is not producing a nutrient load, the focus is on slowing runoff and reducing hydrocarbons. The use of linear and incidental 'rain gardens' and 'nutrient sinks' as demonstrated below and overleaf can be implemented discretely within paving in streets and areas of open space. These devices should be fully integrated with the road drainage promoting passive irrigation of street tree vegetation and controlling hydrocarbon runoff.

Within the context of a dense inner urban area, the design of these WSUD devices need not be natural in appearance but can be incorporated within the urban public realm infrastructure as a contemporary feature as demonstrated below and overleaf.

It is intended that the east-west linear park, although containing broad pedestrian areas, will contain soft landscape areas that will accommodate local drainage that is managed through swale type structures that infiltrate water and passively irrigate trees and other vegetation used in the public realm. This will be subject to the future Local Water Management Strategy.

The use of permeable pavements and porous asphalt treatments in key locations is recommended, possibly associated with lower level threshold treatments of road junctions, should be incorporated as a component of the approach to integrated drainage management.





Examples of Rain Gardens & Swale Designs in an Urban Context (Jolimont Parkside Walk)











Examples of Rain Gardens and Swale Designs in an Urban Context

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## 8. STREET FURNISHING

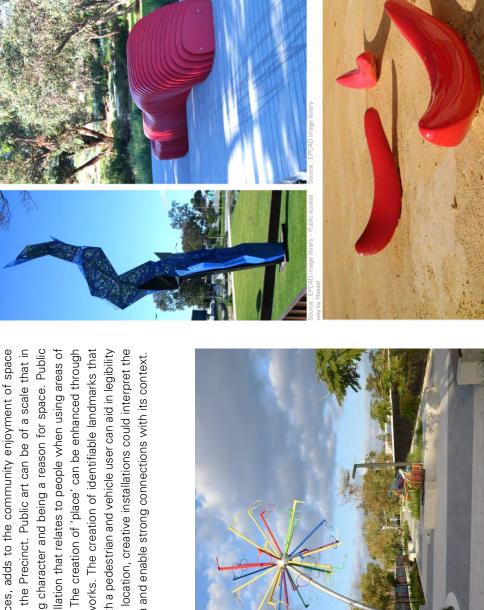
Street furniture should be a selected single suite of items that are consistent across the site. The furniture should be reflective of the heritage and character of the area and located where it can function as more than a single use. For example, seats and benches should be located in a manner to restrict undesired errant access to protect and guide pedestrians as well as performing their obvious use. All furnishing will be from the same suite so that bicycle storage, seats and bollards are seen as one cohesive design style.



## Golden Gateway | Public Realm Strategy

## 9. PUBLIC ART

Public art enhances spaces, makes places, adds to the community enjoyment of space and has a significant role to play within the Precinct. Public art can be of a scale that in itself is a focal point of interest, defining character and being a reason for space. Public art can also be an intimate smaller installation that relates to people when using areas of a sense of identity provided by the artworks. The creation of identifiable landmarks that can be observed and experienced as both a pedestrian and vehicle user can aid in legibility of the development. Importantly, in this location, creative installations could interpret the rest and repose, such as seating areas. The creation of 'place' can be enhanced through cultural and historic narrative of the area and enable strong connections with its context.



# 10, GOLDEN GATEWAY TREE SPECIES

### PARK AND CIVIC SPACE SPECIES 10.1

### STONEHAM STREET AND RESOLUTION DRIVE 10.2



Corymbia calophylla: Marri (large fruiting nuts) 30m+H

Angophora costata: Smooth barked apple 15 – 25m high



Eucalyptus sideroxylon "Rosea" : Red Ironbark 15 -25m H

Eucalyptus sideroxylon "Rosea" : Red Ironbark 15-25m H





### DALY STREET 10.3



Eucalyptus torquate:



Corymbia ficifolia: Red flowering Gum 8-15m

Corymbia calophylla: Marri (large fruiting nuts) 30m+H

Pheonix canariensis: Canary Palm 15m+

Platanus x acerifolia: Spanish or London Plane 20 – 30m

Tipuana tipu: South American <u>Rosewood</u> 7m



## 10.4 CENTRAL STREETS

10.5

# Corymbia ficifolia: Red flowering Gum 8-15m Gum 8-15m Gum 8-15m Gum 8-15m Eucalyptus caesia Eucalyptus torquate Eucalyptus torquate Eucalyptus torquate Corymbia ficifolia: Red flowering Gum 8-15m flower

# LOCAL STREETS PRECINCT 10.6 HIGHLIGHT SPECIES (GATEWAYS)



## 11. IMPLEMENTATION

### 11.1 LANDSCAPE CONSTRUCTION AND MANAGEMENT

The public realm areas in the Golden Gateway area, will primarily be in government implementing the Public Realm Strategy. However, given the significant potential for private redevelopment that is to be generated through the Golden Gateway LSP, it would be possible to recover some or all of the implementation cost from private development ownership; consequently, the City of Belmont will need to assume responsibility for through development contributions or other funding mechanisms.

for the LSP Area. As part of the strategy, a Development Contribution Area (DCA) within LPS 15, under which a Development Contribution Plan (DCP) may be implemented to contribute to the funding of the public infrastructure requirements to facilitate The LSP recommends that the City of Belmont establish an appropriate funding strategy development in the LSP Area would be considered.

accordance with State Planning Policy 3.6 Development Contributions for Infrastructure infrastructure items that would be eligible to be funded under a DCP should be (SPP 3.6) and may include:

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- Land for public open space and community facilities; and
- Landscape treatment for all public realm areas, including local roads.

Furthermore, detailed design of spaces throughout the precinct is encouraged through placemaking opportunities that emerge from community social investment.

## 11.2 WATER MANAGEMENT

sustainability objectives, as well as providing attractive places in which residents and visitors can enjoy, consideration should be given to the conservation of water resources and quality of groundwater. The use of water efficiency measures is encouraged and should promote the investigation of best management practices for irrigation of public Further to the recommendations of Section 7, in order to deliver wider environmental open space. The availability and quality of groundwater within the LSP area is limited at this stage. This will affect the ability of the City of Belmont to irrigate the proposed vegetation within the the future irrigation of vegetation within the POS and public realm areas will need to be artesian groundwater licence, a new irrigation lake or other irrigation strategies will need to be investigated in the future. The City may encourage developers to consider the supplied by other sources. This may include scheme water, stormwater, irrigation (by irrigation of abutting verge vegetation and street trees to ensure the high quality natural public realm areas. Therefore, due to the limitation of groundwater for irrigation purposes, amenity of the public realm is maintained. Alternatively, non-irrigated (dry) landscape may agreement) from the Western Australian Turf Club's (now operating as Perth need to be considered for the public realm areas

### STAGING 11.3

It is not anticipated that the entire landscape masterplan be implemented at once. It is anticipated that the work will be undertaken in stages and progressively rolled out commensurately with the delivery of other key infrastructure, particularly the various road realignments and subdivision works that are required to create the environment for private redevelopment

29

These works would create the framework enabling the public realm works to be implemented. Priority should be given to establishing the road network and lots that frame the POS spine and the primary focus should then be the implementation of this public infrastructure to set the stage for the precincts credentials as a high quality development opportunity.

**Table 1** outlines the staging triggers proposed in the Golden Gateway LSP. The public realm delivery should work in parallel with this program.

Table 1 Golden Gateway LSP Indicative Staging Strategy

COMMENT	No subdivision or development to be approved until the planning framework is in effect.	This work is necessary to establish new road alignments and rationalise cadastral boundaries, prior to development of any land requiring access from those roads either directly or indirectly, or impacted by land assembly requirements.		Development may be permitted to occur prior to upgrades subject to contribution towards upgrade works in cash or kind (where appropriate).
DEVELOPMENT AREAS	Kilns site and adjoining     WATC Admin site     Great Eastern Highway     land	All land referred to in 3 and 4 below	Land north of     Resolution Dve	Land fronting the stated roads
STAGING TRIGGERS	Planning Framework     implementation -     Scheme Rezoning,     Structure Plan approval,     Development     Contribution Plan	2. Realignment and upgrades of Grandstand Rd, Resolution Dve and Stoneham St. Create linear POS reserve.	3. Extension of Matheson Rd to connect to Resolution Dve.	4. Upgrades of Hargreaves, Daly and Grandstand Rd. Landscape works in linear POS.

A Landscape Management Plan will be prepared at each stage of the infrastructure works. Each Landscape Management Plan will address the landscape design, implementation and ongoing maintenance of landscape elements within the site, and should reflect the public realm principles contained in this Strategy.



### Ordinary Council Meeting 26/02/19

Item 12.6 refers

**Attachment 23** 

### Draft Golden Gateway Community Engagement Strategy



### Golden Gateway Local Structure Plan Community Engagement Strategy

### **Project Details**

Project Name	Golden Gateway Structure Plan Community Engagement Strategy
Project Managers	Senior Planner
Project Team Members	Coordinator Design Projects Senior Planner Senior Planner – Projects Planning Officer – Projects
Department	Planning Services

### 1. Project Context

### Brief overview of the context in which the project is being undertaken

The Golden Gateway Local Structure Plan (LSP) has been prepared to coordinate the future subdivision, zoning and development of land generally bound by Great Eastern Highway, the Swan River, Resolution Drive (north), Grandstand Road (north), the southern boundary of Ascot Racecourse, Carbine Road and Hardey Road.

The LSP contemplates a range of mixed land uses (low and high density residential), a local retail centre, commercial land uses and public open space. It also outlines the relocation/construction of roads, land assembly/lot boundary changes and infrastructure upgrades.



Figure 1: Location Plan

The development of the draft Local Structure Plan has included the following consultation stages to date:

• **September 2015:** The City and Department of Planning, Lands and Heritage signed a Memorandum of Understanding (MOU) to prepare the Golden Gateway LSP.

- May 2016: Three separate workshops with City of Belmont Staff, Business/Landowners and Community/Residents.
- May and November 2016: Two online surveys for the wider community to provide additional comments.
- November 2016: One workshop with Business/Landowners and Community/Residents to present three draft concepts.

Council at its meeting of 28 August 2018 deferred the progression of the draft Golden Gateway LSP to formal advertising "to allow further development of a consultation and engagement strategy, which will ensure workshopping, and consideration of implications on the horse racing industry, residents and businesses". This Community Engagement Strategy has been prepared in response to this resolution by outlining the proposed manner in which consultation and advertising will be undertaken for the draft LSP.

### 2. Engagement Objectives

### What are the objectives of the project?

The communication and community consultation objectives of this plan are as follows:

- 1. To successfully communicate to stakeholders information about the project, so they have a clear understanding of the process and project progress.
- 2. To actively consult and with stakeholders to ensure they have opportunities to share feedback and opinions.

### 3. Stakeholders

### **Local Politicians**

Steve Irons MP

Cassie Rowe MLA

Samantha Rowe

### State Ministers

Hon. Rita Saffioti MLA (Minister Transport; Planning; Lands)

Hon. David Alan Templeman MLA (Minister for Local Government; Heritage; Culture and the Arts)

Hon. Peter Charles Tinley AM, MLA (Minister for Housing; Veterans Issues; Youth)

### **Government Departments**

Department of Planning, Lands and Heritage

Main Roads Western Australia

**Public Transport Authority** 

Department of Water and Environmental Regulation

Department of Biodiversity, Conservation and Attractions

Landcorp

Water Corporation

Western Power

### Community

Landowners (Ratepayers) within the Precinct

Occupiers (Residents and Businesses) within the Precinct

Surrounding communities (including Ascot Waters and the Residential and Stables area)

Perth Racing

The Western Australian Turf Club

The National Trust

### 4. Action Plan

### Preliminary Consultation Prior to Formal Advertising Period

### 1. Letter to Western Australian Turf Club

In acknowledgement of the Western Australian Turf Club being a significant landowner in the precinct, a letter will be sent requesting preliminary input on their future intentions of their landholdings within the Golden Gateway LSP area. The City will also seek to encourage face-to-face contact by way of a meeting invitation.

### 2. Request for Extended Advertising Period

The *Planning and Development (Local Planning Schemes) Regulations 2015* – Schedule 2 – Deemed Provisions stipulate that advertising of an LSP can be no less than 14 days and no greater than 21 days, unless otherwise approved by the Western Australian Planning Commission. Due to the complexity of the draft LSP, the City will request an extended advertising period of 28 days to provide the community with additional time for consideration and feedback.

### Formal Advertising Period

### 3. Community Information Session

The City will run a Community Information Session at the commencement of the advertising period to provide an overview of the project to interested parties. The format will include a short introduction by City staff followed by a presentation from the City's planning consultants on the particulars of the project. At the conclusion of the Community Information Session, an opportunity will be provided for questions to be asked by the community.

The Community Information Session will be held outside of business hours to maximise attendance and increase community participation. Details of the Community Information Session will be included in the formal advertising letters to Landowners and Occupiers (see Action 4 below).





Figure 2: Example Community Information Session

### 4. Letters to Landowners and Occupiers

Advertising letters will be sent directly to landowners and occupiers identified within the consultation areas shown below. The letters will specifically state that the City is seeking proactive comment and would welcome suggested alternate proposals with regards to circulation and built form outcomes.



Figure 3: Proposed Consultation Area

As per Action 2, the City will request that the WAPC permit an extended advertising timeframe of 28 days to provide the community with additional time for consideration and feedback.

### 5. Letters to Public Agencies

Advertising letters will be sent to the following public agencies:

- Department of Planning, Lands and Heritage
- Main Roads Western Australia
- Public Transport Authority
- Department of Water and Environmental Regulation
- Department of Biodiversity, Conservation and Attractions
- Landcorp
- Water Corporation
- Western Power

### 6. Brochure

A brochure will be produced and distributed to all landowners, occupiers and public agencies that receive advertising letters. The brochure will be in a format which emphasises key points concisely, using tables, bullet points and/or headings, on a single page printed doubled-sided.

Page 1 of the brochure will detail what the project is about, why it is interesting, how people can get involved and further information sources. Page 2 will include the Draft Local Structure Plan with the main components summarised.

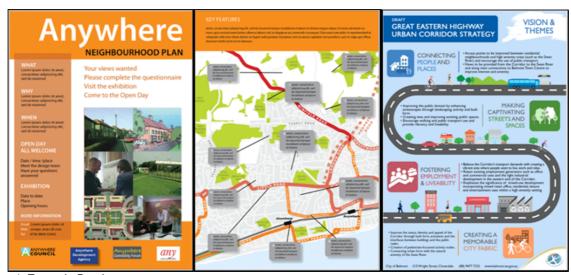


Figure 4: Example Brochures

### 7. Displaying a Notice and Information on the City's Website and Belmont Connect

The City's website and the Belmont Connect page will be updated to include information on the draft LSP. This will include a banner on the homepage of both websites to draw attention to the project. In addition, both websites will include a dedicated page for the project which will include links to the LSP documentation and information on how to make a submission.

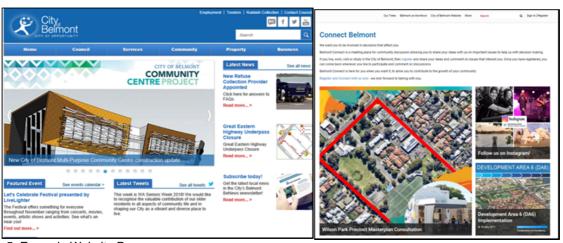


Figure 5: Example Website Pages

### 8. Erecting Advertising Signs

Two advertising signs will be erected in a conspicuous place along Epsom Avenue and at the corner of Stoneham Street and Resolution Drive for the duration of the advertising period. The advertising signs are intended to draw the community's attention to the project and capture any interested parties who may fall beyond the identified consultation area.

It is anticipated that the signs will be approximately 1m x 1.5m in size and designed such that the text is clearly legible and visible from the public realm. The cost of the two advertising signs is expected to range between \$600 to \$2,000.



Figure 6: Example Advertising Signs

Figures 7 and 8 below illustrate the proposed location of each advertising sign.



Figure 7: Proposed Location of Epsom Avenue Advertising Sign

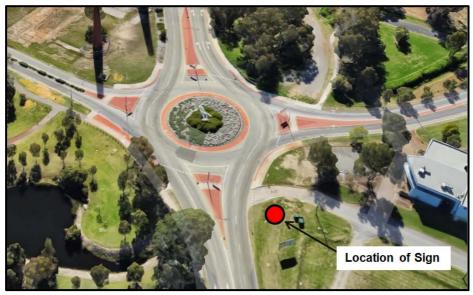


Figure 8: Proposed Location of Resolution Drive//Stoneham StreetAdvertising Sign

### 9. Advertisement in the Southern Gazette Newspaper

A notice will be placed the Southern Gazette newspaper advising of the project and Community Information Sessions. The notice will also explain where further information can be obtained and how submissions are to be made.





Figure 9: Example Advertisement in the Southern Gazette Newspaper

### 10. Social Media Campaign

A social media campaign for the project will be run for the duration of the advertising period. This will involve posts on the City's Facebook and Instagram pages advising of the project, the Community Information Sessions and how submissions can be made on the project.

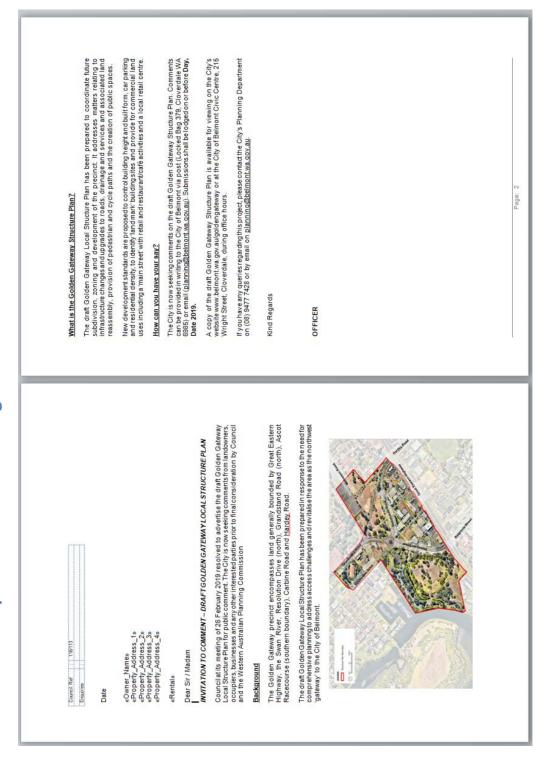
### 5. Project Timeframes

Stage		Action	Timeframe
Formal Advertising Period	1.	Letter to Western Australian Turf Club	March – April 2019
Request for Extended Advertising Period		Request for Extended Advertising Period	March – April 2019
	<ul><li>3. Community Information Session</li><li>4. Letters to Landowners and Occupiers</li></ul>		June – July 2019
			June – July 2019
	5.	Letters to Public Agencies	June – July 2019
	6.	Brochure	June – July 2019
	7.	Displaying a Notice and Information on the City's Website and Belmont Connect	June – July 2019
	8.	Erecting Advertising Signs	June – July 2019
	9.	Advertisement in the Southern Gazette Newspaper	June – July 2019
	10.	Social Media Campaign	June – July 2019

Action	Start	Finish				
Adoption of Community Engagement Strategy and Draft Local Structure Plan for Formal Advertising						
Information Forum – Presentation of Community Engagement Strategy	12 February 2019	N/A				
Agenda Briefing Forum – Presentation of Community Engagement Strategy	19 February 2019	N/A				
Ordinary Council Meeting – Adoption of Community Engagement Strategy	26 February 2019	N/A				
Formal Advertising of Draft Local Structure Plan & Report to WAPC						
Preparation	March 2019	May 2019				
Formal Advertising	June 2019	July 2019				
Assess submissions and Prepare Ordinary Council Meeting Report	August 2019	October 2019				
Information Forum	11 November 2019	N/A				
Agenda Briefing Forum	12 November 2019	N/A				
Ordinary Council Meeting	19 November 2019	N/A				
Report to WAPC	November 2019	N/A				
WAPC Consideration (120 Days)	November 2019	March 2020				

## 6. Advertising Content

# Letter to Landowners / Occupiers / Businesses / Agencies



### **Brochure Content**

Note: Precise layout and design to be confirmed

### **GOLDEN GATEWAY LOCAL STRUCTURE PLAN**

### **Background**

The Golden Gateway precinct encompasses land generally bounded by Great Eastern Highway, the Swan River, Resolution Drive (north), Grandstand Road (north), Ascot Racecourse (southern boundary), Carbine Road and Hardey Road (Figure 1).

The area is recognised as an outstanding location with significant potential for commercial and residential development. Since 2015 the City and the Department of Planning, Lands and Heritage have collaborated on the delivery of the Golden Gateway Local Structure Plan, with a vision to:

"...transform this degraded and fragmented area into a vibrant precinct of residential and mixed use development, with strengthened connections to the Swan River and Ascot Waters...while respecting the area's rich culture and heritage."



Figure 1 - Golden Gateway Precinct

### **Key Proposals**

### Land Use and Built Form

- Provide for mixed residential and commercial land uses focussed between Stoneham Street, Great Eastern Highway, Resolution Drive and Hardey Road through a 'Mixed Use' zoning;
- Transform Daly Street into a 'main street' with a small shopping centre and leafy boulevard;
- Provide for purely residential development south and east of Matheson Road and between Resolution Drive, Grandstand Road and the Ascot Kilns site;

Provide for building heights ranging between 2-15 storeys;

A Development Concept Plan has been prepared to provide an illustration of the development intent of the Golden Gateway Local Structure Plan (Figure 2).



Figure 2 – Development Concept Plan – Note: This graphical representation is indicative only and reflects a long term, mature development scenario; however, it gives an indication of how the public spaces may be developed and the relationship of the public/private interface between the public spaces and new residential areas.

#### **Movement Network**

- Relocate the existing roundabout at the Stoneham Street and Resolution Drive (north) intersection to further north towards Ascot Racecourse, at the intersection of Grandstand Road (north) and Stoneham Street;
- Realign Resolution Drive between Great Eastern Highway and the new roundabout at the intersection of Grandstand Road (north) and Stoneham Street;
- Establish a new signalised intersection at Stoneham Street, Resolution Drive (north) and Daly Street to provide pedestrian access across Stoneham Street.
- Extend Matheson Road through the WA Turf Club land to connect with Resolution Drive (south).
- Extend Grandstand Road (south) through private property to connect with Daly Street.

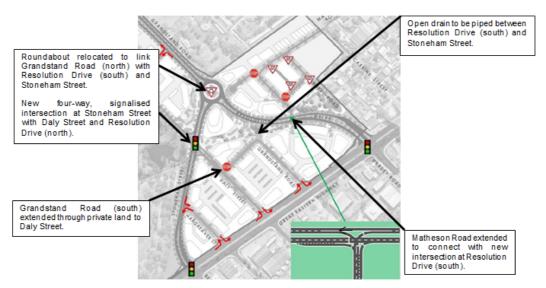


Figure 3 – Movement Network Modifications

#### Public Realm

• Pipe an existing open drain that runs between Resolution Drive and Stoneham Street and create a linear public open space area along the drain alignment to provide connections between the Residential and Stables area and the Swan River.

#### Where to next?

Following advertising of the draft Golden Gateway Local Structure Plan, the following will occur:

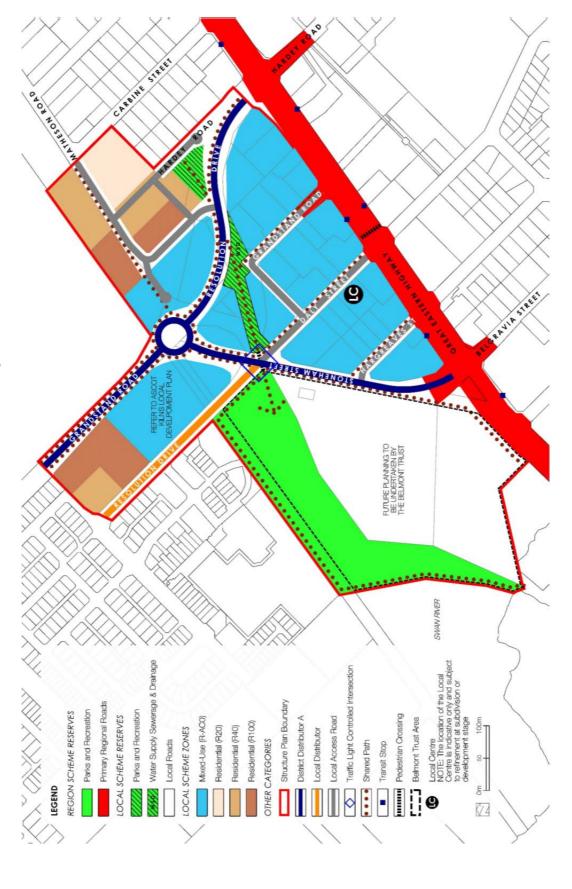
- The City will review all submissions received and consider whether any modifications are required to the draft Golden Gateway Local Structure Plan;
- A report will be prepared to Council and the Western Australian Planning Commission making a recommendation on the progression and determination of the draft Golden Gateway Local Structure Plan;
- The preparation of an amendment to Local Planning Scheme No. 15 to rationalise the zoning of land within the Golden Gateway Precinct;
- The preparation of a Local Planning Policy that establishes specific built form and design controls for the Golden Gateway precinct;
- Further planning will be undertaken separately for the Ascot Kilns and Belmont Trust land via a Local Development Plan.

#### When will development occur?

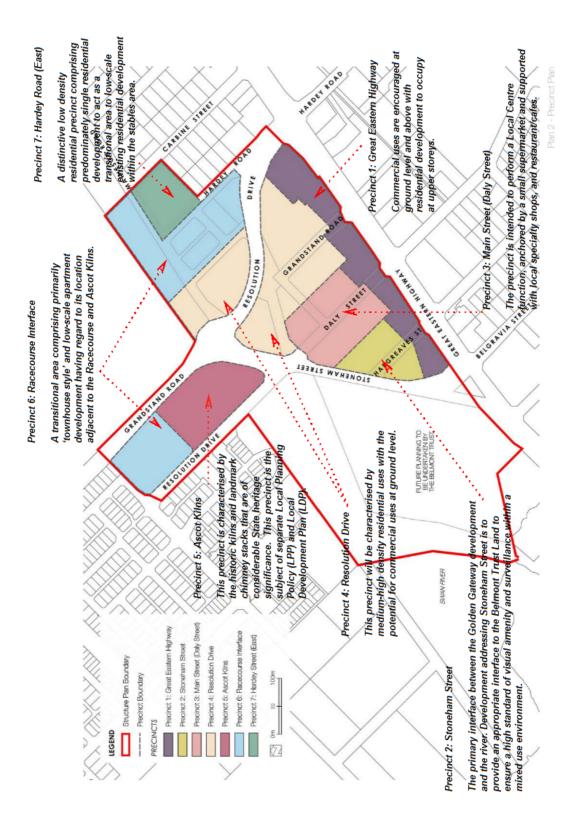
As most of the developable land is fragmented and privately owned, the actual timing and sequence of development will be subject to market demand and individual development intentions. Land within the northern section of the subject land is less constrained by land ownership, with the WA Turf Club and the Western Australian Planning Commission owning the majority of this land and therefore will be more conducive to early development.

Changes to the road network will be a key trigger to enable development throughout most of the precinct. In particular, works necessary prior to development commencement include modifications to Stoneham Street, Resolution Drive, Daly Street, Raconteur Drive and Matheson Road. The timing of these works are unknown.

Attachment 1 – Draft Golden Gateway Local Structure Plan



# Attachment 2 - Precinct Plan





# Ordinary Council Meeting 26/02/19

Item 12.8 refers

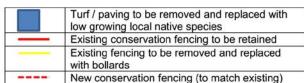
**Attachment 24** 

Proposed Works on Reserve 26219





Attachment 1. Proposed works on Reserve 26219





#### Trees to be removed:

- T1 & T4 Cape lilacs (Melia azedarach)
- T3 Japanese pepper tree (Schinus terebinthifolius)

#### Trees to be retained:

- T2 Tipuana (*Tipu tipuana*)
- T5 European ash (Fraxinus excelsior)

**DBCA Permit condition:** Five replacement trees (*Casuarina obesa* and *Eucalyptus rudis*) to be planted within the vicinity of the trees to be removed.

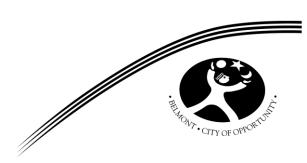


# Ordinary Council Meeting 26/02/19

Item 12.8 refers

**Attachment 25** 

Objections Lodged under Section 9.5



3 Hilton Grove, Ascot WA 6104 14<sup>th</sup> January 2019

To John Christie,

I am writing this letter in the hope you will be able to clarify and answer my questions regarding the property issues that I find myself facing.

I am the owner Margaret Thomas of 3 Hilton Grove, Ascot where I have lived since July 2005. There is a Reserve 26219 (1) Hilton Grove on my side boundary which sits along the Swan River, I have been maintaining The Reserve for the last 13 and half years and so have the previous owners before me since 1960's along with the pre existing Jetty which I have a Licence for. The Belmont City Council were grateful and happy with this arrangement. This Reserve which is under 400sqm is owned by Crown Land and is managed by the BCC.

In 2008-2009 the foreshore of The Reserve 26219 (1) Hilton Grove was upgraded by the BCC this included upgrading the small limestone walls, fixing the broken reticulation (which they are now going to take away) and putting up a fence along the end of Hilton Grove carpark to help with the Security of my home. Also the head of the Security department at BCC came out to check the security at my property.(I live on my own ...my nephew comes and stays to check I am safe.) I have been broken into 3 times and have had prowlers around my house hence Security is a Major issue for me and I am grateful the Belmont Security Petrol comes past six times a day.

I received a letter on the 19<sup>th</sup> May 2017 after a meeting (14<sup>th</sup> May 2017) on the property to outline the changes that were proposed to take place on Reserve 26219 (1) Hilton Grove. This was the first letter I received in regards to the Reserve upgrade.

I Objected to what was being proposed and asked Why this was happening? What was the Benefit of these proposed (new) changes? The cost to the BCC to change and maintain a block under 400sqms that is already taken care of. The resources that would be used for the Upgrade wouldn't they be better spent directed towards more urgent and important projects facing the Shire.

As my Objections were not being Heard I then paid for details under the Freedom of Information in May 2018 in regards to 1-3 Hilton Grove and found that I had not been kept informed by Nicole Davey as the email dated 2nd Nov 2016 shows ...Advice-stabilisation of interface: Hilton Grove (road reserve), 3 Hilton Grove (privately owned) and 1 Hilton Grove (Reserve 26219) Doc No 40 Table 1 Options 1-7. There was a Stabilisation report sent to Miss Davey on the 10th February 2017 giving her different options to her request. Unfortunately the option that was chosen was to cancel my Jetty licence and spend a large amount of BCC ratepayers money to remodel The Reserve. This includes taking up the grassed area, removing trees, the side fence to Hilton Grove carpark (Security issue) and replacing it all with shrubs. I foresee there will then be an ongoing cost to ratepayers for maintenance. I will have a problem with the Security at my home as it will be very exposed to Hilton Grove carpark.

A Permit Application to start works was logged on the 13th April 2018. Approval was given on the 28th May 2018. As you can see from these dates it did not matter that I was Objecting to the Proposed works.

As you can imagine this process has had quite a impact on my physical and mental health. I looked at the option to sell my property but when the Real Estate Agent phoned the BCC, Nicole Davey RECEIVED

supplied her with all the details of what the BCC was proposing to do. The property house price plunged...I cannot afford to sell my property with such a drop in price.

The reason I am writing to you the CEO of Belmont City Council is to inform you that the BCC intend to spend a large sum of money in remodelling under 400sqm's of Reserve 26219. Spending important ratepayers money on re-landscaping and maintaining The Reserve that is already visually pleasant and maintained by myself at No cost to the BCC. Time, Money and effort for what Benefit? Again the Security for me is a Major issue.

I would like to know what the real reasons are? Why this is happening? Why Nicole Davey has asked for my Jetty licence to be Cancelled from Dept of Transport? Why cannot I have the same privileges as my adjoining neighbours do with Jetties on the banks of The Swan River in Ascot? Why they are Not allowing me access to The Reserve to my Jetty? Also if Reserve 26219 Foreshore has to be upgraded what about my neighbour's Foreshores?

So I am asking your Help and Assistance in finding out the cause and actions that are being made against me effecting my Health, my Security, my Property Value that are being proposed after living here 13 and half years. I simply cannot understand why with Garvey Park being so close in proximity to me that you would want to negatively impact my Lifestyle. Taking away my Jetty Licence and doing extensive land works on a Reserve under 400sqm's that joins up to my property. What is the Benefit of all of this? Has anyone thought of the Fire risk? These proposed changes of planting shrubs will be extra close to my property and as BCC doesn't do burn off's this could also be a problem. I already have Garvey Park across the road as a Fire risk.

Thank-you for taking the time to read my letter and I invite you to come to my Property to actually see what my concerns are.

Once again Thank -you Margaret Thomas 0419919425

#### Schedule of Documents - FOI 56/2017-:18/06

Doc. No.	Doc. Date	Doc. Type	Document Description	No. of Pages	Exemption Claimed	Comment
1	11-Aug-93	Letter	Request removal of Front boundary fence	4	3(1)	Personal information about individuals has been redacted
2	16-Aug-93	File Vote	Réquest for fence on boundary of reserve to stay to provide security to 3 Hilton Grove	1	3(1)	Personal information about individuals has been redacted
3	18-Aug-93	Letter	Response to City's letter of 11/08/1993 - Removal of front Loundary fence - request meeting to discuss further	2	3(1)	Personal information about individuals has been redacted
4	31-Aug-93	Letter	Formal request for permission to leave portion of fence	2	3(1)	Personal information about individuals has been redacted
5	01-Nov-93	Meeting Item	Planning Development Committee 01/11/93 Item 8.4 - Unlawful fencing of crown reserve 26219	2	N/A	Public Document
6	10-Nov-93	Letter	Formal request to landowner for fence to be removed from Crown Reserve 26219	1	3(1)	Personal information about individuals has been redacted
7	21-Oct-96	Letter	Request for information in relation 3 Hilton Gove and to any works proposed for adjoining reserve 26219 land	3	3(1)	Personal information about individuals has been redacted
8	01-Nov-96	Letter	Response to letter of 21/10/96 advising of no immediate plans proposed for the reserve 26219	1	3(1)	Personal information about individuals has been redacted
9	31-Jan-00	File Vote	Telephone call relating to ownership of Reserve 26219 Hilton Grove and current developments to the site. City's subsequent inspection, comments and photographs.	- 8	3(1)	Personal information about individuals has been redacted
10	25-Feb-00	Leiter	Request the removal of illegal developments on Reserve 26219 Hilton Grove be removed.	1	3(1)	Personal information about individuals has been redacted
11	15-Mar-00	Letter	Response to request to remove illegal developments, includes report and photographs of the site	15	3(1)	Personal information about individuals has been redacted
12	22-May-00	Note	City Officers Internal notes following site visit	1		Personal information about individuals has been redacted (Due to the difficulty in reading the original handwritten document it has been typed)
13	02-May-01	Internal Memo	Manager of Parks to CEO - Brief description of situation of Illegal construction on the Reserve 26219	1		Personal information about individuals has been redacted
14	01-Aug-01		Complaint in relation to the private use and development of Reserve 26219 Hilton Grove	2		Personal information about individuals has been redacted
15	13-Sep-01		The City's respons a to complainant - Council satisfied with the minor development	1		Personal information about individuals has been redacted

#### Schedule of Documents - FOI 56/2017-:18/06

Doc. No.	Doc. Date	Doc. Type	Document Description	No. of Pages	Exemption Claimed	Comment
16	24-Sep-01	Leiter	Response to enquiry relating to possibility of adverse possession of Crown Reserve 26219 Hilton Grove.	1	3(1)	Personal information about individuals has been redacted
17	05-Oct-01	Letter	Request removal of unauthorised fence to allow public access to Reserve.	1	3(1)	Personal information about individuals has been redacted
18	18-Oct-01	Lefter	Response to letter 5/10/01 - City satisfied that access to reserve will not be denied.	1	3(1)	Personal information about individuals has been redacted
19	25-Feb-02	Letter	Expression of interest to purchase the Reserve 26219 Hilton Grove	2	3(1)	Personal information about individuals has been redacted
20	05-Mar-02	Leiter	Response to expression of interest to purchase the Hilton Grove Reserve 25/02/02	2	3(1)	Personal information about individuals has been redacted
21	18-Jun-03	Photo graphs	Photo's taken of Reserve 26219 by City officers during a site inspection	4	N/A	Released in full
22	30-Jun-03	Leiter	Notification to owner of 3 Hilton Grove advising that no further developments to be undertaken on Reserve 26219 without Council approval	1	3(1)	Personal information about individuals has been redacted
23	30-Jun-03	Letter	Response to enquiry regarding development and maintenance of Reserve 26219	1	3(1)	Personal information about individuals has been redacted
24	04-Aug-03	Letter	Mail merge to surrounding land owners seeking comment on disposal of Reserve 26219	18	3(1)	Personal information about individuals has been redacted     Page No. 4 has been left blan in the original document
25	04-Aug-03	Letter	Mail merge Part 2 to surrounding land owners seeking comment on disposal of Reserve 26219	5	3(1)	Personal information about individuals has been redacted
26	13-Aug-03	Letter	Request for answers to questions in relation to the proposed disposal of Reserve 26219 Hilton Grove	6	3(1)	Personal information about individuals has been redacted
27	22-Aug-03	Letter	The City's respons to residents questions in letter of 13/08/2003 regarding community consultation and proposed disposal of Reserve 26219	9	3(1)	Personal information about individuals has been redacted (Attachment 1 - as provided in docurrent 24 and 25)
28	14-Aug-03	Мєто	Memo from Acting CEO to the Mayor - The Public Open Space Strategy Forum	3	3(1)	Personal information about individuals has been redacted
29	25-Aug-03	Lefter	Response to the Department of Local Government & Regional Development telephone conversation of 21/08/2018 - Provision of background information concerning Reserve 26219 Hilton Grove	7	3(1)	Perso al information about Individuals has been redacted (Attachment 4 - as provided in docurrent 24 and 25) (Attachment 6 - as provided in docurrent 22)
30	17-Sep-03	Мєто	Memo to the Acting CEO regarding community consultation, rezoning and sale of the Hilton Grove parkland	1	3(1)	Personal information about individuals has been redacted

#### Schedule of Documents - FOI 56/2017-18/06

Doc. No.	Doc. Date	Doc. Type	Document Description	No. of Pages	Exemption Claimed	Comment
31	10-Nov-03	Report	Report from Planning & Development Committee Meeting 10/11/2003 regarding not pursuing the disposal of the Reserve	9	N/A	Released in full
32	25-Nov-03	Letter	Mail Merge - The City's response to submissions received regarding pursuing the disposal of the Reserve 26219	3	3(1)	Personal information about individuals has been redacted
33	25-Nov-03	Memo	Memo to Manager Technical Services from Director Development advising Council's decision and request for action relating to part 5 - possibilities of a landscaping plan for public open space utilisation and signage to be carried out	3	3(1)	Personal information about individuals has been redacted (Attachments - as provided in document 34 & 35)
34	26-Nov-03	Letter	Advising Department of Planning & Infrastructure of City's investigation into possible disposal of Reserve 26219. Request for advice regarding the future of reserve 26219 (1) Hilton Grove	3	3(1)	Personal information about individuals has been redacted (Attachment 1 - PDC Minutes 10/11/03 and Attachment 2 - Public Submissions received as provided in document 31)
35	26-Nov-03	Letter	Advising Swan River Trust of City's investigation into possible disposal of Reserve 26219 and request for determination of necessary riverbank restoration work required	3	3(1)	Personal information about individuals has been redacted (Attachment 1 - PDC Minutes 10/11/03 as provided in document 31)
36	19-Dec-03	Fax	Real Estate Enquiry requesting clarification of Councils Intentions of Reserve 26219 Hilton Grove	1	3(1)	Personal information about individuals has been redacted
37	14-Jan-04	Letter	Response to Real Estate request for clarification of Councils resolution in relation to it's intentions for Reserve 26219 Hilton Grove	2	3(1)	Personal information about individuals has been redacted
38	28-Feb-11	Feedback	Complaint relating to the access of Reserve 26219 Hilton Grove	1	3(1)	Personal information about individuals has been redacted
39	26-Jun-15	Emails	Internal emails investigating the ownership and licencing of the Jetty	4		Personal information about individuals has been redacted
40	02-Nov-16	Email & Photos	The City's request for advice for stabilisation and re-design of interface: Hilton Grove (road:reserve), 3 Hilton Grove (privately owned) and 1 Hilton Grove (Reserve 26219)	4		Personal information about individuals has been redacted
41	16-Nov-16	Email	Response to request to provide advice and fee estimate - Stabilisation and design of Interface at Hilton Grove Reserve	7		<ul> <li>Personal information about individuals has been redacted</li> <li>Commercial information has been redacted</li> </ul>
42	08-Feb-17	Email	Update from the CEO to the Mayor in relation to works planned for Reserve 26219 Hilton Grove	1		Personal information about individuals has been redacted

#### Schedule of Documents - FOI 56/2017-18/06

Doc. No.	Doc. Date	Doc. Type	Document Description	No. of Pages	Exemption Claimed	
43	10-Feb-17	Email	Advice received regarding Stabilisation and redesign of interface - Hilton Grove road reserve, 3 Hilton Grove and 1 Hilton Grove	17	3(1)	Pages 1 - 3 have been redacted as they are outside the scope of the claim     Personal information about individuals has been redacted.
44	19-May-17	Letter	Letter to Mrs M Thomas - Summary of meeting held 14/05/2017 and options for consideration - Structures installed on Reserve 26219 (1) Hilton Grove, Ascot	6	3(1)	Personal information about individuals has been redacted
45	26-Jun-17	Letter	Response from Mrs M Thomas to the City's letter of the 19/05/2017 - Options for Structures Installed on Reserve 26219 (1) Hilton Gr	1	3(1)	Personal information about individuals has been redacted
46	31-Jul-17	Email	Internal emails regarding re-alignment of the MRS boundary between 1 Hilton Grove and 3 Hilton Grove	1	3(1)	Personal information about individuals has been redacted
47	14-Jan-18	Email	Emails from and to the Dept. of Planning in relation to potential re-alignment of the reserve boundary	6	3(1)	Personal information about individuals has been redacted
48	06-Mar-18	Email	Advice from Dept. of Biodiversity regarding transfer of jetty ownership to the City or structure be removed	24	3(1)	Personal information about individuals has been redacted
49	04-Apr-18	Letter	Updated advice to Mrs M Thomas on City's intentions following feedback from State government agencies	2		Released in full as this letter was originally sent to the applicant
50	13-Apr-18	Email	Permit Application for proposed works, Hilton Grove Reserve 26219 - Dept. Biodiversity Conservation and Attractions	6		Personal information about individuals has been redacted
51	23-Apr-18	Email	Response from the CEO to enquiry in relation to the disposal and maintenance of the land	2		Personal information about individuals has been redacted
52	14-May-18		Response to Ms M Thomas - Notice of intention to proceed with works - Reserve 26219 (1) Hilton Grove Ascot	1		Personal information about individuals has been redacted
53	28-May-18		Permit P12188 - Remediation and Landscaping of Foreshore Reserve 26219 - Lot 7134 (1) Hilton Gr	2	34.5	Personal information about ndividuals has been redacted

#### Form 4

[reg. 33(1)]

#### Local Government Act 1995

Local Government (Functions and General) Regulations 1996

12 Sections and Generally Regulations 1990
OBJECTION UNDER SECTION 9.5 OF THE ACT
To the (1) Cheef Executive Officer at the Belovent City Course of (3) Butter frame, Ascot 6104
hereby object to the (4) The Works Proposal on the Reserve  26219 (1) Hilton Sprace  to (5) Letter from BCC dated 24th Oct 2018  proposal of Works
The grounds of my objection are as follows:  (6) Security to hyperly - Express to Hellow Grove Carpark.  Effect the Value of thy Property  Jifestyle, Health 2 Stress of data Security.
In support of my objection I attach the following:
(7) A letter
here these works to be done
I have Not been Kept informed - Hence the App for Freedom of Info
Dated the 14th day of January 20.18
(8) Like Tlanos
Person objecting
(1) name of local government (2) full name of person objecting (3) postal address of person objecting (4) identify decision to which objection is made (5) give details of decision

- (5) give details of decision
  (6) give details of grounds of objection
  (7) plans, specifications, letters, notices, or other documents (if appropriate)
  (8) signature of person objecting

CITY OF BELMONT

215 Wright Street (LMB 379) Cloverdale Western Australia 6105

All communications

to be addressed to:
The Chief Executive Office
LMB 379 Cloverdale

Western Australia 6985

Council Ref

30/015, 30/001

Customer Ref

Enquiries

N. Davey

24 October 2018

Mrs Margaret Thomas 3 Hilton Grove ASCOT WA 6103

**Dear Margaret** 

#### RESERVE 26219 (1) HILTON GROVE, ASCOT

Following previous correspondence regarding Reserve 26219 (1) Hilton Grove, Ascot I would like to provide a progress update and advice of works to be undertaken by the City.

As the City has not received any notification from the State Administrative Tribunal that an objection has been lodged, we will be proceeding with the proposed works as shown on the enclosed plan.

The City's Arborist has completed an assessment of the trees on the reserve and all are in good condition. However, three of the trees identified are potential environmental weeds; a Japanese pepper and two Cape Lilacs. Due to their proximity to the Swan River and previous comments from the Department Biodiversity, Conservation & Attractions the City will first stem inject, then remove and stump grind these trees (DBCA approval pending). Three replacement trees will be planted.

Following tree removal, the area between the verge and the reserve will be re-graded, with the existing side fencing removed and replaced with bollards. The garden beds on this area of the reserve will be affected by these works as will the paving around the *Tipuana tipu* (Pride of Bolivia) tree.

Additional conservation style fencing will be installed along the top of the foreshore embankment to prevent access to the unsafe steps and jetty and the City will write to the Department of Transport requesting the jetty licence is revoked.

The irrigation and turf on the reserve will be removed and area replanted with low growing shrubs. Any structures on the reserve (i.e. seating / paving) or plants in the garden bed adjacent to the road verge will need to be relocated onto your property. The garden beds on the boundary adjacent 61 Fauntleroy Avenue will not be affected by the works.



An approximate schedule for implementation is as follows:

Letter to Department of Transport	November 2018	
Stem injection	February/March 2019	
Tree removal and stump grinding	April 2010	
Removal of fencing, irrigation, turf, garden bed and paving, site re-grading and fence and bollard installation	May 2010	
Revegetation	June 2019	

The timing of works has been scheduled to enable planting to occur during suitable weather conditions, immediately following re-grading works. The City's A/ Environmental Officer, Darren Robins will provide notice via email prior to commencing each stage of works.

If you wish to discuss the proposed works or arrangements to relocate items onto your property, please do not hesitate to contact myself on 9477 7295 or email nicole.davey@belmont.wa.gov.au.

Yours sincerely

**NICOLE DAVEY** 

A/MANAGER PARKS & ENVIRONMENT

#### Form 4

[reg. 33(1)]

#### Local Government Act 1995

Local Government (Functions and General) Regulations 1996

OBJECTION UNDER SECTION 9.5 OF THE ACT	4.4
To the (1) Chief Executive Office at the Chy of I, (2) Hargaret R. L. Thomas of (3) 3 Helton lynn, Assot 6104	Belmont.
hereby object to the (4) Concellation of My Jetty Lieux to (5) Remained Confirmation of This from 5.0 T	26/11/2018
The grounds of my objection are as follows:  (6) The Jetty is fre escirtury from 1960's and bear well municipally freezeway form.	Les
In support of my objection I attach the following:  (7) DOC 43 104 Feb 2017  2008-2009 Previous Torsolore Upgrade-Wo Objection to the Concellation of the Jetty  Correspondence  Dated the 14th day of January 2019  (8) Well Manage	Foone
Person objecting	
(1) name of local government	
(2) full name of person objecting	
(3) postal address of person objecting	
(4) identify decision to which objection is made	
(5) give details of decision	
(6) give details of grounds of objection	
(7) plans, specifications, letters, notices, or other documents (if appropriate)	

- (8) signature of person objecting



#### Coastal Infrastructure Business Unit

26 November 2018

Our ref:

LM4511

Enquiries:

Ron Zappara

Ms Margaret Thomas 3 Hilton Gr ASCOT WA 6104

Dear Ms. Thomas

## JETTY LICENCE ADJACENT TO RESERVE 26219 HILTON GROVE, ASCOT

The Department of Transport (DoT) has been advised by the City of Belmont (CoB) as land manager of Reserve 26219 to which your jetty is adjacent, that it is not supportive of the structure being in use and as such as requested that DoT cancel your jetty licence.

DoT was also advised that the City has communicated with you in this regard.

Based on the above circumstances, DoT has no recourse but to cancel your licence and request that you remove the structure as soon as possible.

Please be advised that you will be required to contact and seek approval from the Department of Biodiversity, Conservation and Attractions, Rivers and Estuaries Division (formerly Swan River Trust) prior to removing the structure.

If you have any further queries on this matter, please don't hesitate to contact Ron Zappara, Manager Property Services on 9435 7646.

Yours sincerely

Yi-Lin Na

Property and Maritime Licensing Offier

Coastal Facilities Management

CC City of Belmont

DBCA

Document 40

## Clause 3 (1) Matter is exempt matter if its disclosure would reveal personal information about an individual (whether living or dead)

From:

Nicole Davey

Sent:

2 Nov 2016 07:47:32 +0000

To:

Subject:

Advice- stabilisation & re-design of interface: Hilton Grove (road reserve), 3

FOI

Document Release

13

Hilton Grove (privately owned) and 1 Hilton Grove (Reserve 26219)

Attachments:

Summary.pdf



As mentioned, if you could provide a fee estimate to provide technical advice for the following, that would be great.

We would require the advice by Christmas, if possible.

#### Scope

The City is seeking technical advice for potential redesign of the interface between Hilton Grove (road reserve), 3 Hilton Grove (privately owned) and 1 Hilton Grove (Reserve 26219), including potential enhancement/ extension of the existing stabilisation area (refer attached).

The objectives of the re-design will be to:

- Replace the existing steps with a safe structure providing access to the private jetty
- Improve access between the road reserve and Reserve 26219, for maintenance of the existing stabilisation area
- Possible enhancement/ extension of the existing stabilisation area
- Potential removal of garden beds, fencing and other structures constructed on Reserve 26219

The City does not intend to encourage public use of Reserve 26219, however wishes to address the current lack of delineation between private vs public land. However, in doing so the security and privacy of the resident of 3 Hilton Grove is a key consideration.

#### **Deliverables:**

- Review success of existing stabilisation area and provide general recommendations for enhancement/ improvement
- Sketch/ brief summary of suggestions for re-design of interface

#### **Background information**

Reserve 26219 (1) Hilton Grove, Ascot is a Crown Reserve vested in the City of Belmont (zoned Parks & Recreation).

3 Hilton Grove Ascot is zoned Residential and privately owned.

Reserve 26219 has several structures located on it, installed by previous owner/s of 3 Hilton Grove with permission from the City. These include garden beds, paving, fencing and steps leading to a private jetty.

Document Set ID: 2858687 Version: 1, Version Date: 02/11/2016

The jetty is licensed with Dept Transport to the current owner of 3 Hilton Grove. Dept Transport have advised they can revoke this license if the adjoining landowner (Minister for Lands/ City of Belmont) does not agree to the connection.

The steps providing access to the jetty are not constructed to standards and present a potential public safety and liability risk to the City. As such, they require either removal or replacement with an alternative structure.

In addition, the current access to Reserve 26219 for maintenance is very limited due to the presence of:

- Garden beds
- Trees (Including Japanese peppers)
- Change in level between Hilton Grove and the property



#### **NICOLE DAVEY COORDINATOR- ENVIRONMENT**



City of Belmont, 215 Wright Street, Cloverdale, WA 6105 Postal address: Locked Bag 379, Cloverdale WA 6985

Phone - (08)	Fax - (08) 9478 1473	Mobile -	
Email -	@belmont.wa.gov.au		
Web - www.belmo			

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Document Set ID: 2858687 Version: 1, Version Date: 02/11/2016





Figure 1. View of steps from river

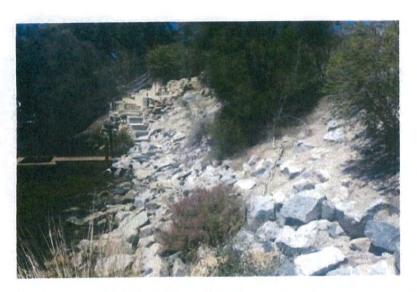


Figure 2. Side view of steps & jetty (looking upstream)





Figure 3. Cadastral/ lot boundary of 1 Hilton Grove, Ascot showing location of steps and paved and lawn areas.



Figure 4. Extent of area that could be re-graded/re-designed

Document 41

FOI Document Release

10

From: Sent:

16 Nov 2016 10:49:09 +0800

To:

Nicole Davey

Subject:

RE: Advice- stabilisation & re-design of interface: Hilton Grove (road reserve), 3

Hilton Grove (privately owned) and 1 Hilton Grove (Reserve 26219)

Attachments:

SP26-2017 CoB - 1 and 3 Hilton Grove.pdf

Hi Nicole,

Thank you for the opportunity to provide a fee estimate for technical advice for the area around 1 and 3 Hilton Grove. Please find our fee estimate attached. I have submitted our proposal under to allow access for you under the WALGA preferred supplier for Natural Area Management and Environmental.

Please let me know if you have any queries or requests for change.



From: Nicole Davey [mailto:

@belmont.wa.gov.au]

Sent: Wednesday, 2 November 2016 3:48 PM

Subject: Advice- stabilisation & re-design of interface: Hilton Grove (road reserve), 3 Hilton Grove

(privately owned) and 1 Hilton Grove (Reserve 26219)

Hi

As mentioned, if you could provide a fee estimate to provide technical advice for the following, that would be great.

We would require the advice by Christmas, if possible.

#### Scope

The City is seeking technical advice for potential redesign of the interface between Hilton Grove (road reserve), 3 Hilton Grove (privately owned) and 1 Hilton Grove (Reserve 26219), including potential enhancement/ extension of the existing stabilisation area (refer attached).

The objectives of the re-design will be to:

- Replace the existing steps with a safe structure providing access to the private jetty
- Improve access between the road reserve and Reserve 26219, for maintenance of the existing
- Possible enhancement/ extension of the existing stabilisation area

Document Set ID: 2878966 Version: 1, Version Date: 30/11/2016

Document Release

Your Ref: P0064478
Our Ref: SE049-01-Rev1

Doc 43

Nicole Davey

Nicole Davey
City of Belmont
LMB 379 Cloverdale WA 6105

Dear Nicole,

## ADVICE - STABILISATION & RE-DESIGN OF INTERFACE: HILTON GROVE (ROAD RESERVE), 3 HILTON GROVE (PRIVATELY OWNED) AND 1 HILTON GROVE (RESERVE 26219)

This document provides the City of Belmont (the City) with advice regarding the stabilisation and redesign of the interface between the privately owned 3 Hilton Grove, the publicly owned 1 Hilton Grove (Reserve 26219) and the Hilton Grove Road Reserve (Figure 1). The site has foreshore stabilisation works not constructed to design, unsafe stairs on Reserve 26129 to access a jetty licenced to 3 Hilton Grove, restricted access to Reserve 26219 for the public and for the City for maintenance.

Existing foreshore works are reviewed and opportunities for modifications to stabilise the foreshore and provide safe access are presented. This includes discussion of:

- Review, and potential enhancement, of existing stabilisation works on Reserve 26129;
- Safe access to the private jetty;
- Improving access from the road reserve to Reserve 26129 for maintenance of foreshore stabilisation works;
- Aesthetic features on Reserve 26129 and change in foreshore levels between 3 Hilton Grove,
   Reserve 26129 and the road reserve; and
- Public access to Reserve 26129 and the impact on owners of 3 Hilton Grove. It should be noted that
  the City does not intend to encourage public use of Reserve 26219. Improvements for delineation
  between public and private land are considered.

This document has been prepared to facilitate discussions between City staff, City councillors and the owners of 3 Hilton Grove before pursuing any concept or detailed designs.

#### 1 SITE DESCRIPTION

The site is a high (+5mAHD) modified foreshore that has had a history of erosion in response to historic modifications to the channel, banks and to landward. It was stabilised with a revetment in 2008/9. An overview of site modifications is included in Table 2 and Figure 6.

Contemporary factors contributing to erosion include:

- · widening and shallowing of the river channel in response to dredging;
- bank scour from overbank runoff;
- wind wave and boat wakes within the range of water level fluctuations;
- local bed scour from boat mooring; and
- transfer of erosion stress from adjacent hard structures.

#### 2 REVIEW OF EXISTING STABILISATION WORKS

The existing revetment on Reserve 26219 was not constructed to specification, but is performing satisfactorily for most non-flood events. There is no high-value infrastructure or assets at risk if damage or



failure of the revetment occurred in a storm event. Temporary fencing could be installed to reduce the City's public liability if failure occurred. No significant improvements are required now for the revetment to improve bank stability. Public liability is the most significant concern, related to the stairs to the jetty.

The existing foreshore stabilisation works on Reserve 26219 are a granite revetment that transitions to a limestone revetment downstream and a series of retaining walls and stair access to a private jetty upstream that are not constructed to standards (Figure 7). Further upstream the stairs transition to private property on 61 Fauntleroy Avenue with an inclined bank stabilised with synthetic turf. The upper bank on Reserve 26219 has been planted with native vegetation and stabilised with bioengineering with lawn to landward.

#### Granite revetment

The revetment was designed to have a two rock width of primary armour over a 0.8m width filter layer underlay by geotextile (Figure 2). The primary armour was to be 0.3t to 1.0t, with the median weight 0.4t and the filter layer to be 0.1m to 0.4m diameter rocks with median diameter 0.25m. It is assumed the original design was for limestone rocks, with primary armour diameters of 0.5 to 0.8m, with median diameter 0.6m. The revetment extended >3m of the existing bank to achieve a face slope of 1V:1.5H between +2.05m and -1mAHD. The upper bank was to have a gradient of 1V:5H to allow revegetation.

The upstream 45 lineal metres of revetment on Reserve 26219 was not constructed to specification, with the Swan River Trust issuing a stop works order mid-way through construction (Table 2; Nicole Davey pers. com). Granite was used in place of limestone, a gabion mattress was used as a base layer under the granite and the revetment was built to a higher elevation than specified.

By the provided advice on how to modify the revetment (Figure 3). The revetment was to be maintained, with a 0.3m layer of soil on top, covered with coir logs and coir netting, with soil watered into the rock voids before planting. No as-constructed drawings were provided. No rationale is available for why the gabion mattress was constructed under the primary armour, with the gabion mattress visible near the jetty.

The revetment is providing adequate function for erosion control for most conditions and will behave as a self-stabilising flexible rock structure (Figure 4; Figure 7H). The granite primary armour is undersized (≈0.25m median diameter of higher density granite) and poorly interlocked. As rocks shift at the toe through rolling, sliding or settling; more rocks will roll down the face to settle lower on the structure. If the gabion mattress under the primary armour was installed well, this will provide increased stability for the bank if the primary armour rocks move as the gabion mattress will restrict the erosive capacity of waves and surface runoff on the bank. Overall the toe provides the most critical area of weakness in the (Figure 3) it appears that the extra toe rocks to revetment. From the sketch by prevent sliding were not installed and it cannot be confirmed the structure was embedded to -1mAHD. An exposed gabion mattress at approximately 0mAHD (Figure 7G) indicated insufficient embedment at the eastern extent of the revetment, near the jetty, where the structure transitioned to the existing retaining wall and concrete paver steps (upgraded in 2007). The revetment toe at the transition to the retaining wall and stairs is a local weak point (Figure 7G), in part due to insufficient embedment and the apparent shifting of granite primary armour to provide a connection from the concrete paver stairs and the 2015 upgraded jetty.

The surface layer of soil has eroded because the slope is steeper than 1V:3H slope which is the maximum steepness for revegetation on an upper bank with no wave action (BMP Section 2.2.2; SRT 2009). Erosion from surface runoff is expected to continue as the jute matting has degraded on the steep slope (Figure 4; Figure 7F). As the loss of soil continues it is expected some shrubs and plants on the steep slope may not survive in the long-term. Vegetation and soil provide an aesthetic and ecological value, and the revegetation component is not required for erosion mitigation. There is evidence of failed remnant





bioengineering at the toe, with wooden stakes and a few remnant sedge (Figure 7H), that do not impact over stability of the revetment.

#### Downstream transition to limestone revetment

Immediately downstream the gabion revetment transitions to the larger rock limestone revetment (Figure 6), constructed following the original design (Figure 2). There is adequate connectivity between the two different rock types and it appears the two revetments abut one another with no overlay.

#### Stairs and retaining walls

The eastern extent of Reserve 26219 are stairs and rendered rock retaining walls constructed around 2006-2007 (Figure 5; Figure 7D - G). No construction details are available. The stairs are a series of 19 steps and landings constructed out of brick and concrete pavers (600mm x 600mm x 55mm) with grout used in some places. The steps are not constructed to standards (AS1657-2013 Fixed platforms, walkways, stairways and ladders—Design, construction and installation) in terms of risers, goings and consistency. The stairs have:

- Risers ranging from 120mm to 380mm when they should range 130mm to 225mm and be uniform within the same flight (±5mm);
- Goings ranging from 200mm to 1380mm when they should range 215mm to 355mm and be uniform within the same flight (±5mm);
- All steps exceed the recommended combination of twice the riser plus the going, which is supposed to be between 540mm and 700mm; and
- Many unstable pavers that are loose and are not level, providing a trip or fall hazard.

A series of six retaining walls are located adjacent to the stairs ranging from 280mm to 1820mm height. One wall continues into a 1m high rock wall fronted by a landing used for seating (table and chairs). The retaining walls appear to be stable with no visible evidencing of sliding or rotating.

The retaining walls extend further riverward than the adjacent bank and were constructed before the granite rock revetment. It appears that the retaining walls transitioned to the eroding bank with loose granite rock (Figure 5). Subsequently the revetment was constructed over this transition area. Surface runoff off the upper bank is focused between the retaining wall and granite revetment with a localised loss of soil and less vegetation coverage.

At the base of the stairs there are trees under stress with exposed roots surrounded by brick rubble (Figure 7D and G). Erosion stress occurs due to surface runoff scour from down the stairs; transfer of stress due to adjacent rock structures on 61 Fauntleroy Avenue and the granite revetment; and from propeller scour from boats accessing the two adjacent private jetties.

#### Upstream transition to 61 Fauntieroy Avenue

Reserve 26129 transitions immediately upstream to the privately owned 61 Fauntleroy Avenue with HWM title (Figure 6; Figure 7F). The bank is an approximately 1V:2H slope stabilised with synthetic turf. The graded bank is lower in elevation than the steps. A chainmesh fence between the properties restricts any public access to 61 Fauntleroy Avenue, with the fence inclined towards 61 Fauntleroy due to the weight from a climbing plant (now removed).

#### Upper bank

The upper bank was remediated and revegetated in 2011-2012, designed to have a 1V:5H slope, and placed on the granite revetment (Figure 4; Figure 7C). The material used to fill the upper bank was mixed according to the ratio of 4 (coarse river sand): 1 (Eclipse Soil Aquamor) with the sand sourced from the Department of Planning's stockpile on Waterview Crescent). The soil appeared to be quite hard, which may be limiting the capacity for vegetation to establish. In addition, the area adjacent to the grass is receiving sprinkler runoff from the grass that may be impacting the health of the native vegetation due to





overwatering and excessive nutrients. There is a steeper gradient between the revegetated area and the grass. Coir logs and jute matting are still present on the upper bank.

Additional item – access for fishing on limestone revetment

Minor commentary is provided regarding access to the limestone revetment for fishing. The area landward of the limestone revetment was fenced to allow time for vegetation to establish, with people presently climbing the fence near the car park on Hilton Grove or at the downstream end of the fence to access the terraced log walling. It is recommended to remove two sections of fence in these areas where people are already accessing the site, that is near the car park and at the downstream end. These access paths would require ongoing top-up with sand by City of Belmont as a maintenance item.

#### 3 OPTIONS

A range of options are discussed with regard to enhancing existing stabilisation works, replacing existing steps with safe access to the private jetty, improving access to Reserve 26219 and modifications to aesthetic features on Reserve 26219.

#### 3.1 OPTIONS TO ENHANCE EXISTING STABILISATION WORKS

Options to enhance existing stabilisation works depend on what is selected for the provision of safe access to the private jetty on Reserve 26219 (Table 1; Section 3.2). At present, it is difficult to access the site with machinery to undertake substantial works on the granite revetment or to access the lower revetment face easily to top-up soil levels and undertake additional revegetation.

It is recommended to monitor the integrity of the revetment toe, the lower part of the revetment in the active hydraulic zone (subject to regular wave action) and at the upstream transition to the stairs and retaining walls. Monitoring should be undertaken annually. If the stairs and retaining walls are maintained in their existing arrangement (Table 1; Section 3.2), some additional granite rocks may be required at the base of the stairs and between the stairs and the jetty. This would only be required if progressive damage was observed in the annual monitoring.

Additional revegetation on the steep slope could reduce erosion from surface runoff, with vegetation focused on creepers or other low elevation plants.

Stabilisation works may be required at the base of the stairs if the trees were to be maintained. An arborist should be consulted about the likelihood of the trees surviving in this area before considering any works.

## 3.2 OPTIONS TO REPLACE EXISTING STEPS WITH SAFE ACCESS TO THE PRIVATE JETTY

Seven options are presented in Table 1 for managing or replacing the existing steps that access the jetty licenced to 3 Hilton Grove. The commentary provided regarding public liability should be confirmed with an insurance expert and considered when assessing potential inclusions in the preparation of a deed of indemnification for the owners of 3 Hilton Grove.

No single option is recommended by due to the complexity of liability and source of funding at this site with mixed public and private use. All options require consideration of:

- Constructability including access by machinery to undertake the works;
- Surface runoff management;
- Improved bank stabilisation at the toe; and
- Who is responsible for maintenance, removal and reconstruction of any proposed works in Table 1.





#### Table 1: Options for access to jetty

Options	Public liability	Requirements	Cost	Commentary	
Remove stairs and use neighbour stairs  PL deferred to 61 Fauntlerov Ave		Agreement with 61 Fauntieroy Avenue owners to allow access, at no cost to them. Platform from base of stairs at 61 Fauntieroy Ave to 3 Hilton Grove jetty. 2 lockable gates to 61 Fauntieroy Avenue at top and base of stairs. Remove stairs and replace with regraded synthetic turf to match the bank at 61 Fauntieroy. Tie-in needs runoff management.	Mod.	Complex liability and maintenance costs with regard to the stairs on 61 Fauntleroy to be resolved between owners of 61 Fauntleroy Avenue and 3 Hilton Grove.	
2. Block stairs and use neighbour stairs	owners.	Agreement with 61 Fauntieroy Avenue owners to allow access, at no cost to them. Platform from base of stairs at 61 Fauntieroy Ave to 3 Hilton Grove Jetty. 2 lockable gates to 61 Fauntieroy Avenue at top and base of stairs. Fence off stairs on Reserve 26219 at top and base to restrict access.	Low	Complex liability and maintenance costs with regard to the stairs on 61 Fauntleroy to be resolved between owners of 61 Fauntleroy Avenue and 3 Hilton Grove. Low aesthetic value.	
Reconstruct stairs that satisfy AS1657-2013 and maintain existing retaining walls	City	Remove existing stairs.  Stabilise bank under the new stairs using pinned geotextile.  Maintain retaining walls.  Stumped stairs that satisfy AS1657-2013 similar to those on 61 Fauntieroy Ave.  Deck to connect base of stairs to jetty  Reconstruct fencing between Reserve 26219 and 61 Fauntieroy Ave	High	Capital and maintenance costs to be	
<ol> <li>Remove retaining wall and stairs, extend stabilisation works and reconstruct stairs that satisfy AS1657-2013</li> </ol>	City	Remove existing stairs and retaining walls. Regrade bank.  Stabilise bank under the new stairs using pinned geotextile.  Stumped stairs that satisfy AS1657-2013 similar to those on 61 Fauntleroy Ave.  Extend existing revetment to western extent of stairs with careful tie-in required.  Deck to connect base of stairs to jetty  Reconstruct fencing between Reserve 26219 and 61 Fauntleroy Ave	Very high	resolved between City and 3 Hilton Grove	
5. Extend stabilisation works with biscuit rock limestone		Remove existing retaining walls and stairs. Regrade bank. Install biscuit rock limestone revetment for bank stabilisation with careful tie-in required to existing revetment and 61 Fauntleroy synthetic turf. The biscuit rock could allow informal access to the jetty.  Reconstruct fencing between Reserve 26219 and 61 Fauntleroy Ave	High to very high	Step height of biscuit rock to be discussed with 3 Hilton Grove	
6. Block stairs and revoke jetty licence	No liability concern	Revoke jetty licence and remove jetty. Install fencing at top and base of stairs to restrict access.	Very	Lowest capital cost. Low aesthetic value.	
7. Remove stairs and retaining wall, extend revetment and revoke jetty license	No liability concern	Revoke jetty licence and remove jetty.  Remove existing retaining walls and stairs. Regrade bank.  Extend existing revetment to 61 Fauntleroy Avenue fence with careful tie-in required.  Reconstruct fencing between Reserve 26219 and 61 Fauntleroy Ave	Mod to high		



5



#### 3.3 IMPROVING ACCESS FROM ROAD RESERVE TO RESERVE 26129

The existing access from the Road Reserve to Reserve 26129 is shown in Figure 8, with rapid changes in foreshore levels and access via a small gap in the fencing. One option is presented for improving access to Reserve 26129 from the Hilton Grove car park for foreshore maintenance. This option minimised disturbance to aesthetic features on Reserve 26129 installed by the owners of 3 Hilton Grove and minimised bank regrading. Existing levels were obtained from a feature survey from March 2008.

It is recommended to provide a 2.5m width crushed limestone or pea gravel path by (Figure 9):

- removing 3m of fencing on the corner adjacent to garden beds;
- temporarily removing two lengths (≈5m) of fencing between the car park and the fence corner.
   This fencing should be replaced following the installation of the limestone path;
- removing 2.5m (cross-shore) x 6.5m (alongshore) of garden bed including non-native plants;
- trimming the tree closest to the river to ensure vehicle access can be maintained;
- regrading the bank from the car park to eastern edge of the garden bed (Figure 10) with adjacent battering to achieve a grade of ≈1V:5H instead of the existing 1V:1.5H to 1V:2H;
- installing a crushed limestone or pea gravel path;
- replacing the 3m of removed fencing with a lockable gate. Consider a privacy screen or planting
  a vine along the gate to improve privacy for the owners of 3 Hilton Grove because the existing
  vine will be removed with the fence.

If fencing was not installed between the road reserve and Reserve 26129, other options for improving the delineation between private and public land, such as the installation of a low wall along the property boundary of 3 Hilton Grove, may be required.

#### 3.4 OTHER MODIFICATIONS TO RESERVE 26129

There are additional aesthetic features on Reserve 26129 placed by the owners of 3 Hilton Grove including: fencing; brick-paved area at the top of the stairs; paved area in the western garden bed; paving fronting the house; garden beds; lawn area with irrigation; and five large trees. It would be possible to remove these features and replace the whole Reserve to the boundary with 3 Hilton Grove with native vegetation. However, this report focuses on removing the features required to improve access to the foreshore outlined in Section 3.3 and the brick paving at the top of the staircase (if the staircase is removed). The City should decide if the relative benefits of removing the Japanese Pepper tree and other introduced trees with natives are greater than the negatives of losing an established tree.

Removal of approximately 2m of lawn and irrigation should be considered to provide a smoother gradient to transition between the upper bank bioengineering and revegetation and the lawn. A small swale could be incorporated to reduce the surface runoff onto the native vegetation. The alongshore extent of the 2m regrading of the lawn will require tying in with the crushed limestone path installed to improve access to the Reserve 26219 for maintenance (Section 3.3).

It is recommended the jetty licenced to 3 Hilton Grove has private property signs installed on the jetty.

#### 4 SUMMARY

This information is provided to facilitate discussions between the City of Belmont and the owners of 3 Hilton Grove. A range of options are presented, with no single option recommended by due to the complexity of liability and source of funding at this site with mixed public and private use. It is recommended a site feature survey is obtained before preparing concept designs and that regular monitoring be undertaken of the revetment within the hydraulic zone.

Document 48

12

FOI

Document Release

Sent:

From:

6 Mar 2018 10:27:43 +0000

Nicole Davey

**Rivers Planning** 

Subject:

FW: Hilton Grove Reserve 26219

Attachments:

RFQ- Hilton Grove Reserve 26219 (2).pdf, Attachment 1- Advice

Letter.pdf, Attachment 2- Garvey Park Survey 20170120 SHEET 2.pdf

Hi Nicole.

Thanks for providing the Department of Biodiversity, Conservation and Attractions (DBCA) with the opportunity to comment on the attached documents. It is understood that the City of Belmont (the City) is currently seeking quotes to develop a landscape concept design for 1 Hilton Grove, Ascot (Reserve 26219). The proposal to (a) improve access through the reserve for public use and future maintenance and (b) improve public amenity to the western portion of the reserve is supported by DBCA.

It is understood that currently, there are a number of unauthorised structures within the reserve such as the staircase (and brick paved area) leading to the privately owned jetty, fencing, garden beds and trees, irrigated lawn and paved areas (with seating furniture). It is understood that all these features have been installed by the owners of 3 Hilton Grove, which is adjacent to the Reserve 26219. These unauthorised structures and features gives the perception that the reserve is under private ownership and has the potential to alienate public access and use of the reserve. It Is noted that the City's Request of Quote document state that a 2m section of the lawn and irrigation area will be remove (and revegetation). It is recommended that the draft landscape concept design considers the removal of all the fencing, garden beds and trees (if nonnative), irrigated lawn and paved areas, with these areas to be rehabilitated and revegetated with appropriate native plant species.

In regards to privately owned jetty, please be advised that the jetty was previously repaired without prior approval from DBCA. DBCA's Planning for Jetties within the Swan Canning Development Control Area (Policy 44) states that where the jetty has become separated from the associated privately owned lot because land adjacent to the river has been acquired by the state, DBCA would not support any further renewals of the jetty licence for private (domestic) use, and will instead support the transfer of the jetty licence to a public authority (i.e. City of Belmont) so it can become a public jetty. If the continued use of the jetty as a public asset is not considered appropriate by the City, DBCA would recommend that the structure be removed and site rehabilitated (at the expense of the jetty owner/licence). Therefore, future remedial works to the staircase as outlined in the City's Request for Quote should only be considered if these structures (i.e. jetty and staircase) are intended to be acquired and managed by the City, and is able to demonstrate a public benefit. Otherwise, it is recommended that the City considers options to remove the staircase from Reserve 26219.

Please note that features proposed on the reserve to provide a sense of separation/privacy for the resident at 3 Hilton Grove as outlined in the City's Request for Quote should be softscape features (i.e. trees). If any hardscape features are proposed (e.g. retaining walls or fencing), these should be installed on the boundary of the reserve and the residential lot. Please refer to DBCA's Planning for Development Setback Requirements Affecting the Swan Canning Development Control Area (Policy 48) for height and design specifications of boundary fences abutting the Swan Canning Development Control Area (DCA).

The draft landscape concept plan should also consider options to remediate the existing foreshore stabilisation structure, including revegetation as recommended by Seashore Engineering (P0064478), which will also assist with reducing the visual impact of the rock revetment.

DBCA would like the opportunity to review the draft landscape design once it has been completed. Further advice on future planning approvals required could also be provided at that point, as the reserve is wholly within the DCA.

Document Set ID: 3700463 Version: 1, Version Date: 07/03/2018

DOC 48 100

## Clause 3 (1) Matter is exempt matter if its disclosure would reveal personal information about an individual (whether living or dead)

#### Kind regards

| Environmental Officer
Rivers and Estuaries Division | Parks and Wildlife Services
Department of Biodiversity, Conservation and Attractions
08 | 17 Dick Perry Avenue, Kensington WA 6151
Postal: Locked Bag 104, Bentley Delivery Centre WA 6983
www.swanrivertrust.dpaw.wa.gov.au or www.dpaw.wa.gov.au
CARING FOR THE SWAN CANNING RIVERPARK



Set X

----Original Message-----

From: Nicole Davey <Nicole.Davey@belmont.wa.gov.au>

Sent: Tuesday, 16 January 2018 3:31 PM

To: @dbca.wa.gov.au>; Rivers Planning

<rivers.planning@dbca.wa.gov.au>
Subject: Hilton Grove Reserve 26219

As discussed, the City is intending to distribute the attached RFQ for Hilton Grove Reserve 26219.

It would be appreciated if the Rivers & Estuaries Division could advise of any concerns with what is intended for the site.

The reserve is wholly within the Swan River Development Control area, and would remain open and accessible to the public.

The City has investigated the possibility of amendment of the MRS boundary of the reserve with the WAPC.

They were not supportive, and instead suggested removing all unapproved structures from the reserve and fencing the property boundary.

Regards

NICOLE DAVEY
COORDINATOR- ENVIRONMENT

City of Belmont, 215 Wright Street, Cloverdale, WA 6105 Postal address: Locked Bag 379, Cloverdale WA 6985

Phone – (08) Fax – (08) 9478 1473 Mobile – Email – @belmont.wa.gov.au Web – www.belmont.wa.gov.au

☐ Think before you print

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Your message is ready to be sent with the following file or link attachments:

RFQ- Hilton Grove Reserve 26219 (2).pdf

Document Set ID: 3700463 Version: 1, Version Date: 07/03/2018



# Ordinary Council Meeting 26/02/19

Item 12.9 refers

**Attachment 26** 

# Annual Electors' Meeting Minutes 12 December 2018





# **City of Belmont**

# ANNUAL ELECTORS' MEETING MINUTES

## TABLE OF CONTENTS

12 December 2018

ITE	EM SUBJECT	PAGE
NC	OTICE OF MEETING	
1.	OFFICIAL OPENING	4
2.	APOLOGIES AND LEAVE OF ABSENCE	4
3.	REPORT OF THE MAYOR	4
4.	REPORT OF THE CHIEF EXECUTIVE OFFICER	4
5.	RECEIVING THE ANNUAL REPORT 2017-2018	5
6.	GENERAL BUSINESS	9
7.	CLOSURE	1 <sup>·</sup>

#### **TABLED ATTACHMENTS INDEX**

Tabled Attachment 1 - Item 4 refers



# City of Belmont ANNUAL ELECTORS' MEETING MINUTES

#### **12 December 2018**

## INFORMATION ON HOW THE ANNUAL ELECTOR'S MEETING IS CONDUCTED

The purpose of the Annual Electors Meeting (AEM) is to receive the City's Annual Report and any other general business pertaining to Council. In accordance with the *Local Government Act 1995* and supporting Regulations, the Mayor is to preside at a General or Special Meeting of Electors and shall determine the procedure to be followed, as set out below:

- 1. The Standing Orders Local Law 2017 will apply subject to the discretion of the Presiding Member.
- 2. All present are required to sign the attendance register at the entry to the Chambers, including name and address.
- 3. Speakers must be Electors or Ratepayers of the City of Belmont.
- 4. During General Business, questions or statements may only relate to matters that affect the City of Belmont and will be accepted at the discretion of the Presiding Member.
- Motions from Electors, where presented in writing, will be read aloud by the Presiding Member to ensure that everyone is clear about what they are voting on. Motions from the floor will only be accepted at the discretion of the Presiding Member. An Annual Electors' Meeting Notice of Motion Form to submit your Motion is available on the City's website and copies will also be available at the meeting.
- 6. The Presiding Member will call for a mover and a seconder for a motion.
- 7. No motion or amendment is open to debate until it has been seconded. Only one amendment on any one motion shall be received at a time and such amendment shall be dealt with before any further amendment can be received; however any number of amendments may be proposed.
- 8. Upon a motion being proposed, the Presiding Member will call for speakers to address the Chair.
- 9. When addressing the meeting a person is to:
  - a) Rise and move to the podium unless unable to do so by reason of sickness or disability
  - b) State his or her name and address for recording in the minutes
  - c) Address the meeting through the Presiding Member.
- 10. The mover of a motion (but not the mover of an amendment) has the right of reply, and this closes the debate.

## ANNUAL ELECTORS' MEETING 12 December 2018

- 11. An Elector may rise and move without discussion "That the question be now put", which, on being duly seconded and carried by a majority, will result in submission of the motion at once to the meeting, after the mover has replied.
- 12. The Presiding Member will then ask for a vote on the motion on the floor.
- 13. Each Elector has one vote. An Elector does not have to vote.
- 14. Voting is determined by a show of hands.
- 15. A simple majority carries the vote.
- 16. Minutes of this meeting will be available for inspection by members of the public. Any motions arising from the meeting requiring action will be presented to the next available Ordinary Meeting of Council for consideration.

The decisions of the AEM are not binding on the Council, but as required by the *Local Government Act 1995*, the reasons for any Council decision on a decision of this meeting are to be recorded in the minutes of the Council Meeting.

#### **ANNUAL ELECTORS MEETING**

#### **MINUTES**

#### **PRESENT**

Cr P Marks, Mayor (Presiding Member)

Cr B Ryan

Cr J Powell

Cr S Wolff

Cr L Cayoun

Cr G Sekulla, JP

East Ward

West Ward

West Ward

#### **IN ATTENDANCE**

Mr J Christie Chief Executive Officer Mr R Garrett Director Corporate and Governance **Director Community and Statutory Services** Mrs J Hammah Mr S Morrison A/Director Technical Services Manager Governance Mr J Olynyk, JP Mrs M Lymon Principal Governance and Compliance Advisor Ms D Morton Media and Communications Officer Mrs J Barnes Senior Governance Officer Ms S D'Agnone Governance Officer

#### **MEMBERS OF THE GALLERY**

Ms G Godfrey

Ms D Panzich

Mr A Reilly

Ms B Scharfenstein

Mr R Foster

Mr C Floyd

Mr R Birch

Ms L Hollands

Ms J Gee

Mr B Childs

Ms M Saldanha

There was one press representative in attendance.

#### 1. OFFICIAL OPENING

## 7.08pm The Presiding Member welcomed all those in attendance and declared the meeting open.

The Presiding Member advised that public notice of the meeting has been made in the Southern Gazette on Tuesday 27 November 2018 and Tuesday 11 December 2018.

A public notice was placed on the Notice Board of the Ruth Faulkner Library and the City of Belmont Administration Building Notice Board, in accordance with the *Local Government Act 1995*. Notification was also provided through the City's Facebook page.

The notices also stated that copies of the 2017-2018 Annual Report were available from the City's website and the Ruth Faulkner Public Library.

The Presiding Member read the Acknowledgement of Country.

Before I begin I would like to acknowledge the traditional owners of the land on which we are meeting today, the Noongar Whadjuk people, and pay respect to Elders past, present and future leaders.

#### 2. APOLOGIES AND LEAVE OF ABSENCE

Cr R Rossi, JP, Deputy Mayor (Apology) Cr M Bass (Apology) Cr J Davis (Absent) West Ward East Ward South Ward

#### 3. REPORT OF THE MAYOR

The Mayor provided an overview of the Mayor's Report included in the 2017-2018 Annual Report.

#### 4. REPORT OF THE CHIEF EXECUTIVE OFFICER

The Chief Executive Officer provided a presentation outlining the Chief Executive Officer's Report included in the 2017-2018 Annual Report (refer Tabled Attachment 1).

#### 5. RECEIVING THE ANNUAL REPORT 2017-2018

The Mayor invited questions on the Annual Report 2017-2018.

Ms Hollands asked the following questions:

1. How many Community Perception Surveys went out and what percentage of that number was returned?

#### Response

The Director Corporate and Governance advised there were in excess of 4,000 surveys sent out directly to specific contacts of the City. In addition there were provisions made for every member of the community to participate in the survey through Belmont Connect and substantial publicity. Invitations to participate were extended to the entire community. In excess of 690 responses were received and this figure is substantially greater than in previous years.

2. Where can we get a copy of the survey?

#### Response

The Director Corporate and Governance advised that the survey results have been available on the City's website since September. Results for surveys dating back to 2014 are available on the <u>City of Belmont website</u>.

Ms Gee asked the following questions:

1. Does the Council check the questions that are on the survey?

#### Response

The Director Corporate and Governance advised that yes the questions are checked by the City.

2. I saw the last survey and there were questions relating to the Joint Development Assessment Panel (JDAP). Many residents do not know what a JDAP is. You are not going to get the answers that you want.

#### Response

The Director Corporate and Governance advised that the survey includes a variety of questions with the intention that they will be understood by the greatest majority of residents. There is an option in the responses to indicate if a question has not been understood.

Item 5 Continued

Mr Floyd asked the following questions:

1. The survey seemed like a back slapping exercise. When so many questions are asked, where are the negative comments? Who comes to these conclusions?

#### Response

The Director Corporate and Governance encouraged all members of the public to read the complete survey document in order to better understand the findings.

The survey contained a vast array of questions, with results categorised into suburbs, community groups, age groups, and a variety of demographics.

Results in some categories where not as high as in the overall score and areas of concern have been identified, for example, safety and graffiti. Responses to the survey allow the City to take action where the community considers it a priority.

The Director Corporate and Governance reiterated his recommendation that the complete survey document be read so that a full understanding of the results can be gained.

2. I didn't see anything mentioned about the youth of the City. What is the City doing to give youth somewhere to go where they are not seen as gang people? Is the Council actively trying to do something for the Youth?

#### Response

The Director Community and Statutory Services advised that the CEO's report did refer to the number of visits made to The Base, which is a youth facility provided by the City and run by the YMCA for youth in Belmont. A Youth Strategy has recently been adopted by the City and will move forward over the next few years. The City remains committed to engaging with youth in a number of ways and acknowledge that they are a valued part of our community.

Ms Hollands asked the following questions:

3. What is the 2017 achievement assessment, how is it rated and how is it put together?

#### Response

The Chief Executive Officer advised there are a number of key areas addressed in the survey and the City of Belmont has been identified as an industry leader in six of these key areas. Over 40 local governments carried out similar surveys with benchmark questions and the City of Belmont achieved the highest overall score.

Apart from over 4,000 surveys being distributed, there was ample opportunity for all members of the community to participate in the survey. Almost 700 responses were received which makes the survey statistically valid.

Item 5 Continued

Ms Gee asked the following questions:

3. Does the survey have any contact numbers that people who don't understand the questions can ring?

#### Response

The Director Corporate and Governance advised there was a contact number on the survey for responders who needed assistance to complete the survey.

4. Did many people ring the assistance number?

#### Response

The Director Corporate and Governance advised that the consultants who were engaged to manage the survey addressed all enquiries and therefore he could not answer this question.

Ms Hollands asked the following questions:

4. 4,000 invitations to undertake the survey is less than 10% of Belmont's population. Why weren't more surveyed?

#### Response

The Director Corporate and Governance reiterated that the entire community was given an opportunity to participate in the survey and the nearly 700 responses received made the survey statistically valid and is considered an accurate representation of community views.

5. Was there a phone number?

#### Response

The Director Corporate and Governance advised that this question had already been asked and he has advised that yes there was a phone number for survey participants to call for assistance.

6. The CEO mentioned that the community building was another achievement. How much was the cost? Has that changed? I asked this question last night and have not received an answer.

#### Response

The Chief Executive Officer advised that Ms Hollands' question from the Ordinary Council Meeting the previous night has been taken on notice and will be responded to.

7. Has the City approached the state government to get more police into the City?

#### Response

The Chief Executive Officer advised that the City, in conjunction with the local member, continue to work closely with the WA Police, who have made significant changes at the state government level. The CEO stated that from memory the number of police officers in the City has increased by three or four in the last year as a result of the changes to the Policing model implemented by the State Government.

#### Item 5 Continued

8. Why is the City not putting rules into their own policies that require developers to plant more trees when developments go up?

#### Response

The Director Community and Statutory Services advised that each development is unique and a 'one size fits all' policy is not workable. It is a requirement that larger developments provide landscaping plans and where large trees are removed they are required to be replaced. Moving forward it is proposed that tree preservation in medium density developments will be looked at in more detail.

9. Is the City planning to make rules in their policies that require developers to plant more trees?

#### Response

The Director Community and Statutory Services advised that the Housing Strategy will address vegetation across the board, including in new developments.

10. What is the time frame?

#### Response

The Director Community and Statutory Services advised that she could not comment on the Canopy Plan, however Scheme reviews take several years to develop and involve a large amount of community consultation to ensure the desires of the community can be considered.

The Chief Executive Officer advised that the City's Urban Forest Strategy has recently been endorsed by Council and the Canopy Plan is in its final stages of development. The City will continue to remain committed to retaining trees wherever and whenever possible.

11. New developments have no room on the blocks? Is the City looking at how we can change that?

#### Response

The Presiding Member advised that the City has been aware of this difficult circumstance for many years and it has been discussed in many different forms over that time.

The Director Community and Statutory Services advised that the City continues to address this issue however improvements cannot happen overnight. The WA Design Guidelines from the Western Australian Planning Commission will assist to give the City more teeth regarding issues around environmental and root zone trees.

#### **RECOMMENDATION**

#### SEKULLA MOVED, POWELL SECONDED

That the <u>City of Belmont Annual Report 2017-2018</u> be received.

**CARRIED UNANIMOUSLY** 

#### 6. GENERAL BUSINESS

#### 6.1 NOTICE OF MOTION

#### MS JANET GEE MOVED, MS LISA HOLLANDS SECONDED,

That 'Recording of the Ordinary Council Meeting in their entirety and making them available on the City's website for all residents to listen to' be listed as an item on a meeting agenda in the first half of 2019 (by June 2019).

**CARRIED** 

#### 6.2 NOTICE OF MOTION

Ms Hollands put forward the following motion.

That the City of Belmont make available via their website a copy of the approved recordings of Public Question Time to all residents of Belmont free of charge.

Ms Godfrey suggested an amendment to the original motion which was agreed to by Ms Hollands and Ms Scharfenstein.

#### MS LISA HOLLANDS MOVED, MS BELLA SCHARFENSTEIN SECONDED,

That the City of Belmont make available via their website a copy of the approved recordings of Public Question Time to all residents of Belmont free of charge for a trial period of six months.

**CARRIED** 

#### 6.3 NOTICE OF MOTION

#### MS LISA HOLLANDS MOVED, MR RICHARD FOSTER SECONDED,

That the City of Belmont conduct a survey of the residents of Belmont on their use of the local roads to access Perth Airport. It is to include route of travel, starting or ending location and frequency of use.

**CARRIED** 

#### 6.4 NOTICE OF MOTION

#### MS LISA HOLLANDS MOVED, MS JANET GEE SECONDED,

That the City of Belmont adopt a more conciliatory approach when dealing with residents and use litigation as a last resort.

**CARRIED** 

8.45pm The Governance Officer departed the meeting.

8.47pm The Governance Officer returned to the meeting.

Item 6 Continued

#### 6.5 NOTICE OF MOTION

#### MS LISA HOLLANDS MOVED, MS MARINA SALDANHA SECONDED,

That the City of Belmont is transparent and provides an adequate response to the ratepayers of Belmont as to why there is a significant discrepancy on the amount of costs awarded by the courts in the Saldanha case and that of the actual costs and advised by the CEO as it is ratepayer's money.

**CARRIED** 

#### 6.6 NOTICE OF MOTION

#### MS LISA HOLLANDS MOVED, MS BELLA SCHARFENSTEIN SECONDED,

As a result of the adoption of the more formal rules for Public Question Time now it is being recorded and which is subsequently disadvantaging the residents, that the City of Belmont amends rules of Public Question Time as to allow residents adequate time to ask questions and get a response. If Councillors don't support this tonight and when it is raised in 2019, they should consider if they are there to represent the residents.

**CARRIED** 

- 9.12pm The Director Community and Statutory Services departed the meeting.
- 9.15pm The Director Community and Statutory Services returned to the meeting.

#### 6.7 Ms B Scharfenstein, 140 Coolgardie Avenue, Redcliffe

1. What is the City of Belmont's opinion of Conservative commentator Paul Murray's recent articles in the West Australian contending that the new WALGA policies will serve to censor Councillors and undermine the democratic process?

#### Response

The Chief Executive Officer advised that he was not aware of the content of Mr Murray's articles and therefore did not have an opinion.

Ms Scharfenstein requested that the remainder of her questions be taken on notice.

- 2. Following its revision and analysis of the Revised DA6 Vision Plan, what specific changes has WAPC requested be incorporated in the Structure Plan that is delaying its completion- excluding the closure of Brearley Avenue?
- 3. Why has COB not undertaken any significant foreshore restoration to areas of severe bank degradation during 2018 and when will it reinstate a comprehensive bank restoration program?
- 4. Has the CEO written to the responsible authorities about the expanding sandbank near the Tonkin Bridge consequent to silt washing out of the Southern Main Drain into the Swan River and what response/s have been received?

Item 6 Continued

#### 6.8 Mr R Foster, 140 Coolgardie Avenue, Redcliffe

Mr Foster requested that his questions be taken on notice.

- 1. In five or six years of attending City of Belmont OCMs, last night's OCM was probably the worst. Over the last five years you, Mr Mayor, have allowed us a certain amount of leeway with all of our questions and you have been extremely fair by allowing our questions. What has prompted the rules changes that turned last night's meeting into an absolute dog's breakfast?
- 2. What is the total floor area of the new community centre?
- 3. What will be the total floor area of the library, the digital hub, the recording studio, the museum, the crèche, the senior citizens club, the not for profit offices and the café?
- 4. Will the digital hub-recording studio be available for public use, such as music recording by local bands?
- 5. Can community organisations like BRRAG utilise the not for profit offices?
- 6. Cultural Centre in the DA6 in line with the Airport/Train Station/River in a Wagyl shape encompassed heritage/arts as a tourist attraction?

#### 7. CLOSURE

There being no further business, the Presiding Member thanked everyone for their attendance and closed the meeting at 9.21pm.



Item 12.10 refers

## **Attachment 27**

Corporate Business Plan 2019-2023 (To be circulated under separate cover)





Item 12.12 refers

**Attachment 28** 

Accounts for Payment December 2018



Compiled: 07/01/19 12:10

### City of Belmont



PLE Computers

### **Accounts for Payment - December 2018**

Pmnt_Ref	Date	CR_Code	Supplier	Pmnt Amnt	Description
Contractors		_		_	
EF059298	07/12/18	00491	Fuji Xerox Australia Pty Ltd	239.47	Photocopy Expenses
EF059302	07/12/18	00707	LoGo Appointments		Labour/Personnel Hire
EF059303	07/12/18	00736	McLeods		Legal Expenses
EF059304 EF059305	07/12/18 07/12/18	00783 00943	iSentia Pty Ltd Cirrena Pty Ltd		Professional Fees - Marketing Computer Software Maintenance
EF059308	07/12/18	01082	Sparks Refrigeration & Airconditioning		Airconditioning/Refrigeration Maintenance
EF059309	07/12/18	01088	Sports Turf Technology Pty Ltd		Professional Fees - Testing
EF059310	07/12/18	01090	St John Ambulance Australia Inc	215.60	First Aid Service
EF059311	07/12/18	01180	Position Partners	33.00	Survey Expenses
EF059312	07/12/18	01221	Landgate		Valuation Expense
EF059313 EF059314	07/12/18 07/12/18	01243 01353	WARP Group Aurion Corporation Pty Ltd	,	Traffic Control Computer Software Maintenance
EF059316	07/12/18	01333	Hays Specialist Recruitment		Labour/Personnel Hire
EF059317	07/12/18	01507	The Pressure King	,	Graffiti Removal
EF059319	07/12/18	01580	Workpower Inc	923.50	Gardening Contractor
EF059320	07/12/18	01614	Maxwell Robinson & Phelps		Pest Control
EF059321	07/12/18	01734	Chemwest		Plant Parts & Repairs
EF059324	07/12/18	02050	Austraffic W A		Traffic Control
EF059325 EF059327	07/12/18 07/12/18	02078 02216	Psyco Sand Western Australia Police		Gardening Contractor Volunteer National Police Check
EF059329	07/12/18	02210	Pelican Linemarking		Line Marking
EF059331	07/12/18	02615	Air-born Amusements		Plant/Equipment Hire
EF059333	07/12/18	02663	Road and Traffic Services		Kerbing Contractor
EF059335	07/12/18	02672	Ruah Community Services	12,941.43	Domestic Violence Advocate
EF059336	07/12/18	02711	CPG Research & Advisory Pty Ltd		Professional Fees - Analysis
EF059338 EF059339	07/12/18 07/12/18	02775 02837	Robert K Roach Surveyor GLG Greenlife Group		Survey Expenses
EF059339 EF059340	07/12/18	02057	Brown McAllister Surveyors		Gardening Contractor Survey Expenses
EF059343	07/12/18	03202	Spraymaster Spray Shop		Tools/Tool Repairs
EF059345	07/12/18	03571	Perrott Painting Maintenance Contracts		Painting Contractor
EF059346	07/12/18	03655	Acclaimed Catering	2,435.40	Catering/Catering Supplies
EF059347	07/12/18	03790	Storage King Perth Airport		Records Storage
EF059348	07/12/18	03816	Triple B International Pty Ltd		Home Care
EF059349 EF059352	07/12/18 07/12/18	03892 03941	Morgan Scarfe - Ricochet Circus Metro Bee Services		Music/Entertainment Expenses Bee Removal
EF059353	07/12/18	03980	Val Brooks		Community Art Classes
EF059356	07/12/18	04106	Effects Picture Framing		Photography/Framing Expenses
EF059357	07/12/18	04120	Randstad Pty Ltd		Labour/Personnel Hire
EF059358	07/12/18	04137	Greive Panelbeaters		Plant Parts & Repairs
EF059359	07/12/18	04211	Advance Scanning Services		Survey Expenses
EF059362	07/12/18	04477 04508	Place Laboratory Pty Ltd		Professional Fees - Landscaping
EF059364 EF059365	07/12/18 07/12/18	04506	Rev Sealin Garlett - JILBARR Mills Resources		Music/Entertainment Expenses Labour/Personnel Hire
EF059366	07/12/18	04677	3 Monkeys Audiovisual		Plant/Equipment Hire
EF059367	07/12/18	04693	Allwest Plant Hire Australia Pty Ltd	29,377.84	Plant/Equipment Hire
EF059368	07/12/18	04694	Eluma Event Solutions	99.00	Plant/Equipment Hire
EF059369	07/12/18	04720	TPG Aged Care		Home Care
EF059370 EF059372	07/12/18 07/12/18	04772 04825	ATM Advanced Traffic Management Reece's Event Hire		Plant/Equipment Hire Plant/Equipment Hire
EF059374	07/12/18	04967	Cockburn Party Hire	,	Plant/Equipment Hire
EF059375	07/12/18	04984	Spaced Out Placemakers		Plant/Equipment Hire
EF059376	07/12/18	05016	Cyclus Pty Ltd	7,456.64	Labour/Personnel Hire
EF059377	07/12/18	05049	HenderCare Pty Ltd		Home Care
EF059379	07/12/18	05074	Brook & Marsh Pty Ltd		Survey Expenses
EF059383 EF059385	07/12/18 07/12/18	05127 05188	Champion Music Takeshi Sotokawauchi - Rancho Sombrero		Music/Entertainment Expenses Catering/Catering Supplies
EF059386	07/12/18	05100	Meast Pty Ltd		Catering/Catering Supplies
EF059387	07/12/18	05212	Turnpoint Software Pty Ltd		Computer Software Maintenance
EF059388	07/12/18	05240	Otium Planning Group Pty Ltd	14,850.00	Professional Fees - Analysis
EF059389	07/12/18	05283	IRP Industrial Recruitment Partners		Labour/Personnel Hire
EF059390	07/12/18	05284	Shimearts		Surrey Road Underpass Mural
EF059391 EF059392	07/12/18 07/12/18	05299 05320	Sidekicker Eskima Bosordinga Phyl td		Labour/Personnel Hire Music/Entertainment Expenses
EF059392 EF059393	07/12/18	05320	Eskimo Recordings Pty Ltd Matt Storer		Music/Entertainment Expenses  Music/Entertainment Expenses
EF059394	07/12/18	05329	Menagerie Entertainment		Music/Entertainment Expenses
EF059395	07/12/18	05330	Sugar Blue Burlesque Pty Ltd		Music/Entertainment Expenses
EF059396	07/12/18	05331	Perth Cabaret Collective	1,500.00	Music/Entertainment Expenses
EF059397	07/12/18	05333	Mega Vision Australia Pty Ltd		Plant/Equipment Hire
EF059398	07/12/18	05334	BarbieQ - Matthew McPharlin		Music/Entertainment Expenses
EF059399 EF059400	07/12/18 07/12/18	05342 05351	J C Danks & M D Magnus Umbrella Entertainment Pty Ltd		Music/Entertainment Expenses Music/Entertainment Expenses
EF059401	07/12/18	05364	Lounge Backline Pty Ltd		Plant/Equipment Hire
EF059402	07/12/18	05371	Alex Polgardy		Plant/Equipment Hire
EF059403	07/12/18	05374	Keith Gurden - Mega Soundz	279.00	Plant/Equipment Hire
EF059404	07/12/18	05379	Kyla Morgan - Skylark Face & Body Art		Music/Entertainment Expenses
EF059406	07/12/18	05387	Sian Choyce		Music/Entertainment Expenses
EF059407 EF059408	07/12/18	05388 05389	Colin Ebsworth Calum McLean		Music/Entertainment Expenses Music/Entertainment Expenses
EF059409	07/12/18 07/12/18	05369	Grant Mushet		Music/Entertainment Expenses  Music/Entertainment Expenses
EF059411	07/12/18	05393	Rosa Beckett		Library - Entertainment Expense
EF059299	07/12/18	00557	City Subaru		Plant Parts & Repairs
EF059328	07/12/18	02254	PLE Computers	649.00	Stationery & Printing

649.00 Stationery & Printing

Downt Dof	Data	OD Code	Complian.	Donat Amount	Description A628
Pmnt_Ref EF059434	<b>Date</b> 14/12/18	<b>CR_Code</b> 00118	Supplier Australia Post	Pmnt_Amnt 4,177.02	
EF059436	14/12/18	00195	Bin Bath Australia Pty Ltd		Cleaning Services
EF059438	14/12/18	00204	Bollig Design Group Pty Ltd		Professional Fees - Architect
EF059441 EF059443	14/12/18 14/12/18	00346 00391	Action Couriers		Courier Service Professional Fees - Testing
EF059445 EF059445	14/12/18	00585	Chemistry Centre (WA) t/as ChemCentre Hydroquip Pumps		Bore Drilling/ Maintenance
EF059448	14/12/18	00707	LoGo Appointments		Labour/Personnel Hire
EF059450	14/12/18	00821	Leasechoice Pty Ltd		Plant/Equipment Hire
EF059453 EF059454	14/12/18	01088 01090	Sports Turf Technology Pty Ltd St John Ambulance Australia Inc		Professional Fees - Testing First Aid Service
EF059454 EF059455	14/12/18 14/12/18	01090	Dept of Premier & Cabinet - State Law Publisher		Advertising
EF059456	14/12/18	01170	Relay Concrete		Concrete Contractor
EF059459	14/12/18	01276	Westside Fire Services		Fire Equipment/Service
EF059461 EF059462	14/12/18 14/12/18	01447 01476	Initial Hygiene Hays Specialist Recruitment		Cleaning Services Labour/Personnel Hire
EF059463	14/12/18	01507	The Pressure King		Graffiti Removal
EF059467	14/12/18	01797	Green Skills (Ecojobs)		Labour/Personnel Hire
EF059469	14/12/18	02032	M J B Payne Consultants		Professional Fees - Building
EF059470 EF059471	14/12/18 14/12/18	02050 02059	Austraffic W A Western Resource Recovery Pty Ltd		Traffic Control Rubbish Removals
EF059472	14/12/18	02078	Psyco Sand		Gardening Contractor
EF059475	14/12/18	02711	CPG Research & Advisory Pty Ltd		Professional Fees - Analysis
EF059478	14/12/18	03308	Surveytech		Survey Expenses
EF059479 EF059480	14/12/18	03335 03593	Baycorp (WA) Pty Ltd	,	Professional Fees - Debt Collection
EF059481	14/12/18 14/12/18	03614	Philip Swain Julies Boarding Kennels & Cattery		Labour/Personnel Hire Pound Expenses
EF059482	14/12/18	03655	Acclaimed Catering		Catering/Catering Supplies
EF059484	14/12/18	03816	Triple B International Pty Ltd	,	Home Care
EF059486	14/12/18	03892	Morgan Scarfe - Ricochet Circus		Music/Entertainment Expenses
EF059487 EF059488	14/12/18 14/12/18	03941 04062	Metro Bee Services Grant Thornton Audit Pty Ltd		Bee Removal Audit Fee
EF059489	14/12/18	04067	Taylor Burrell Barnett		Professional Fees - Planning
EF059490	14/12/18	04105	Cleanflow Environmental Solutions		Drainage Maintenance
EF059492	14/12/18	04253	Camstruct Design & Build Pty Ltd		Building Construction
EF059493 EF059494	14/12/18 14/12/18	04259 04269	Urbis Pty Ltd The Scene Team		Professional Fees - Planning Photography/Framing Expenses
EF059495	14/12/18	04209	IAS Fine Art Logistics Pty Ltd		Records Storage
EF059496	14/12/18	04332	Happy Healthy Hoops		Music/Entertainment Expenses
EF059497	14/12/18	04371	ID Fleet Hire		Plant/Equipment Hire
EF059500 EF059501	14/12/18	04524	Moore Stephens	11,000.00	
EF059501 EF059502	14/12/18 14/12/18	04528 04574	J D Penangke - Jade Dolman BikeValet Australia		Music/Entertainment Expenses Bike Valet Services at Events
EF059503	14/12/18	04579	Mills Resources		Labour/Personnel Hire
EF059505	14/12/18	04608	Exclusive Pest Control		Pest Control
EF059506	14/12/18	04675	Bindi Bindi Dreaming		Catering/Catering Supplies
EF059508 EF059509	14/12/18 14/12/18	04723 04779	Future Logic One 20 Productions		Computer Hardware Maintenance Equipment Hire - Let's Celebrate Belmont festival
EF059511	14/12/18	04863	Cameron Hunt		Building Construction
EF059512	14/12/18	04941	Perth Pet Cremation		Pound Expenses
EF059514	14/12/18	05016	Cyclus Pty Ltd		Labour/Personnel Hire
EF059515 EF059516	14/12/18 14/12/18	05042 05049	Jenna-Lee Rodney HenderCare Pty Ltd		Music/Entertainment Expenses Home Care
EF059517	14/12/18	05123	Coachtrans - West Coast Bus Charters	,	Plant/Equipment Hire
EF059518	14/12/18	05127	Champion Music		Music/Entertainment Expenses
EF059522	14/12/18	05283	IRP Industrial Recruitment Partners		Labour/Personnel Hire
EF059523 EF059524	14/12/18 14/12/18	05296 05299	Citywest Concrete Pumping Sidekicker		Concrete Contractor Labour/Personnel Hire
EF059525	14/12/18	05303	Veraison		Professional Fees - Analysis
EF059526	14/12/18	05307	David Broadway Photographer		Photography/Framing Expenses
EF059527	14/12/18	05336	West-Sure Group Pty Ltd		Security Services
EF059528 EF059529	14/12/18 14/12/18	05340 05365	Perth Face Painting Company The Jungle Bod Squad		Music/Entertainment Expenses Music/Entertainment Expenses
EF059530	14/12/18	05303	OKMG Pty Ltd		Photography/Framing Expenses
EF059532	14/12/18	05394	DFP Recruitment Services Pty Ltd	2,519.37	Labour/Personnel Hire
EF059533	14/12/18	05397	Tor Snyder		Music/Entertainment Expenses
EF059534 EF059535	14/12/18 14/12/18	05398 05399	Umar Azad Comedy Simone Springer		Music/Entertainment Expenses Music/Entertainment Expenses
EF059537	14/12/18	05407	The Wooden Box		Catering/Catering Supplies
EF059531	14/12/18	05392	Complete DJ Group Pty Ltd	1,250.00	Plant/Equipment Hire
EF059442	14/12/18	00377	Dell Australia Pty Ltd		Stationery & Printing
EF059519 EF059549	14/12/18 19/12/18	05235 00001	Ben Sgherza A & B Canvas Australia		Disability Access and Inclusion Consultation Plant Parts & Repairs
EF059550	19/12/18	00147	Dormakaba Australia Pty Ltd		Building Maintenance
EF059551	19/12/18	00166	BSC Motion Technology Bearing Service		Plant Parts & Repairs
EF059555	19/12/18	00247	CAI Fences	2,931.50	•
EF059558 EF059560	19/12/18 19/12/18	00295 00313	Capital Recycling		Rubbish Removals
EF059562	19/12/18 19/12/18	00313	Coates Hire Operations Pty Ltd Department of Health WA		Plant/Equipment Hire Immunisation Expenses
EF059564	19/12/18	00404	DMS Tinting & Graphics		Window Treatments
EF059566	19/12/18	00412	Dowsing Group Pty Ltd		Concrete Contractor
EF059567	19/12/18	00413	Drive Straight Alignment & Brake Services		Plant Parts & Repairs
EF059569 EF059574	19/12/18 19/12/18	00424 00568	Eastern Metropolitan Regional Council Home Chef		Rubbish Removals Meals On Wheels
EF059575	19/12/18	00608	Programmed Integrated Workforce Ltd		Labour/Personnel Hire
EF059576	19/12/18	00623	Jaram Fleet Equipment		Plant Parts & Repairs
EF059581	19/12/18	00736	McLeods		Legal Expenses
EF059582 EF059583	19/12/18 19/12/18	00783 00784	iSentia Pty Ltd Bucher Municipal		Professional Fees - Marketing Plant Parts & Repairs
	. 5 2. 10	30.01		110.00	

					Description A629
Pmnt_Ref EF059584	<b>Date</b> 19/12/18	<b>CR_Code</b> 00815	Supplier New Town Toyota	Pmnt_Amnt	Plant Parts & Repairs
EF059585	19/12/18	00830	Oce Australia - Canon Group Company		Photocopy Expenses
EF059587	19/12/18	00856	John Papas Trailers Pty Ltd		Plant Parts & Repairs
EF059588 EF059589	19/12/18 19/12/18	00917 00927	Positive Auto Electrics Professional Glass & Maintenance		Plant Parts & Repairs Building Maintenance
EF059590	19/12/18	00927	Sonic HealthPlus Pty Ltd		Medical Examinations
EF059592	19/12/18	00962	Ricoh Australia Pty Ltd		Photocopy Expenses
EF059593	19/12/18	00972	Repco Auto Parts		Plant Parts & Repairs
EF059594 EF059595	19/12/18 19/12/18	01002 01059	RAC Businesswise Vehicle Breakdowns Sledgehammer Concrete Cutting Service	,	Plant Parts & Repairs Concrete Contractor
EF059596	19/12/18	01033	Sunny Industrial Brushware		Plant Parts & Repairs
EF059597	19/12/18	01180	Position Partners	444.40	Survey Expenses
EF059599	19/12/18	01186	ZircoDATA Pty Ltd		Records Storage
EF059600 EF059601	19/12/18 19/12/18	01192 01204	Martins Trailer Parts Pty Ltd Tutt Bryant Equipment		Plant Parts & Repairs Plant Parts & Repairs
EF059603	19/12/18	01210	Victoria Physiotherapy Group		Medical Examinations
EF059604	19/12/18	01233	Stihl Shop Redcliffe		Tools/Tool Repairs
EF059605	19/12/18	01243	WARP Group		Traffic Control
EF059606 EF059608	19/12/18 19/12/18	01255 01279	Wattleup Tractors Westrac Pty Ltd		Plant Parts & Repairs Plant Parts & Repairs
EF059610	19/12/18	01317	W A Hino Sales & Service		Plant Parts & Repairs
EF059611	19/12/18	01318	Flexi Staff Pty Ltd	,	Labour/Personnel Hire
EF059612 EF059613	19/12/18 19/12/18	01350 01358	ACE Plus Plumbing Kevrek Australia Pty Ltd		Plumbing Maintenance/Supplies Plant Parts & Repairs
EF059616	19/12/18	01498	Autosweep Industrial Sweeping		Plant/Equipment Hire
EF059617	19/12/18	01508	Danish Patisserie - Cakes West Pty Ltd		Catering/Catering Supplies
EF059618	19/12/18	01533	WC Convenience Management Pty Ltd		Building Maintenance
EF059620 EF059621	19/12/18 19/12/18	01675 01705	Horizons West Bus & Coachlines Econo-Mow Lawn & Garden Care		Plant/Equipment Hire Home Care - Garden Service
EF059622	19/12/18	01714	Total Eden Pty Ltd	,	Reticulation Parts & Repairs
EF059623	19/12/18	01721	Fulton Hogan Industries		Road Building Contractor
EF059624	19/12/18	01772	Data#3 Limited		Computer Software Maintenance
EF059625 EF059626	19/12/18 19/12/18	01784 01985	DVG Burswood Honda Oceanis International Pty Ltd		Plant Parts & Repairs Oasis Expenses
EF059627	19/12/18	02023	YMCA Perth Youth & Community Services Inc		Youth Services Expenses
EF059630	19/12/18	02207	Wilson Security		Security Services
EF059631	19/12/18	02229 02316	Belgravia Leisure Pty Ltd		Oasis Expenses
EF059632 EF059634	19/12/18 19/12/18	02371	Ayres Tyre Service Kone Elevators Pty Ltd		Plant Parts & Repairs Building Maintenance
EF059635	19/12/18	02387	Triton Electrical Contractors Pty Ltd		Electrical Contractor
EF059636	19/12/18	02425	Prestige Alarms		Security Services
EF059637 EF059639	19/12/18	02458	Technology One Ltd Zenien		Computer Software Maintenance
EF059642	19/12/18 19/12/18	02589 02779	Natural Area Holdings Pty Ltd		Security Services Gardening Contractor
EF059644	19/12/18	02941	Taman Tools - Quality Nominees Pty Ltd		Tools/Tool Repairs
EF059645	19/12/18	03031	Retech Rubber		Plant Parts & Repairs
EF059646 EF059648	19/12/18 19/12/18	03194 03202	Datatel Electrical & Communications Spraymaster Spray Shop		Electrical Contractor Tools/Tool Repairs
EF059649	19/12/18	03366	Daimler Trucks Perth		Plant Parts & Repairs
EF059652	19/12/18	03571	Perrott Painting Maintenance Contracts		Painting Contractor
EF059653	19/12/18	03824	Konica Minolta		Photocopy Expenses
EF059654 EF059655	19/12/18 19/12/18	04026 04125	H K Calibration Technologies Pty Ltd Pressure Cleaner Shop WA		Plant Parts & Repairs Plant Parts & Repairs
EF059656	19/12/18	04137	Greive Panelbeaters		Plant Parts & Repairs
EF059657	19/12/18	04146	JB Hi-Fi Group Commercial Account	,	Electrical Goods
EF059658	19/12/18	04163	CMS Engineering		Airconditioning/Refrigeration Maintenance
EF059659 EF059660	19/12/18 19/12/18	04202 04246	City Retravision Bibliotheca Australia Pty Ltd		Electrical Goods Computer Software Maintenance
EF059661	19/12/18	04305	A V Media Systems W A		Electrical Goods
EF059662	19/12/18	04352	Grama Bazita Service & Maintenance		Fire Equipment/Service
EF059663	19/12/18	04454	FM Contract Solutions Pty Ltd		Professional Fees - Analysis
EF059664 EF059665	19/12/18 19/12/18	04467 04477	Rent a Fence Pty Ltd Place Laboratory Pty Ltd	5,986.75 12 340 90	Professional Fees - Planning
EF059666	19/12/18	04693	Allwest Plant Hire Australia Pty Ltd		Plant/Equipment Hire
EF059669	19/12/18	04943	Cleandustrial Services Pty Ltd		Cleaning Services
EF059670	19/12/18	04974	Turf Care WA Pty Ltd		Gardening Contractor
EF059671 EF059675	19/12/18 19/12/18	04976 05101	Redstar Equipment Supplies De Lage Landen Pty Ltd		Plant Parts & Repairs Plant/Equipment Hire
EF059676	19/12/18	05109	Onsite Rental Group Operations Pty Ltd		Plant/Equipment Hire
EF059678	19/12/18	05177	Thrifty Car & Truck Rentals WA		Plant/Equipment Hire
EF059679 EF059680	19/12/18 19/12/18	05210 05239	Mulloway Studio Pty Ltd Majestic Plumbing Pty Ltd		Professional Fees - Architect Plumbing Maintenance/Supplies
EF059681	19/12/18	05259	AAAC Towing Pty Ltd		Towing Vehicles
EF059682	19/12/18	05253	Galt Geotechnics		Professional Fees - Engineering
EF059683	19/12/18	05308	Modern Motor Trimmers		Plant Parts & Repairs
EF059684 EF059685	19/12/18 19/12/18	05313 05353	Zimoch Surveys United Equipment Pty Ltd		Survey Expenses Plant/Equipment Hire
EF059686	19/12/18	05362	NOVA Entertainment (Perth) Pty Ltd		Advertising
EF059688	19/12/18	05409	Hosemasters International	190.96	Plant Parts & Repairs
EF059689	19/12/18	99966	MAIA Financial Pty Ltd		Plant/Equipment Hire
EF059687 EF059552	19/12/18 19/12/18	05384 00221	Auswest Displays - Watway Pty Ltd John Hughes Group		Plant/Equipment Hire Plant Parts & Repairs
EF059573	19/12/18	00557	City Subaru	,	Plant Parts & Repairs
EF059651	19/12/18	03567	Gardner Autos Pty Ltd T/as Gardner Holden	890.00	Plant Parts & Repairs
EF059667	19/12/18	04870	Bandit Tree Equipment		Plant Parts & Repairs
EF059674 EF059709	19/12/18 20/12/18	05056 00083	Atdec Pty Ltd Ascot Veterinary Hospital		Stationery & Printing Pound Expenses
EF059713	20/12/18	00346	Action Couriers		Courier Service

Pmnt_Ref	Date	CR_Code	Supplier	Pmnt_Amnt	Description A630
EF059714	20/12/18	00390	Landgate - Land Enquiry	_	Title Searches
EF059715	20/12/18	00491	Fuji Xerox Australia Pty Ltd		Photocopy Expenses
EF059716	20/12/18	00585	Hydroquip Pumps	17,217.20	Bore Drilling/ Maintenance
EF059717	20/12/18	00651	Kerbing West	20,181.49	Kerbing Contractor
EF059718	20/12/18	00699	Marketforce Pty Ltd	33,773.10	Advertising and Printing
EF059720	20/12/18	00943	Cirrena Pty Ltd	1,980.00	Computer Software Maintenance
EF059721	20/12/18	01047	Show West	8,000.00	Plant/Equipment Hire
EF059723	20/12/18	01170	Relay Concrete		Concrete Contractor
EF059725	20/12/18	01476	Hays Specialist Recruitment		Labour/Personnel Hire
EF059728	20/12/18	01614	Maxwell Robinson & Phelps		Pest Control
EF059729	20/12/18	01712	Donegan Enterprises Pty Ltd		Playground Inspections/Repairs
EF059733	20/12/18	02107	Mercer (Australia) Pty Ltd	,	Professional Fees - Analysis
EF059735 EF059736	20/12/18 20/12/18	02292 02298	About Bike Hire Pelican Linemarking		Plant/Equipment Hire
EF059738	20/12/18	02296	Tomato Lake Cafe - Xing Guang Rao		Line Marking Catering/Catering Supplies
EF059739	20/12/18	02330	Allsports Linemarking		Line Marking
EF059742	20/12/18	02557	Hooked on Hessian		Library - Entertainment Expense
EF059745	20/12/18	03080	Ground Support Systems (Aust)		Plant/Equipment Hire
EF059746	20/12/18	03504	Classic Tree Services		Gardening Contractor
EF059747	20/12/18	03655	Acclaimed Catering		Catering/Catering Supplies
EF059748	20/12/18	03941	Metro Bee Services		Bee Removal
EF059750	20/12/18	04120	Randstad Pty Ltd		Labour/Personnel Hire
EF059751	20/12/18	04131	Total Green Recycling Pty Ltd	374.00	Rubbish Removals
EF059752	20/12/18	04154	Kinetica Circus Arts Ltd	2,640.00	Music/Entertainment Expenses
EF059753	20/12/18	04302	Southern Cross Housing Ltd	6,500.00	Independent Living Units Management
EF059754	20/12/18	04320	ABM Landscaping		Bricks/Bricklaying
EF059755	20/12/18	04393	Christina Neubaurer / People on Bicycles		Community Exercise Classes
EF059758	20/12/18	04529	Southern Cross Care (WA) Inc		Independent Living Units Management
EF059759	20/12/18	04579	Mills Resources		Labour/Personnel Hire
EF059760	20/12/18	04677	3 Monkeys Audiovisual		Computer Hardware Maintenance
EF059761	20/12/18	04720	TPG Aged Care		Home Care
EF059762 EF059763	20/12/18 20/12/18	04772 04825	ATM Advanced Traffic Management Reece's Event Hire		Plant/Equipment Hire Plant/Equipment Hire
EF059764	20/12/18	04023	Cockburn Party Hire		Plant/Equipment Hire
EF059765	20/12/18	05016	Cyclus Pty Ltd		Labour/Personnel Hire
EF059766	20/12/18	05023	Skyline Landscape Services Group Pty Ltd		Gardening Contractor
EF059767	20/12/18	05043	GTA Consultants (WA) Pty Ltd		Professional Fees - Design
EF059768	20/12/18	05051	ProSurveys		Survey Expenses
EF059770	20/12/18	05150	PACT Construction Pty Ltd		Building Construction
EF059771	20/12/18	05282	WCP Civil Pty Ltd	300,181.34	Road Building Contractor
EF059772	20/12/18	05293	Bellrock Cleaning Services - Scottish Pacific	2,577.85	Cleaning Services
EF059773	20/12/18	05302	Sensorium Theatre Inc	1,501.50	Library - Entertainment Expense
EF059774	20/12/18	05320	Eskimo Recordings Pty Ltd		Music/Entertainment Expenses
EF059775	20/12/18	05321	Cafe 224		Catering/Catering Supplies
EF059776	20/12/18	05343	The Beehives Go Go Dancers		Music/Entertainment Expenses
EF059777	20/12/18	05344	SUEZ Recycling and Recovery Pty Ltd	,	Rubbish Removals
EF059778	20/12/18	05361	Soltex Pty Ltd	,	Window Treatments
EF059781 EF059782	20/12/18 20/12/18	05408	K-Line Ag		Mower Parts & Repairs Catering/Catering Supplies
EF059762 EF059732	20/12/18	05412 02009	Nicole Yee - Don't Have a Cow Real Food Co Parchem Construction Supplies		Drainage Maintenance
LI 033132	Contractors To		i arenem construction oupplies	3,082,752.95	
Councillor Pa		, tui		0,002,102.00	=
787980	07/12/18	00919	Janet Powell		Councillor Sitting Fee/Reimbursement
787984	07/12/18	01369	Philip Marks		Councillor Sitting Fee/Reimbursement
EF059296	07/12/18	00158	Margie Bass		Councillor Sitting Fee/Reimbursement
EF059318	07/12/18	01520	Stephen Wolff		Councillor Sitting Fee/Reimbursement
EF059326	07/12/18	02145	Robert Rossi		Councillor Sitting Fee/Reimbursement
EF059350	07/12/18	03912	Lauren Cayoun		Councillor Sitting Fee/Reimbursement
EF059351	07/12/18	03916	Bernard Ryan	8,716.00	Councillor Sitting Fee/Reimbursement
EF059380 EF059381	07/12/18 07/12/18	05084 05085	Jenny Davis George Sekulla		Councillor Sitting Fee/Reimbursement
FI 00900 I	Councillor Pay		George Genula		Councillor Sitting Fee/Reimbursement
	Councilion Pay	menta Iutal		109,259.25	

Pmnt Ref	Date	CR_Code	Supplier	Pmnt_Amnt	Description A631
Fuels and Utilitie		2 <b>00</b> 00	Саррио		23001101177001
87981	07/12/18	01142	Telstra Corporation Limited	3,701.52	Phone/Internet expenses
'87982	07/12/18	01252	Water Corporation	3,351.07	Water, Annual & Excess
87983	07/12/18	01274	Synergy	,	Light, Power, Gas
F059300	07/12/18	00573	Maureen Hooper		Volunteer Driver Fuel Allowance
F059330	07/12/18	02471	Western Power		Light, Power, Gas
F059332	07/12/18	02635	MessageMedia - Message4U Pty Ltd		Phone/Internet expenses
			David McKinlay		·
F059341	07/12/18	03067	,		Volunteer Driver Fuel Allowance
F059360	07/12/18	04216	Robert Boulter		Volunteer Driver Fuel Allowance
F059361	07/12/18	04367	Ian Smith		Volunteer Driver Fuel Allowance
87991	14/12/18	01142	Telstra Corporation Limited	6,335.39	Phone/Internet expenses
87992	14/12/18	01252	Water Corporation	6,775.21	Water, Annual & Excess
F059474	14/12/18	02336	Vodafone Messaging	5.08	Phone/Internet expenses
F059477	14/12/18	02911	Dianne Dettlinger	50.05	Volunteer Driver Fuel Allowance
F059491	14/12/18	04216	Robert Boulter	18.20	Volunteer Driver Fuel Allowance
F059510	14/12/18	04837	Perth Energy Pty Ltd	31 203 01	Light, Power, Gas
F059536	14/12/18	05405	lan Stirling		Volunteer Driver Fuel Allowance
F059557	19/12/18	00264	Castrol Australia Pty Ltd		Fuel, Oil, Additives
F059703			Google Australia Pty Ltd		Phone/Internet expenses
	19/12/18	05249	,		·
87995	20/12/18	01142	Telstra Corporation Limited		Phone/Internet expenses
87996	20/12/18	01252	Water Corporation	,	Water, Annual & Excess
87998	20/12/18	01274	Synergy	7,396.90	Light, Power, Gas
F059719	20/12/18	00788	Motorcharge Ltd (Wright Express Aust)	17,713.04	Fuel, Oil, Additives
F059726	20/12/18	01488	ZettaNet Pty Ltd	1,684.98	Phone/Internet expenses
F059741	20/12/18	02471	Western Power	1,881.00	Light, Power, Gas
F059743	20/12/18	02631	Caltex Energy WA		Fuel, Oil, Additives
F059757	20/12/18	04498	Garry Hare	,	Volunteer Driver Fuel Allowance
F059780	20/12/18	05405	lan Stirling		Volunteer Driver Fuel Allowance
	uels and Utili		Salmiy	217,287.04	- S.G.MOOF BITTOFF GOT/MIOWALIOC
	ucio anu Utili	ues Iulai		211,201.04	±
Materials	07/40/40	00000	Cofo Cornerate	005.00	Craesias
F059294	07/12/18	00009	Cafe Corporate		Groceries
F059306	07/12/18	01040	Sheridans Badges & Engraving		Badges & Pendants
F059307	07/12/18	01066	Snap Printing - Belsnap Pty Ltd		Stationery & Printing
F059322	07/12/18	01780	Bodycare Workplace Solutions - Healthworks	296.34	Publications/Newspapers
F059323	07/12/18	01906	Frazzcon Enterprises	658.94	Signs
F059337	07/12/18	02757	Quick Corporate Australia Pty Ltd	13.95	Stationery & Printing
F059342	07/12/18	03197	West Coast Turf		Gardening - Plants/Supplies
F059344	07/12/18	03403	Visual Lighting		Lights & Light Fittings
F059354	07/12/18	04041	The Plant Supply Co		Gardening - Plants/Supplies
F059355	07/12/18	04053	Totally Workwear		Safety Clothing/Equipment
					, , ,
F059363	07/12/18	04491	Woolworths Limited (WA)		Groceries
F059371	07/12/18	04805	Mai Flower Supplies		Flowers
F059373	07/12/18	04867	Easy Signs Pty Ltd	1,589.69	•
F059382	07/12/18	05120	Crush	22,885.50	Stationery & Printing
F059384	07/12/18	05144	Tangibility Pty Ltd	660.00	Stationery & Printing
F059435	14/12/18	00174	Cellarbrations at Belmont	903.81	Beverages
F059437	14/12/18	00203	BOC Gases Australia Ltd		Welding Equipment/Supplies
F059440	14/12/18	00281	Child Education Services		Books/CDs/DVDs
F059447	14/12/18	00664	Kmart Australia Limited		Toys, Stationery and Books
F059451	14/12/18	01019	Barmah Hats		Safety Clothing/Equipment
					, , ,
F059452	14/12/18	01074	Specialised Security Shredding - Shred-X Pty Ltd		Stationery & Printing
F059458	14/12/18	01261	Wesfarmers Kleenheat Gas Pty Ltd		Welding Equipment/Supplies
F059464	14/12/18	01547	Big W		Groceries & Catering Supplies
F059468	14/12/18	01983	Whistlers Products Pty Ltd	457.80	Groceries
F059473	14/12/18	02201	Neverfail Springwater Limited	39.60	Beverages
F059476	14/12/18	02862	James Bennett Pty Ltd	2,112.15	Books/CDs/DVDs
F059483	14/12/18	03660	Safe T Card Australia Pty Ltd		Safety Clothing/Equipment
F059485	14/12/18	03856	SEM Distribution		Publications/Newspapers
F059498	14/12/18	04394	JB Hi-Fi Belmont Forum		Books/CDs/DVDs
F059499	14/12/18	04394	Woolworths Limited (WA)		Groceries
			,		
F059504	14/12/18	04607	Ink Station		Stationery & Printing
F059507	14/12/18	04705	Quality Press Digital		Stationery & Printing
F059513	14/12/18	05011	WA Fresh Delivered		Groceries
F059520	14/12/18	05263	Trophy Warehouse		Badges & Pendants
F059446	14/12/18	00601	Institute of Public Works Engineering Sydney	545.66	Books/CDs/DVDs
F059553	19/12/18	00231	Bunnings Group Ltd	11,232.05	Hardware
F059554	19/12/18	00233	Bunzl Limited		Cleaning Products
F059556	19/12/18	00261	Atom Supply		Metal Goods
F059559	19/12/18	00311	Cloverdale Hardware & Western Supply		Hardware
F059561	19/12/18	00314	Coca-Cola Amatil (Aust) Pty Ltd		Beverages
			· · · ·		•
E050563	19/12/18	00403	Boral Construction Materials Group Ltd		Road/Drainage Material
	19/12/18	00406	Domus Nursery		Gardening - Plants/Supplies
F059565	19/12/18	00414	Dulux Australia		Paint & Accessories
F059565 F059568		00438	Thermo Fisher Scientific Aust Pty Ltd		Gardening - Plants/Supplies
F059565 F059568 F059570	19/12/18	00443	Enviropacific Pty Ltd	45.00	Cleaning Products
F059565 F059568 F059570	19/12/18 19/12/18		Saferight Pty Ltd	265.00	Safety Clothing/Equipment
F059565 F059568 F059570 F059571		00475			Signs
F059565 F059568 F059570 F059571 F059572	19/12/18 19/12/18	00475	9 ,	020.00	
F059565 F059568 F059570 F059571 F059572 F059577	19/12/18 19/12/18 19/12/18	00475 00627	Jason Signmakers		•
F059565 F059568 F059570 F059571 F059572 F059577 F059578	19/12/18 19/12/18 19/12/18 19/12/18	00475 00627 00634	Jason Signmakers Johns Building Supplies Pty Ltd	79.19	Building Material
F059565 F059568 F059570 F059571 F059572 F059577 F059578 F059579	19/12/18 19/12/18 19/12/18 19/12/18 19/12/18	00475 00627 00634 00653	Jason Signmakers Johns Building Supplies Pty Ltd Humes - Holcim (Australia) Pty Ltd	79.19 2,233.00	Building Material Concrete Products
F059565 F059568 F059570 F059571 F059572 F059577 F059578 F059579 F059580	19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18	00475 00627 00634 00653 00697	Jason Signmakers Johns Building Supplies Pty Ltd Humes - Holcim (Australia) Pty Ltd Landmark Operations Limited	79.19 2,233.00 2,323.42	Building Material Concrete Products Gardening - Plants/Supplies
F059565 F059568 F059570 F059571 F059572 F059577 F059578 F059579 F059580 F059586	19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18	00475 00627 00634 00653 00697 00850	Jason Signmakers Johns Building Supplies Pty Ltd Humes - Holcim (Australia) Pty Ltd Landmark Operations Limited Pacific Safety Wear	79.19 2,233.00 2,323.42 105.60	Building Material Concrete Products Gardening - Plants/Supplies Safety Clothing/Equipment
F059565 F059568 F059570 F059571 F059572 F059577 F059578 F059579 F059580 F059586	19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18	00475 00627 00634 00653 00697	Jason Signmakers Johns Building Supplies Pty Ltd Humes - Holcim (Australia) Pty Ltd Landmark Operations Limited	79.19 2,233.00 2,323.42 105.60	Building Material Concrete Products Gardening - Plants/Supplies
F059563 F059565 F059568 F059570 F059571 F059572 F059577 F059578 F059579 F059580 F059580 F059581 F059591	19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18	00475 00627 00634 00653 00697 00850	Jason Signmakers Johns Building Supplies Pty Ltd Humes - Holcim (Australia) Pty Ltd Landmark Operations Limited Pacific Safety Wear	79.19 2,233.00 2,323.42 105.60 217.80	Building Material Concrete Products Gardening - Plants/Supplies Safety Clothing/Equipment
F059565 F059568 F059570 F059571 F059572 F059577 F059578 F059579 F059580 F059586 F059591 F059598	19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18	00475 00627 00634 00653 00697 00850 00936	Jason Signmakers Johns Building Supplies Pty Ltd Humes - Holcim (Australia) Pty Ltd Landmark Operations Limited Pacific Safety Wear Pro-Lamps Pty Ltd	79.19 2,233.00 2,323.42 105.60 217.80 3,432.00	Building Material Concrete Products Gardening - Plants/Supplies Safety Clothing/Equipment Lights & Light Fittings
F059565 F059568 F059570 F059571 F059572 F059577 F059578 F059579 F059580 F059586 F059591 F059598 F059598 F059602	19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18	00475 00627 00634 00653 00697 00850 00936 01183 01206	Jason Signmakers Johns Building Supplies Pty Ltd Humes - Holcim (Australia) Pty Ltd Landmark Operations Limited Pacific Safety Wear Pro-Lamps Pty Ltd Total Packaging (WA) Pty Ltd Access Icon Pty Ltd t/a Cascada	79.19 2,233.00 2,323.42 105.60 217.80 3,432.00 9,890.61	Building Material Concrete Products Gardening - Plants/Supplies Safety Clothing/Equipment Lights & Light Fittings Cleaning Products Concrete Products
F059565 F059568 F059570 F059571 F059572 F059577 F059578 F059579 F059580 F059580 F059591 F059598 F059598 F059602 F059607	19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18	00475 00627 00634 00653 00697 00850 00936 01183 01206 01263	Jason Signmakers Johns Building Supplies Pty Ltd Humes - Holcim (Australia) Pty Ltd Landmark Operations Limited Pacific Safety Wear Pro-Lamps Pty Ltd Total Packaging (WA) Pty Ltd Access Icon Pty Ltd t/a Cascada West Australian Newspapers Ltd	79.19 2,233.00 2,323.42 105.60 217.80 3,432.00 9,890.61 203.85	Building Material Concrete Products Gardening - Plants/Supplies Safety Clothing/Equipment Lights & Light Fittings Cleaning Products Concrete Products Publications/Newspapers
F059565 F059568 F059570 F059571 F059572 F059577 F059578 F059579 F059580 F059580 F059591 F059598 F059598 F059598	19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18 19/12/18	00475 00627 00634 00653 00697 00850 00936 01183 01206	Jason Signmakers Johns Building Supplies Pty Ltd Humes - Holcim (Australia) Pty Ltd Landmark Operations Limited Pacific Safety Wear Pro-Lamps Pty Ltd Total Packaging (WA) Pty Ltd Access Icon Pty Ltd t/a Cascada	79.19 2,233.00 2,323.42 105.60 217.80 3,432.00 9,890.61 203.85 123.25	Building Material Concrete Products Gardening - Plants/Supplies Safety Clothing/Equipment Lights & Light Fittings Cleaning Products Concrete Products

	Date	CR_Code	Supplier	Pmnt_Amnt	Description A632
Pmnt_Ref EF059619	19/12/18	01570	Blackwoods	_	Hardware Description 71002
EF059628	19/12/18	02088	Lock Stock & Farrell Locksmith		Hardware
EF059633	19/12/18	02365	Officemax Australia Ltd		Stationery & Printing
EF059638	19/12/18	02363	A1 Steel & Alloy		Metal Goods
EF059641	19/12/18	02459	Quick Corporate Australia Pty Ltd		Stationery & Printing
		02922			
EF059643	19/12/18		United Fasteners		Hardware
F059647	19/12/18	03197	West Coast Turf		Gardening - Plants/Supplies
F059650	19/12/18	03552	Perth Safety Products Pty Ltd		Safety Clothing/Equipment
F059672	19/12/18	04996	Signman	70.00	•
F059677	19/12/18	05120	Crush		Stationery & Printing
EF059693	19/12/18	03010	City of Belmont Corporate Card Catering		Groceries
EF059698	19/12/18	04348	City of Belmont Corporate Card Events		Events Equipment, Advertising & Subscriptions
F059700	19/12/18	04471	Booktopia Pty Ltd		Books/CDs/DVDs
F059704	19/12/18	05385	Oh Clocks.com.au - Nice Enterprises Pty Ltd	229.00	Clock
F059697	19/12/18	04003	JB Hi-Fi	79.00	Stationery & Printing
F059779	20/12/18	05363	isupply Plywood	790.00	Building Material
F059711	20/12/18	00203	BOC Gases Australia Ltd	125.66	Welding Equipment/Supplies
F059722	20/12/18	01115	Supa IGA Belmont Belvidere Street	1,868.29	Groceries
F059730	20/12/18	01771	Arborwest Tree Farm	1,155.00	Gardening - Plants/Supplies
F059731	20/12/18	01906	Frazzcon Enterprises	2,569.96	- · · · · · · · · · · · · · · · · · · ·
F059734	20/12/18	02198	W A & J King Pty Ltd		Gardening - Plants/Supplies
F059737	20/12/18	02320	Ambius Indoor Plants		Gardening - Plants/Supplies
F059740	20/12/18	02431	ASB Marketing Pty Ltd		Uniforms
F059744	20/12/18	02431	Eclipse Soils Pty Ltd		Sand/Soil
			· · · · · · · · · · · · · · · · · · ·	,	Uniforms
F059749	20/12/18	04053	Totally Workwear	-,	
F059769	20/12/18	05096	IntelligenceBank Pty Ltd		Computer Software
Nih au	Materials Total			145,391.54	•
Other					2019 Adachi student delegation
87976	07/12/18	00290	City of Belmont Municipal Account	4,000.00	tour to Japan - cash advance
87977	07/12/18	00388	Housing Authority	682.25	Rate Refund
87978	07/12/18	00889	Petty Cash - Finance	921.75	Petty Cash Recoup
87979	07/12/18	00890	Petty Cash - Harman Park Adult Day Care		Petty Cash Recoup
87985	07/12/18	99999	M A E Saldanha		Mediation Fee Contribution
87986	07/12/18	99999	Coryn Peta Keith		Rate Refund
87987	07/12/18	99999	John William Lugg		Rate Refund
87988	07/12/18	99999	Jorge Alquimedes Soria		Rate Refund
87989	07/12/18	99999	The Estate of Samuel Ferris		Rate Refund
	07/12/18	165026	Karen Welfare Association of WA Inc		
F059282					Bond Payment/Refund
F059283	07/12/18	165598	Aziz Serat		Bond Payment/Refund
F059284	07/12/18	166259	Salini Impregilo-NRW JV		Bond Payment/Refund
F059285	07/12/18	166384	Nikki and Jerko Jerry Zuvela		Bond Payment/Refund
EF059286	07/12/18	166439	Swordfish Fencing Club Inc		Bond Payment/Refund
EF059287	07/12/18	166588	Cody Fullbrook		Bond Payment/Refund
EF059288	07/12/18	166591	Bakyt Abishbayeva		Bond Payment/Refund
EF059289	07/12/18	166593	Linda Barber	400.00	Bond Payment/Refund
F059290	07/12/18	166596	Ertha Andi	400.00	Bond Payment/Refund
F059291	07/12/18	166597	Ralene Fielding	1,000.00	Bond Payment/Refund
F059292	07/12/18	166599	Homeless and Abused Animal Rescue Team	1,000.00	Bond Payment/Refund
F059293	07/12/18	166601	Yolanda Trigger	400.00	Bond Payment/Refund
F059297	07/12/18	00238	Ruby Benjamin Animal Foundation Inc		Donation
F059315	07/12/18	01396	Volunteering WA		Membership Fee
F059405	07/12/18	05380	Garage Sale Trail Foundation Ltd		Membership Fee
F059410	07/12/18	05391	Belmont City Physiotherapy Clinic		Belmont Business Innovation Grant
F059412	07/12/18	05400	Lisa Bretnall		Adachi student delegation gifts
		99998			5 5
F059418	07/12/18		Belmont Forum Shopping Centre PL		Rate Refund (reduced GRV - demolition of shops) Rate Refund
F059419	07/12/18	99998	Century 21 Wentworth Real Estate		
F059420	07/12/18	99998	Rahmatullah Layeghi		Rate Refund
F059421	07/12/18	99998	Paige Craven Bower		Sporting Trip Donation
F059422	07/12/18	99998	SSB Pty Ltd		Application Fee Refund
F059423	07/12/18	99998	Perry Lau		Application Fee Refund
F059424	07/12/18	99998	Toddville Prospecting Pty Ltd		Application Fee Refund
F059334	07/12/18	02666	Vic Bijl		Quantified Tree Risk Assessment membership
87990	14/12/18	00890	Petty Cash - Harman Park Adult Day Care	179.55	Petty Cash Recoup
87993	14/12/18	99999	Francis James Beake	151.79	Rate Refund
F059425	14/12/18	166424	Kalamunda Christian School	400.00	Bond Payment/Refund
F059426	14/12/18	166590	Morgan Kinsella	400.00	Bond Payment/Refund
F059427	14/12/18	166594	Paolo Di Bucci		Bond Payment/Refund
F059428	14/12/18	166598	Independent Living Centre WA		Bond Payment/Refund
F059429	14/12/18	166600	Jyotsana Pradhan		Bond Payment/Refund
F059430	14/12/18	166605	Cheryl Blanton		Bond Payment/Refund
F059431	14/12/18	166606	Kingz Production	400.00	Bond Payment/Refund
F059432	14/12/18	166608	Cheng Keng Li		Bond Payment/Refund
F059439	14/12/18	00242	Cabcharge Australia Pty Ltd		Taxi Fares
F059439 F059444	14/12/18	00242	Robin Garrett		
					CPA Membership Fee & Phone/Internet expense
F059449	14/12/18	00797	LGIS Workcare		Insurance Premiums
F059457	14/12/18	01236	Department of Fire & Emergency Services		Emergency Services Levy
	14/12/18	05269	Great Aussie Patios		Application Fee Refund
	14/12/18	99998	Anystate Renovations & Repairs		Application Fee Refund
	14/12/18	99998	Home Group WA Pty Ltd	467.15	Application Fee Refund
F059543		99998	New Sensation Homes Pty Ltd	624.00	Application Fee Refund
F059543 F059544	14/12/18				Data Dafund
F059543 F059544 F059545	14/12/18 14/12/18	99998	RMAX a div of Huntsman Chemical Co	5,920.56	Rate Refund
F059543 F059544 F059545 F059546			RMAX a div of Huntsman Chemical Co Kaiden Pisani		
F059543 F059544 F059545 F059546 F059547	14/12/18 14/12/18	99998		400.00	Sporting Trip Donation Membership Fee
F059543 F059544 F059545 F059546 F059547 F059629	14/12/18 14/12/18 19/12/18	99998 99998 02126	Kaiden Pisani Treenet Inc	400.00 1,600.00	Sporting Trip Donation Membership Fee
F059543 F059544 F059545 F059546 F059547 F059629 F059690	14/12/18 14/12/18 19/12/18 19/12/18	99998 99998 02126 00446	Kaiden Pisani Treenet Inc CPA Australia Ltd	400.00 1,600.00 720.00	Sporting Trip Donation Membership Fee Membership Fee - S Monks
EF059521 EF059543 EF059544 EF059545 EF059546 EF059629 EF059690 EF059692 EF059694	14/12/18 14/12/18 19/12/18	99998 99998 02126	Kaiden Pisani Treenet Inc	400.00 1,600.00 720.00 596.00	Sporting Trip Donation Membership Fee

Pmnt Ref	Date	CR_Code	Supplier	Pmnt_Amnt	Description A633
787994	20/12/18	00890	Petty Cash - Harman Park Adult Day Care	_	Petty Cash Recoup
787999	20/12/18	99999	David Borg	55.00	Application Fee Refund
'88000	20/12/18	99999	JB Homes Pty Ltd	1,728.50	Application Fee Refund
F059705	20/12/18	166287	Perth Zimbabwean Methodist Church Inc		Bond Payment/Refund
F059706	20/12/18	166586	Masters Athletics WA Inc		Bond Payment/Refund
F059707	20/12/18	166602	Asmita Shrestha		Bond Payment/Refund
F059708	20/12/18	166611	Nicole Louise Rosalia		Bond Payment/Refund
. 000700	20/12/10	100011	1410010 Edulos 1400ulla		Accommodation Support and Small Business
F059710	20/12/18	00169	Belmont Business Enterprise Centre Inc	43,269.40	Awards Sponsorship
F059712	20/12/19	00242	Cababarga Australia Pty Ltd	22.50	Taxi Fares - P Marks
	20/12/18	00242	Cabcharge Australia Pty Ltd		
F059724	20/12/18	01236	Department of Fire & Emergency Services		Emergency Services Levy
F059727	20/12/18	01538	Department of Justice - Fines Enforcement Registry		Reimbursements
F059756	20/12/18	04426	Crawford Connell		TravelSmart Active Be Rewarded gift vouchers
F059789	20/12/18	99998	Complete Approvals		Application Fee Refund
F059790	20/12/18	99998	Lhasa Hatch		Application Fee Refund
F059791	20/12/18	99998	Liya Yi	55.00	Application Fee Refund
F059792	20/12/18	99998	Plunkett Homes	1,090.33	Application Fee Refund
F059793	21/12/18	166612	Omran Tiba	400.00	Bond Payment/Refund
	Other Total			914,069.19	<u>_</u>
roperty, Plant	t & Equipment				-
F059673	19/12/18	05010	Kyocera Document Solutions Australia	698.50	Office Equipment
F059668	19/12/18	04906	E P Draffin Manufacturing Pty Ltd	10,728.30	Street Furniture
	Property, Plant	& Equipme	<u> </u>	11,426.80	
alaries/Wages					
F059278	05/12/18	99971	Superchoice	129.712.41	Superannuation Contribution
VG051218	06/12/18	COB	City of Belmont Payroll		Salaries/Wages
F059413	07/12/18	99950	Australian Services Union		Salaries/Wages
F059414	07/12/18	99952	Child Support Agency		Salaries/Wages
F059415	07/12/18	99954	City of Belmont Social Club		Salaries/Wages
F059415	07/12/18	99960	Health Insurance Fund of WA		Salaries/Wages Salaries/Wages
F059417	07/12/18	99962	LGRCEU - WA Shire Councils Union		Salaries/Wages Salaries/Wages
L121218	13/12/18	COB			Salaries/Wages Salaries/Wages
			City of Belmont Payroll		•
F059538	14/12/18	99950	Australian Services Union		Salaries/Wages
F059539	14/12/18	99952	Child Support Agency		Salaries/Wages
F059540	14/12/18	99954	City of Belmont Social Club		Salaries/Wages
F059541	14/12/18	99960	Health Insurance Fund of WA		Salaries/Wages
F059542	14/12/18	99962	LGRCEU - WA Shire Councils Union	153.76	Salaries/Wages
F059548	17/12/18	99971	Superchoice	126,447.25	Superannuation Contribution
SL261218	19/12/18	COB	City of Belmont Payroll	627,909.13	Salaries/Wages
VG181218	19/12/18	COB	City of Belmont Payroll	155,629.16	Salaries/Wages
VG191218	19/12/18	COB	City of Belmont Payroll	202.05	Salaries/Wages
EF059783	20/12/18	99950	Australian Services Union	77.70	Salaries/Wages
F059784	20/12/18	99952	Child Support Agency	541.67	Salaries/Wages
F059785	20/12/18	99954	City of Belmont Social Club		Salaries/Wages
EF059786	20/12/18	99960	Health Insurance Fund of WA		Salaries/Wages
EF059787	20/12/18	99962	LGRCEU - WA Shire Councils Union		Salaries/Wages
21 000101	Salaries/Wages		ESTABLE WAY STILL GOLD STILL	1,827,423.98	_ Galarico/Wagoo
raining and C		· otal		1,021,120.00	≣
_					Project Management Workshop - V Popescu
F059378	07/12/18	05063	Australian Institute of Project Management	200.00	- registration
F059295	07/12/18	00110	Australian Institute of Management	382.00	Project Management Workshop - V Popescu
F059301	07/12/18	00601	Institute of Public Works Engineering Sydney	297.00	- registration Asset Management seminar - S Brennan
					- registration WA Down South Playspace Technical Tour
F059460	14/12/18	01413	Parks & Leisure Australia	176.00	- G Healy - registration
F059465	14/12/18	01605	ATM Australian Training Management	1,754.21	Mobile Plant Training & Assessments
F059466	14/12/18	01660	Local Government Planners Assoc	65.00	Strategic Planning seminar - J Hammah - registration
	40/40/40	00500	City of Belmont Corporate Card	202.5-	Local Government Chief Officers' Group meeting
F059695	19/12/18	03526	Executive Assistant Mayor/CEO	690.85	- J Christie - Trinity Wharf accommodation
F059640	19/12/18	02719	Aveling	110.00	Staff Safety Inductions
F059691	19/12/18	00762	Qantas Airways Limited		AMAC Executive meeting - P Marks - airfare
F059696	19/12/18	03580	Virgin Australia		AMAC Executive meeting - P Marks - airfare
F059701	19/12/18	05121	City of Belmont Corporate Card CEO		Local Government Chief Officers' Group Conference - J Christie - accommodation,
500701	10/12/10	00121	on, or bollion composate data of	1,004.00	meals, car hire
F059702	19/12/18	05185	LGPA Perth WA	65.00	Strategic Planning seminar - J Davis - registration
EF059788	20/12/18	99998	Aust Aged Care Quality Agency	352.00	Aged Care New Standards webinar - M Taylor - registration
	Training and Co	nferences '	<b>Fotal</b>	6,497.74	
//UNI Total				6,314,108.49	
rust Funds					
F059279	06/12/18	150748	Building & Construction Industry Training Levy	65 006 00	Building & Construction Industry Training Levy
F059279	06/12/18	154102	Building Commission Building Services Levy		Building Commission Building Services Levy
F059281	06/12/18	164040	Department of Planning DAP fees		Department of Planning DAP fees
F059433	13/12/18	164040	Department of Planning DAP fees  Department of Planning DAP fees		Department of Planning DAP fees  Department of Planning DAP fees
-1 009433	Trust Funds To		Department of Frankling DAF 1885	120,431.28	Department of Fiantiling DAF ites
DUST Total	Trust Fullus 10	ıaı			
RUST Total				120,431.28	
rand Total				6,434,539.77	

Breakdown - Cheques : EFT : 195,913.00

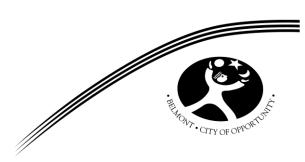
6,238,626.77



Item 12.13 refers

**Attachment 29** 

Accounts for Payment January 2019



Compiled: 04/02/19 11:33

### City of Belmont

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### **Accounts for Payment - January 2019**

Pmnt_Ref	Date	CR Code	Supplier	Pmnt_Amnt	Description
Contractors	Date	OI\_OOGE	оприне	i iiiii_Aiiiiii	Description
EF059795	02/01/19	00295	Capital Recycling	2,540.60	Rubbish Removals
EF059796	02/01/19	00313	Coates Hire Operations Pty Ltd	97.02	Plant/Equipment Hire
EF059798	02/01/19	00412	Dowsing Group Pty Ltd		Concrete Contractor
EF059799	02/01/19	00491	Fuji Xerox Australia Pty Ltd		Photocopy Expenses
EF059800	02/01/19	00736 01002	McLeods  PAC Puningsowing Vehicle Progledowns		Legal Expenses
EF059801 EF059802	02/01/19 02/01/19	01002	RAC Businesswise Vehicle Breakdowns Sledgehammer Concrete Cutting Service		Plant Parts & Repairs Concrete Contractor
EF059803	02/01/19	01221	Landgate		Valuation Expense
EF059804	02/01/19	01721	Fulton Hogan Industries		Road Building Contractor
EF059805	02/01/19	01976	Ecoscape Australia Pty Ltd		Professional Fees - Landscaping
EF059806	02/01/19	02229	Belgravia Leisure Pty Ltd	4,804.00	Oasis Expenses
EF059811	02/01/19	03790	Storage King Perth Airport		Records Storage
EF059813	02/01/19	04105	Cleanflow Environmental Solutions	,	Drainage Maintenance
EF059814	02/01/19	04137	Greive Panelbeaters		Plant Parts & Repairs
EF059815 EF059816	02/01/19 02/01/19	04320 04337	ABM Landscaping Gary Lynn		Bricks/Bricklaying Library - Entertainment Expense
EF059817	02/01/19	04357	Full Circle Design Services		Professional Fees - Building
EF059819	02/01/19	04693	Allwest Plant Hire Australia Pty Ltd		Plant/Equipment Hire
EF059822	02/01/19	05239	Majestic Plumbing Pty Ltd		Plumbing Maintenance/Supplies
EF059823	02/01/19	05287	Carbine Trailers	17,600.00	Plant/Equipment Hire
EF059824	02/01/19	05353	United Equipment Pty Ltd		Plant/Equipment Hire
EF059825	02/01/19	05370	OKMG Pty Ltd		Photography/Framing Expenses
EF059827	02/01/19	05382	McGees Property		Valuation Expense
EF059828 EF059837	02/01/19 04/01/19	05394 00491	DFP Recruitment Services Pty Ltd Fuji Xerox Australia Pty Ltd		Labour/Personnel Hire Photocopy Expenses
EF059838	04/01/19	00707	LoGo Appointments		Labour/Personnel Hire
EF059839	04/01/19	00736	McLeods	,	Legal Expenses
EF059841	04/01/19	01082	Sparks Refrigeration & Airconditioning		Airconditioning/Refrigeration Maintenance
EF059842	04/01/19	01090	St John Ambulance Australia Inc	277.20	First Aid Service
EF059843	04/01/19	01170	Relay Concrete	990.00	Concrete Contractor
EF059844	04/01/19	01393	Comestibles		Catering/Catering Supplies
EF059845	04/01/19	01614	Maxwell Robinson & Phelps		Pest Control
EF059846 EF059847	04/01/19 04/01/19	01797 01976	Green Skills (Ecojobs)		Labour/Personnel Hire
EF059849	04/01/19	02229	Ecoscape Australia Pty Ltd Belgravia Leisure Pty Ltd		Professional Fees - Landscaping Oasis Expenses
EF059851	04/01/19	02298	Pelican Linemarking		Line Marking
EF059852	04/01/19	02393	Zipform Pty Ltd		Rate Notice Production and Postage
EF059854	04/01/19	02918	Hospitality Accessories	192.50	Catering/Catering Supplies
EF059855	04/01/19	03194	Datatel Electrical & Communications	3,121.27	Electrical Contractor
EF059856	04/01/19	03504	Classic Tree Services		Gardening Contractor
EF059857	04/01/19	03571	Perrott Painting Maintenance Contracts		Painting Contractor
EF059858	04/01/19	03854	Invision Investigations & Consulting		Professional Fees - Human Resources
EF059860 EF059862	04/01/19 04/01/19	04253 04454	Camstruct Design & Build Pty Ltd FM Contract Solutions Pty Ltd		Building Construction Professional Fees - Analysis
EF059864	04/01/19	04496	Azure Painting Pty Ltd		Painting Contractor
EF059865	04/01/19	04579	Mills Resources - Octet Finance Pty Ltd		Labour/Personnel Hire
EF059867	04/01/19	04772	ATM Advanced Traffic Management		Plant/Equipment Hire
EF059870	04/01/19	04917	Environmental Industries Pty Ltd	12,912.36	Professional Fees - Landscaping
EF059871	04/01/19	04974	Turf Care WA Pty Ltd	,	Gardening Contractor
EF059872	04/01/19	05016	Cyclus Pty Ltd		Labour/Personnel Hire
EF059874	04/01/19	05283	IRP Industrial Recruitment Partners		Labour/Personnel Hire
EF059875 EF059877	04/01/19 04/01/19	05369 05394	Ace Security & Event Svcs DFP Recruitment Services Pty Ltd		Security Services Labour/Personnel Hire
EF059878	04/01/19	05406	Guardall Security		Security Services
EF059879	04/01/19	05411	Rubicor Gov Pty Ltd		Labour/Personnel Hire
EF059880	04/01/19	05417	Nadia Collins		Music/Entertainment Expenses
EF059894	11/01/19	00163	Bayswater Fire Protection	225.50	Fire Equipment/Service
EF059897	11/01/19	00283	Coast to Coast the Golden Roast	,	Catering/Catering Supplies
EF059898	11/01/19	00295	Capital Recycling		Rubbish Removals
EF059901 EF059905	11/01/19	00491 00941	Fuji Xerox Australia Pty Ltd Carnival Amusements		Photocopy Expenses
EF059907	11/01/19 11/01/19	01002	RAC Businesswise Vehicle Breakdowns		Plant/Equipment Hire Plant Parts & Repairs
EF059912	11/01/19	01507	The Pressure King		Graffiti Removal
EF059914	11/01/19	02050	Austraffic W A		Traffic Control
EF059915	11/01/19	02078	Psyco Sand	6,413.00	Gardening Contractor
EF059917	11/01/19	02316	Ayres Tyre Service	300.00	Plant Parts & Repairs
EF059918	11/01/19	02322	AFL SportsReady Limited		Labour/Personnel Hire
EF059919	11/01/19	02330	Tomato Lake Cafe - Xing Guang Rao		Catering/Catering Supplies
EF059921	11/01/19	02860	Pristine Kleen Drycleaners		Cleaning Services
EF059924 EF059928	11/01/19 11/01/19	02933 03419	Q Engineering Designs Pty Ltd Gott Health		Professional Fees - Design Community Exercise Classes
EF059929	11/01/19	03655	Acclaimed Catering		Catering/Catering Supplies
EF059931	11/01/19	03736	Beth Mullany - West Oz Wildlife		Library - Entertainment Expense
EF059932	11/01/19	03816	Triple B International Pty Ltd		Home Care
EF059933	11/01/19	04320	ABM Landscaping		Bricks/Bricklaying
EF059937	11/01/19	04524	Moore Stephens		Audit Fee
EF059939	11/01/19	04555	MG Group WA		Building Construction
EF059941	11/01/19	04720	TPG Aged Care		Home Care
EF059942 EF059943	11/01/19	04783 04941	Coffey Services Australia Pty Ltd		Professional Fees - Engineering Pound Expenses
EF059943 EF059944	11/01/19 11/01/19	04941	Perth Pet Cremation Turf Care WA Pty Ltd		Gardening Contractor
EF059945	11/01/19	05049	HenderCare Pty Ltd		Home Care
EF059946	11/01/19	05127	Champion Music		Music/Entertainment Expenses
EF059948	11/01/19	05177	Thrifty Car & Truck Rentals WA	989.78	Plant/Equipment Hire

Pmnt_Ref	Date	CR_Code	Supplier	Pmnt_Amnt	Description A636
059949	11/01/19	05212	Turnpoint Software Pty Ltd	_	Computer Software Maintenance
059950	11/01/19	05344	SUEZ Recycling and Recovery Pty Ltd		Rubbish Removals
059951	11/01/19	05370	OKMG Pty Ltd		Photography/Framing Expenses
059952	11/01/19	05373	Imagineaction - Kitsiri DeSilva		Library - Entertainment Expense
059953	11/01/19	05381	Metro Traffic Planning Pty Ltd		Traffic Control
059925	11/01/19	02985	Bank of IDEAS	11,000.00	Professional Fees - Multicultural Engagement Strategy
059970	18/01/19	00118	Australia Post	3,616.85	•
059972	18/01/19	00204	Bollig Design Group Pty Ltd		Professional Fees - Architect
059974	18/01/19	00295	Capital Recycling		Rubbish Removals
059975	18/01/19	00309	Cleanaway	13,686.12	Rubbish Removals
059976	18/01/19	00346	Action Couriers	346.29	Courier Service
059980	18/01/19	00686	Listech Pty Ltd	1,039.50	Computer Software Maintenance
059981	18/01/19	00707	LoGo Appointments		Labour/Personnel Hire
059982	18/01/19	00818	Morries Backhoe & Plant Hire		Plant/Equipment Hire
059983	18/01/19	00943	Cirrena Pty Ltd		Computer Software Maintenance
059985	18/01/19	01082	Sparks Refrigeration & Airconditioning		Airconditioning/Refrigeration Maintenance
059986	18/01/19 18/01/19	01170	Relay Concrete		Concrete Contractor
059987 059989	18/01/19	01221 01476	Landgate Hays Specialist Recruitment		Valuation Expense Labour/Personnel Hire
059990	18/01/19	01507	The Pressure King		Graffiti Removal
059992	18/01/19	01693	Gallery 360	,	Photography/Framing Expenses
059993	18/01/19	02078	Psyco Sand		Gardening Contractor
059995	18/01/19	02229	Belgravia Leisure Pty Ltd		Oasis Expenses
059997	18/01/19	02411	Allsports Linemarking		Line Marking
059998	18/01/19	02425	Prestige Alarms		Security Services
060001	18/01/19	02711	CPG Research & Advisory Pty Ltd		Professional Fees - Analysis
060002	18/01/19	02837	GLG Greenlife Group		Gardening Contractor
060003	18/01/19	03201	Artistralia Pty Ltd		Movie Screening Copyright Fees
060005	18/01/19	03614	Julies Boarding Kennels & Cattery	2,347.50	Pound Expenses
060006	18/01/19	03619	Kidsafe W A		Playground Inspections/Repairs
060009	18/01/19	04131	Total Green Recycling Pty Ltd		Rubbish Removals
060010	18/01/19	04274	Perth Office Equipment Repairs		Office Equipment Maintenance
060012	18/01/19	04579	Mills Resources - Octet Finance Pty Ltd		Labour/Personnel Hire
060014 060015	18/01/19 18/01/19	04720 04772	TPG Aged Care ATM Advanced Traffic Management	,	Home Care Plant/Equipment Hire
060015	18/01/19	04772	Cameron Hunt		Building Construction
060017	18/01/19	04976	Redstar Equipment Supplies		Plant Parts & Repairs
060017	18/01/19	04986	Jan McCahon Marshall		Oral Historian
060022	18/01/19	05101	De Lage Landen Pty Ltd		Plant/Equipment Hire
060023	18/01/19	05150	PACT Construction Pty Ltd		Building Construction
060024	18/01/19	05205	N and H Sanders		Floor Coverings
060025	18/01/19	05215	The Retic & Landscape Shop		Reticulation Parts & Repairs
060026	18/01/19	05283	IRP Industrial Recruitment Partners	619.08	Labour/Personnel Hire
060027	18/01/19	05293	Bellrock Cleaning Services - Scottish Pacific	37,480.42	Cleaning Services
060028	18/01/19	05303	Veraison	1,386.00	Professional Fees - Analysis
060030	18/01/19	05383	Synaco Global Recruitment Pty Ltd	3,744.56	Labour/Personnel Hire
060031	18/01/19	05394	DFP Recruitment Services Pty Ltd		Labour/Personnel Hire
060032	18/01/19	05403	Sean Conway Comedy		Music/Entertainment Expenses
060033	18/01/19	05411	Rubicor Gov Pty Ltd		Labour/Personnel Hire
F060035 F060036	18/01/19 18/01/19	05416 05422	Tag Snaps Events Rib Worx		Music/Entertainment Expenses Plant Parts & Repairs
059978	18/01/19	00557	City Subaru		Plant Parts & Repairs
060054	25/01/19	00204	Bollig Design Group Pty Ltd		Professional Fees - Architect
060055	25/01/19	00230	Jackson McDonald		Legal Expenses
060056	25/01/19	00256	Cardile International Fireworks		Music/Entertainment Expenses
060057	25/01/19	00390	Landgate		Title Searches
060059	25/01/19	01276	Westside Fire Services	,	Fire Equipment/Service
060061	25/01/19	01447	Initial Hygiene	1,146.30	Cleaning Services
060064	25/01/19	01675	Horizons West Bus & Coachlines	572.00	Plant/Equipment Hire
060065	25/01/19	02155	Tim Davies Landscaping Pty Ltd		Gardening Contractor
060072	25/01/19	03504	Classic Tree Services		Gardening Contractor
060073	25/01/19	03655	Acclaimed Catering		Catering/Catering Supplies
060074	25/01/19	03816	Triple B International Pty Ltd		Home Care
060076	25/01/19	04062	Grant Thornton Audit Pty Ltd		Audit Fee
060077	25/01/19	04211	Advance Scanning Services Southern Cross Housing Ltd		Survey Expenses
060078	25/01/19	04302 04454	Southern Cross Housing Ltd  EM Contract Solutions Ptv Ltd		Independent Living Units Management
060080 060082	25/01/19 25/01/19	04454 04529	FM Contract Solutions Pty Ltd Southern Cross Care (WA) Inc		Professional Fees - Analysis Independent Living Units Management
060083	25/01/19	04529	One 20 Productions		Plant/Equipment Hire
060085	25/01/19	04779	Turf Care WA Pty Ltd		Gardening Contractor
060086	25/01/19	05016	Cyclus Pty Ltd		Labour/Personnel Hire
060089	25/01/19	05352	Eco Faeries		Music/Entertainment Expenses
060090	25/01/19	05368	Complete Hire & Sales Pty Ltd		Plant/Equipment Hire
060091	25/01/19	05382	McGees Property		Valuation Expense
060092	25/01/19	05383	Synaco Global Recruitment Pty Ltd	4,465.12	Labour/Personnel Hire
060095	25/01/19	05429	The Hire Guys Balcatta	1,237.50	Plant/Equipment Hire
060096	25/01/19	05433	Raptrap Entertainment		Music/Entertainment Expenses
060097	25/01/19	05436	Pooltime Belmont		Building Maintenance
060105	30/01/19	00013	Air-Met Scientific Pty Ltd		Plant Parts & Repairs
060106	30/01/19	00083	Ascot Veterinary Hospital		Pound Expenses
060107	30/01/19	00086	Aslab Pty Ltd		Professional Fees - Testing
060109	30/01/19	00147	Dormakaba Australia Pty Ltd		Building Maintenance
060110	30/01/19	00166	BSC Motion Technology Bearing Service		Plant Parts & Repairs
060111	30/01/19	00168	Boya Equipment  Pio Both Australia Phyl I d		Plant Parts & Repairs
060112	30/01/19	00195	Bin Bath Australia Pty Ltd		Cleaning Services
060115	30/01/19	00295	Capital Recycling	,	Rubbish Removals
060118 060121	30/01/19 30/01/19	00313 00391	Coates Hire Operations Pty Ltd Chemistry Centre (WA) t/as ChemCentre		Plant/Equipment Hire Professional Fees - Testing
/UU 14 I	30/01/19	00391	Paragon Window Tinting, Signage & Graphics		Window Treatments

					A C O 7
Pmnt_Ref	Date	CR_Code	Supplier	Pmnt_Amnt	Description A637
EF060124 EF060126	30/01/19 30/01/19	00412 00424	Dowsing Group Pty Ltd Eastern Metropolitan Regional Council		Concrete Contractor Rubbish Removals
EF060127	30/01/19	00424	Dial Before You Dig WA Limited	,	Survey Expenses
EF060129	30/01/19	00471	Filters Plus		Plant Parts & Repairs
F060131	30/01/19	00491	Fuji Xerox Australia Pty Ltd	2,837.47	Photocopy Expenses
F060133	30/01/19	00565	Hobart Food Equipment Pty Ltd	702.37	Plumbing Maintenance/Supplies
F060134	30/01/19	00606	International Rehabilitation & Soilstabilisation		Gardening Contractor
F060135	30/01/19	00608	Programmed Integrated Workforce Ltd	•	Labour/Personnel Hire
F060137	30/01/19	00665	Kennards Hire Pty Ltd	,	Plant/Equipment Hire
F060138 F060139	30/01/19 30/01/19	00718 00734	Major Motors Pty Ltd McIntosh & Son WA		Plant Parts & Repairs Plant Parts & Repairs
F060140	30/01/19	00734	McLeods		Legal Expenses
F060141	30/01/19	00738	Lloyd George Acoustics Pty Ltd		Professional Fees - Testing
F060142	30/01/19	00783	iSentia Pty Ltd		Professional Fees - Marketing
F060143	30/01/19	00815	New Town Toyota		Plant Parts & Repairs
F060144	30/01/19	00830	Oce Australia - Canon Group Company	145.52	Photocopy Expenses
F060146	30/01/19	00858	Park Motor Body Builders		Plant Parts & Repairs
F060147	30/01/19	00906	Pirtek Welshpool		Plant Parts & Repairs
F060148	30/01/19	00927	Professional Glass & Maintenance		Building Maintenance
F060149	30/01/19	00931	Sonic HealthPlus Pty Ltd		Medical Examinations
F060150 F060151	30/01/19 30/01/19	00972 00988	Repco Auto Parts Reece Australia Pty Ltd		Plant Parts & Repairs Plumbing Maintenance/Supplies
F060151	30/01/19	01059	Sledgehammer Concrete Cutting Service	,	Concrete Contractor
F060153	30/01/19	01033	Sunny Industrial Brushware		Plant Parts & Repairs
F060155	30/01/19	01149	The Lifting Company Pty Ltd		Plant Parts & Repairs
F060156	30/01/19		T L Engineering (Aust) Pty Ltd		Plant Parts & Repairs
F060157	30/01/19	01186	ZircoDATA Pty Ltd		Records Storage
F060158	30/01/19	01233	Stihl Shop Redcliffe		Tools/Tool Repairs
F060160	30/01/19	01243	WARP Group	104,113.89	Traffic Control
F060161	30/01/19	01255	Wattleup Tractors		Plant Parts & Repairs
F060162	30/01/19	01318	Flexi Staff Pty Ltd	,	Labour/Personnel Hire
F060166	30/01/19	01498	Autosweep Industrial Sweeping		Plant/Equipment Hire
F060167	30/01/19		WC Convenience Management Pty Ltd		Building Maintenance
F060169	30/01/19	01705	Econo-Mow Lawn & Garden Care		Home Care - Garden Service
F060170 F060171	30/01/19 30/01/19	01712 01714	Donegan Enterprises Pty Ltd Total Eden Pty Ltd		Gardening Contractor Reticulation Parts & Repairs
F060171 F060172	30/01/19	01714	Fulton Hogan Industries		Reduction Parts & Repairs  Road Building Contractor
F060172 F060173	30/01/19	01721	DVG Burswood Honda		Plant Parts & Repairs
F060175	30/01/19	01976	Ecoscape Australia Pty Ltd		Professional Fees - Landscaping
F060177	30/01/19	02023	YMCA Perth Youth & Community Services Inc		Youth Services Expenses
F060178	30/01/19	02049	Noise & Vibration Measurement Systems		Plant Parts & Repairs
F060180	30/01/19	02207	Wilson Security	113,960.18	Security Services
F060181	30/01/19	02316	Ayres Tyre Service	10,320.00	Plant Parts & Repairs
F060182	30/01/19		AFL SportsReady Limited	- /	Labour/Personnel Hire
F060183	30/01/19	02387	Triton Electrical Contractors Pty Ltd		Electrical Contractor
F060184	30/01/19	02425	Prestige Alarms		Security Services
F060186	30/01/19	02779	Natural Area Holdings Pty Ltd		Gardening Contractor
F060187 F060188	30/01/19 30/01/19	02941 03194	Taman Tools - Quality Nominees Pty Ltd Datatel Electrical & Communications		Tools/Tool Repairs Electrical Contractor
F060190	30/01/19	03194	Spraymaster Spray Shop		Tools/Tool Repairs
F060191	30/01/19	03366	Daimler Trucks Perth		Plant Parts & Repairs
F060193	30/01/19	03824	Konica Minolta		Photocopy Expenses
F060196	30/01/19	04146	JB Hi-Fi Group	2,117.42	Mobile Phones & Electrical Goods
F060197	30/01/19	04309	IAS Fine Art Logistics Pty Ltd	420.77	Records Storage
F060198	30/01/19	04320	ABM Landscaping	8,438.10	Bricks/Bricklaying
F060199	30/01/19	04340	WA Profiling / WA Stabilising	,	Road Building Contractor
F060200	30/01/19	04352	Grama Bazita Service & Maintenance		Fire Equipment/Service
F060201	30/01/19	04467	Rent a Fence Pty Ltd	5,890.50	
F060202 F060203	30/01/19 30/01/19	04477 04594	Place Laboratory Pty Ltd Website Weed & Past W A Pty Ltd	,	Professional Fees - Landscaping Weed Control
F060203 F060204	30/01/19	04594 04693	Website Weed & Pest W A Pty Ltd Allwest Plant Hire Australia Pty Ltd	,	Plant/Equipment Hire
F06020 <del>4</del> F060206	30/01/19	04963	Centigrade		Airconditioning/Refrigeration Maintenance
F060208	30/01/19	05083	Dent Dismissal		Plant Parts & Repairs
F060209	30/01/19	05109	Onsite Rental Group Operations Pty Ltd		Plant/Equipment Hire
F060210	30/01/19	05239	Majestic Plumbing Pty Ltd		Plumbing Maintenance/Supplies
F060211	30/01/19	05252	AAAC Towing Pty Ltd		Towing Vehicles
F060212	30/01/19	05294	Bardfield Engineering		Professional Fees - Engineering
F060213	30/01/19	05308	Modern Motor Trimmers		Plant Parts & Repairs
F060214	30/01/19	05336	West-Sure Group Pty Ltd		Security Services
F060215	30/01/19	05353	United Equipment Pty Ltd		Plant/Equipment Hire
F060216 F060217	30/01/19	05409 05414	Hosemasters International West Australian Opera		Plant Parts & Repairs
-060217 -060116	30/01/19 30/01/19	00301	West Australian Opera City Toyota		Library - Entertainment Expense Plant Parts & Repairs
F060132	30/01/19	00557	City Subaru		Plant Parts & Repairs
F060218	31/01/19	00204	Bollig Design Group Pty Ltd		Professional Fees - Architect
-060222	31/01/19	00491	Fuji Xerox Australia Pty Ltd		Photocopy Expenses
-060223	31/01/19	00585	Hydroquip Pumps		Bore Drilling/ Maintenance
-060224	31/01/19	00699	Marketforce Pty Ltd	12,498.36	Advertising and Printing
F060225	31/01/19	00707	LoGo Appointments		Labour/Personnel Hire
F060226	31/01/19	00717	Main Roads Western Australia		Road Building Contractor
F060227	31/01/19	00736	McLeods		Legal Expenses
F060229	31/01/19	00815	New Town Toyota		Plant Parts & Repairs
F060230	31/01/19	01082	Sparks Refrigeration & Airconditioning		Airconditioning/Refrigeration Maintenance
F060234	31/01/19	01476	Hays Specialist Recruitment  Pagish Patissoria, Cakes West Phyl Id.		Labour/Personnel Hire
F060235	31/01/19 31/01/19	01508 01614	Danish Patisserie - Cakes West Pty Ltd		Catering/Catering Supplies
EU6U332		01614 01797	Maxwell Robinson & Phelps Green Skills (Ecojobs)		Pest Control Labour/Personnel Hire
F060237 F060238	31/01/10				
F060238	31/01/19 31/01/19			,	
	31/01/19 31/01/19 31/01/19	02155 02229	Tim Davies Landscaping Pty Ltd Belgravia Leisure Pty Ltd	17,089.52	Gardening Contractor Oasis Expenses

					4.000
Pmnt_Ref	Date	CR_Code	• •	Pmnt_Amnt	Description A638
EF060242	31/01/19	02411	Allsports Linemarking		Line Marking
EF060246 EF060248	31/01/19 31/01/19	02918 03504	Hospitality Accessories Classic Tree Services		Catering/Catering Supplies Gardening Contractor
EF060249	31/01/19	04067	Taylor Burrell Barnett		Professional Fees - Planning
EF060250	31/01/19	04105	Cleanflow Environmental Solutions		Drainage Maintenance
EF060251	31/01/19	04246	Bibliotheca Australia Pty Ltd		Computer Software Maintenance
EF060252	31/01/19	04309	IAS Fine Art Logistics Pty Ltd		Records Storage
EF060253	31/01/19	04320	ABM Landscaping		Bricks/Bricklaying
EF060254 EF060255	31/01/19 31/01/19	04401 04482	Doublegee Entertainment Allan Davies & Trevor Chudleigh Architects		Music/Entertainment Expenses Professional Fees - Architect
EF060257	31/01/19	04496	Azure Painting Pty Ltd		Painting Contractor
EF060258	31/01/19	04565	Heritage Conservation Solutions		Professional Fees - Analysis
EF060259	31/01/19	04579	Mills Resources - Octet Finance Pty Ltd		Labour/Personnel Hire
EF060262	31/01/19	04772	ATM Advanced Traffic Management		Plant/Equipment Hire
EF060263	31/01/19	04783	Coffey Services Australia Pty Ltd		Professional Fees - Engineering
EF060266	31/01/19	04863	Cameron Hunt		Building Construction
EF060267 EF060268	31/01/19 31/01/19	04917 04974	Environmental Industries Pty Ltd Turf Care WA Pty Ltd		Professional Fees - Landscaping Gardening Contractor
EF060269	31/01/19	05038	Swan Blinds		Window Treatments
EF060270	31/01/19		HenderCare Pty Ltd		Home Care
EF060271	31/01/19	05053	Creative Flooring Pty Ltd	7,892.50	Floor Coverings
EF060272	31/01/19	05246	Kooda Pty Ltd	66.00	Rubbish Removals
EF060273	31/01/19	05293	Bellrock Cleaning Services - Scottish Pacific		Cleaning Services
EF060274	31/01/19	05383	Synaco Global Recruitment Pty Ltd	,	Labour/Personnel Hire
EF060275	31/01/19	05394	DFP Recruitment Services Pty Ltd	,	Labour/Personnel Hire
EF060276 EF060247	31/01/19 31/01/19	05401 03496	Creative Spaces Signs & Lines		Plant Parts & Repairs  Professional Foos Design
LI 000247	Contractors		Signs & Lines	3,157,037.12	Professional Fees - Design
Fuels and Ut				3,101,001.12	
EF059808	02/01/19	02422	Connect Call Centre Services		Phone/Internet expenses
788004	04/01/19	01142	Telstra Corporation Limited		Phone/Internet expenses
788005	11/01/19	00042	Alinta Energy		Light, Power, Gas
788007 788008	11/01/19 11/01/19	00392 01252	Department of Transport Water Corporation		Vehicle Licences Water, Annual & Excess
788009	11/01/19	01232	Synergy		Light, Power, Gas
EF059902	11/01/19	00573	Maureen Hooper		Volunteer Driver Fuel Allowance
EF059911	11/01/19	01488	ZettaNet Pty Ltd		Phone/Internet expenses
EF059920	11/01/19	02635	MessageMedia - Message4U Pty Ltd	33.00	Phone/Internet expenses
EF059923	11/01/19	02911	Dianne Dettlinger		Volunteer Driver Fuel Allowance
EF059926	11/01/19	03067	David McKinlay		Volunteer Driver Fuel Allowance
EF059934	11/01/19	04367	lan Smith		Volunteer Driver Fuel Allowance
EF059938 788013	11/01/19 18/01/19	04549 01142	Jan O'Mahoney Telstra Corporation Limited		Volunteer Driver Fuel Allowance Phone/Internet expenses
788014	18/01/19	01142	Water Corporation		Water, Annual & Excess
788015	18/01/19	01274	Synergy		Light, Power, Gas
EF059999	18/01/19	02471	Western Power		Belmont Ave - Underground Power prepayment
EF060000	18/01/19	02631	Caltex Energy WA	19,941.80	Fuel, Oil, Additives
788016	25/01/19	00042	Alinta Energy		Light, Power, Gas
788018	25/01/19	01142	Telstra Corporation Limited		Phone/Internet expenses
788019 788021	25/01/19 25/01/19	01252 01274	Water Corporation		Water, Annual & Excess Light, Power, Gas
EF060046	25/01/19	05249	Synergy Google Australia Pty Ltd		Phone/Internet expenses
EF060067	25/01/19	02422	Connect Call Centre Services		Phone/Internet expenses
788024	31/01/19	01274	Synergy		Light, Power, Gas
EF060228	31/01/19	00788	Motorcharge Ltd (Wright Express Aust)	13,430.66	Fuel, Oil, Additives
EF060243	31/01/19	02471	Western Power		Light, Power, Gas
EF060264	31/01/19	04837	Perth Energy Pty Ltd		Light, Power, Gas
Materials	Fuels and Ut	ilities Lotai		4,054,091.52	•
EF059794	02/01/19	00220	Burswood Trophies	305.80	Badges & Pendants
EF059810	02/01/19	03552	Perth Safety Products Pty Ltd		Safety Clothing/Equipment
EF059818	02/01/19	04607	Ink Station		Stationery & Printing
EF059820	02/01/19	04717	Perma Composites Pty Ltd		Building Material
EF059821	02/01/19	04843	Carbon Neutral Pty Ltd		Gardening - Plants/Supplies
EF059826 EF059848	02/01/19 04/01/19	05377 02043	Encyclopaedia Britannica Australia Ltd Risk Management & Safety Systems Pty Ltd		Computer Software Computer Software
EF059848 EF059850	04/01/19	02043	Party Frenzy / Perth Novelty Company		Promotional Items
EF059863	04/01/19	04491	Woolworths Limited (WA)		Groceries
EF059866	04/01/19	04639	Topline Foods		Groceries
EF059868	04/01/19	04805	Mai Flower Supplies		Flowers
			Accidental Health & Safety - Perth	1.574.44	Medical/First Aid Supplies
EF059873	04/01/19	05082			
EF059873 EF059861	04/01/19 04/01/19	04267	Auscore Fitness	4,422.88	•
EF059873 EF059861 EF059895	04/01/19 04/01/19 11/01/19	04267 00203	BOC Gases Australia Ltd	4,422.88 9.70	Welding Equipment/Supplies
EF059873 EF059861 EF059895 EF059904	04/01/19 04/01/19 11/01/19 11/01/19	04267 00203 00664	BOC Gases Australia Ltd Kmart Australia Limited	4,422.88 9.70 257.00	Welding Equipment/Supplies Kitchenware
EF059873 EF059861 EF059895 EF059904 EF059908	04/01/19 04/01/19 11/01/19	04267 00203	BOC Gases Australia Ltd	4,422.88 9.70 257.00 20.24	Welding Equipment/Supplies
EF059873 EF059861 EF059895 EF059904 EF059908 EF059909	04/01/19 04/01/19 11/01/19 11/01/19 11/01/19	04267 00203 00664 01074	BOC Gases Australia Ltd Kmart Australia Limited Shred-X Pty Ltd	4,422.88 9.70 257.00 20.24 279.40	Welding Equipment/Supplies Kitchenware Stationery & Printing
EF059873 EF059861 EF059895 EF059904 EF059908 EF059909 EF059913	04/01/19 04/01/19 11/01/19 11/01/19 11/01/19 11/01/19	04267 00203 00664 01074 01173	BOC Gases Australia Ltd Kmart Australia Limited Shred-X Pty Ltd Global Spill Control	4,422.88 9.70 257.00 20.24 279.40 296.34	Welding Equipment/Supplies Kitchenware Stationery & Printing Cleaning Products
EF059873 EF059861 EF059895 EF059904 EF059908 EF059909 EF059913 EF059922 EF059930	04/01/19 04/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19	04267 00203 00664 01074 01173 01780 02862 03660	BOC Gases Australia Ltd Kmart Australia Limited Shred-X Pty Ltd Global Spill Control Bodycare Workplace Solutions James Bennett Pty Ltd Safe T Card Australia Pty Ltd	4,422.88 9.70 257.00 20.24 279.40 296.34 298.78 528.00	Welding Equipment/Supplies Kitchenware Stationery & Printing Cleaning Products Publications/Newspapers Books/CDs/DVDs Safety Clothing/Equipment
EF059873 EF059861 EF059895 EF059904 EF059908 EF059909 EF059913 EF059922 EF059930 EF059935	04/01/19 04/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19	04267 00203 00664 01074 01173 01780 02862 03660 04394	BOC Gases Australia Ltd Kmart Australia Limited Shred-X Pty Ltd Global Spill Control Bodycare Workplace Solutions James Bennett Pty Ltd Safe T Card Australia Pty Ltd JB Hi-Fi Belmont Forum	4,422.88 9.70 257.00 20.24 279.40 296.34 298.78 528.00 1,199.94	Welding Equipment/Supplies Kitchenware Stationery & Printing Cleaning Products Publications/Newspapers Books/CDs/DVDs Safety Clothing/Equipment Books/CDs/DVDs
EF059873 EF059861 EF059895 EF059904 EF059908 EF059909 EF059913 EF059922 EF059930 EF059935 EF059936	04/01/19 04/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19	04267 00203 00664 01074 01173 01780 02862 03660 04394 04491	BOC Gases Australia Ltd Kmart Australia Limited Shred-X Pty Ltd Global Spill Control Bodycare Workplace Solutions James Bennett Pty Ltd Safe T Card Australia Pty Ltd JB Hi-Fi Belmont Forum Woolworths Limited (WA)	4,422.88 9.70 257.00 20.24 279.40 296.34 298.78 528.00 1,199.94 591.39	Welding Equipment/Supplies Kitchenware Stationery & Printing Cleaning Products Publications/Newspapers Books/CDs/DVDs Safety Clothing/Equipment Books/CDs/DVDs Groceries
EF059873 EF059861 EF059895 EF059904 EF059908 EF059909 EF059913 EF059922 EF059930 EF059935 EF059936 EF059940	04/01/19 04/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19	04267 00203 00664 01074 01173 01780 02862 03660 04394 04491 04607	BOC Gases Australia Ltd Kmart Australia Limited Shred-X Pty Ltd Global Spill Control Bodycare Workplace Solutions James Bennett Pty Ltd Safe T Card Australia Pty Ltd JB Hi-Fi Belmont Forum Woolworths Limited (WA) Ink Station	4,422.88 9.70 257.00 20.24 279.40 296.34 298.78 528.00 1,199.94 591.39 369.29	Welding Equipment/Supplies Kitchenware Stationery & Printing Cleaning Products Publications/Newspapers Books/CDs/DVDs Safety Clothing/Equipment Books/CDs/DVDs Groceries Stationery & Printing
EF059873 EF059861 EF059895 EF059904 EF059908 EF059909 EF059913 EF059922 EF059930 EF059930 EF059936 EF059940 EF059971	04/01/19 04/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19	04267 00203 00664 01074 01173 01780 02862 03660 04394 04491 04607 00203	BOC Gases Australia Ltd Kmart Australia Limited Shred-X Pty Ltd Global Spill Control Bodycare Workplace Solutions James Bennett Pty Ltd Safe T Card Australia Pty Ltd JB Hi-Fi Belmont Forum Woolworths Limited (WA) Ink Station BOC Gases Australia Ltd	4,422.88 9.70 257.00 20.24 279.40 296.34 298.78 528.00 1,199.94 591.39 369.29 129.86	Welding Equipment/Supplies Kitchenware Stationery & Printing Cleaning Products Publications/Newspapers Books/CDs/DVDs Safety Clothing/Equipment Books/CDs/DVDs Groceries Stationery & Printing Welding Equipment/Supplies
EF059873 EF059861 EF059895 EF059904 EF059908 EF059909 EF059913 EF059922 EF059930 EF059935 EF059936 EF059940 EF059971 EF059973	04/01/19 04/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 18/01/19	04267 00203 00664 01074 01173 01780 02862 03660 04394 04491 04607 00203 00220	BOC Gases Australia Ltd Kmart Australia Limited Shred-X Pty Ltd Global Spill Control Bodycare Workplace Solutions James Bennett Pty Ltd Safe T Card Australia Pty Ltd JB Hi-Fi Belmont Forum Woolworths Limited (WA) Ink Station BOC Gases Australia Ltd Burswood Trophies	4,422.88 9.70 257.00 20.24 279.40 296.34 298.78 528.00 1,199.94 591.39 369.29 129.86 469.70	Welding Equipment/Supplies Kitchenware Stationery & Printing Cleaning Products Publications/Newspapers Books/CDs/DVDs Safety Clothing/Equipment Books/CDs/DVDs Groceries Stationery & Printing Welding Equipment/Supplies Badges & Pendants
EF059873 EF059861 EF059895 EF059904 EF059908 EF059909 EF059913 EF059922 EF059930 EF059935 EF059936 EF059940 EF059971 EF059973 EF059984	04/01/19 04/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19	04267 00203 00664 01074 01173 01780 02862 03660 04394 04491 04607 00203	BOC Gases Australia Ltd Kmart Australia Limited Shred-X Pty Ltd Global Spill Control Bodycare Workplace Solutions James Bennett Pty Ltd Safe T Card Australia Pty Ltd JB Hi-Fi Belmont Forum Woolworths Limited (WA) Ink Station BOC Gases Australia Ltd	4,422.88 9.70 257.00 20.24 279.40 296.34 298.78 528.00 1,199.94 591.39 369.29 129.86 469.70 10.12	Welding Equipment/Supplies Kitchenware Stationery & Printing Cleaning Products Publications/Newspapers Books/CDs/DVDs Safety Clothing/Equipment Books/CDs/DVDs Groceries Stationery & Printing Welding Equipment/Supplies
EF059873 EF059861 EF059895 EF059904 EF059908 EF059909 EF059913 EF059922 EF059930 EF059935 EF059936 EF059940 EF059971 EF059973 EF059984 EF059984 EF059984	04/01/19 04/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 18/01/19 18/01/19	04267 00203 00664 01074 01173 01780 02862 03660 04394 04491 04607 00203 00220 01074	BOC Gases Australia Ltd Kmart Australia Limited Shred-X Pty Ltd Global Spill Control Bodycare Workplace Solutions James Bennett Pty Ltd Safe T Card Australia Pty Ltd JB Hi-Fi Belmont Forum Woolworths Limited (WA) Ink Station BOC Gases Australia Ltd Burswood Trophies Shred-X Pty Ltd	4,422.88 9.70 257.00 20.24 279.40 296.34 298.78 528.00 1,199.94 591.39 369.29 129.86 469.70 10.12 58.74	Welding Equipment/Supplies Kitchenware Stationery & Printing Cleaning Products Publications/Newspapers Books/CDs/DVDs Safety Clothing/Equipment Books/CDs/DVDs Groceries Stationery & Printing Welding Equipment/Supplies Badges & Pendants Stationery & Printing
EF059873 EF059861 EF059904 EF059908 EF059909 EF059913 EF059922 EF059930 EF059935 EF059936 EF059940 EF059971 EF059973 EF059984 EF059984 EF060007 EF060008	04/01/19 04/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 18/01/19 18/01/19 18/01/19	04267 00203 00664 01074 01173 01780 02862 03660 04394 04491 04607 00203 00220 01074 03856	BOC Gases Australia Ltd Kmart Australia Limited Shred-X Pty Ltd Global Spill Control Bodycare Workplace Solutions James Bennett Pty Ltd Safe T Card Australia Pty Ltd JB Hi-Fi Belmont Forum Woolworths Limited (WA) Ink Station BOC Gases Australia Ltd Burswood Trophies Shred-X Pty Ltd SEM Distribution	4,422.88 9.70 257.00 20.24 279.40 296.34 298.78 528.00 1,199.94 591.39 369.29 129.86 469.70 10.12 58.74 56.62	Welding Equipment/Supplies Kitchenware Stationery & Printing Cleaning Products Publications/Newspapers Books/CDs/DVDs Safety Clothing/Equipment Books/CDs/DVDs Groceries Stationery & Printing Welding Equipment/Supplies Badges & Pendants Stationery & Printing Publications/Newspapers
EF059873 EF059861 EF059904 EF059909 EF059909 EF059913 EF059922 EF059930 EF059935 EF059936 EF059940 EF059971 EF059973 EF059984 EF060007 EF060008 EF0600011 EF0600013 EF060020	04/01/19 04/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 11/01/19 18/01/19 18/01/19 18/01/19	04267 00203 00664 01074 01173 01780 02862 03660 04394 04491 04607 00203 00203 00220 01074 03856 04053	BOC Gases Australia Ltd Kmart Australia Limited Shred-X Pty Ltd Global Spill Control Bodycare Workplace Solutions James Bennett Pty Ltd Safe T Card Australia Pty Ltd JB Hi-Fi Belmont Forum Woolworths Limited (WA) Ink Station BOC Gases Australia Ltd Burswood Trophies Shred-X Pty Ltd SEM Distribution Totally Workwear	4,422.88 9.70 257.00 20.24 279.40 296.34 298.78 528.00 1,199.94 591.39 369.29 129.86 469.70 10.12 58.74 56.62 725.05 964.96	Welding Equipment/Supplies Kitchenware Stationery & Printing Cleaning Products Publications/Newspapers Books/CDs/DVDs Safety Clothing/Equipment Books/CDs/DVDs Groceries Stationery & Printing Welding Equipment/Supplies Badges & Pendants Stationery & Printing Publications/Newspapers Safety Clothing/Equipment

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Pmnt_Ref		CR_Code	• •	Pmnt_Amnt	Description A639
EF060021 EF060029	18/01/19 18/01/19	05057 05312	Planted Passion Pty Ltd Auspole - International Poles		Gardening - Plants/Supplies Lights & Light Fittings
EF060040	25/01/19	00832	Officeworks		Stationery & Printing
EF060041	25/01/19	03010	City of Belmont Corporate Card Catering		Groceries
EF060042	25/01/19	04348	City of Belmont Corporate Card Events		Events Equipment, Advertising and Subscriptions
EF060043	25/01/19	04422	KnowBe4 Inc		Computer Software
EF060047	25/01/19	05430	City of Belmont Corporate Card Director Corp & Gov	902.50	Promotional Items for Kidz Fest
F060053	25/01/19	00174	Cellarbrations at Belmont		Beverages
F060062	25/01/19	01547	Big W		Craft/Display Materials
F060070	25/01/19	02862	James Bennett Pty Ltd		Books/CDs/DVDs
F060075	25/01/19	03856	SEM Distribution		Publications/Newspapers
F060079	25/01/19	04394	JB Hi-Fi Belmont Forum		Electrical Goods
F060081	25/01/19	04491	Woolworths Limited (WA)		Groceries
F060087	25/01/19	05115	Artistic Den Pty Ltd		Craft/Display Materials
F060093 F060094	25/01/19 25/01/19	05419 05421	Top End Importers Pty Ltd Andrew Graham Engineering		Safety Clothing/Equipment Hardware
F060103	25/01/19	99998	Specialised Security Shredding		Stationery & Printing
F060108	30/01/19	00099	Ausrecord Pty Ltd		Stationery & Printing
F060113	30/01/19	00231	Bunnings Group Ltd		Hardware
F060114	30/01/19	00233	Bunzl Limited	770.72	Cleaning Products
F060117	30/01/19	00311	Cloverdale Hardware & Western Supply	24.47	Hardware
F060119	30/01/19	00314	Coca-Cola Amatil (Aust) Pty Ltd	409.20	Beverages
F060120	30/01/19	00368	Holcim (Australia) Pty Ltd	3,050.88	Road/Drainage Material
F060122	30/01/19	00403	Boral Construction Materials Group Ltd		Road/Drainage Material
F060125	30/01/19	00414	Dulux Australia		Paint & Accessories
F060128	30/01/19	00462	Fast Finishing Services & Classic Bookbinders		Stationery & Printing
F060130	30/01/19	00475	Saferight Pty Ltd		Safety Clothing/Equipment
F060136	30/01/19	00627	Jason Signmakers	313.50	=
F060145	30/01/19	00850	Pacific Safety Wear Malaga		Safety Clothing/Equipment
F060154	30/01/19 30/01/19	01115	Supa IGA Belmont Belvidere Street		Groceries Sand/Soil
F060159 F060163	30/01/19 30/01/19	01239 01325	WA Limestone Co Poolegrave Signs & Engraving	93.46 82.50	Sand/Soil Signs
F060163 F060164	30/01/19	01325	Winc Australia Pty Ltd		Stationery & Printing
F060165	30/01/19	01396	Sprayline Spraying Equipment		Gardening - Plants/Supplies
F060168	30/01/19	01570	Blackwoods		Hardware
F060176	30/01/19	02021	R S E A Pty Ltd		Safety Clothing/Equipment
F060179	30/01/19	02088	Lock Stock & Farrell Locksmith		Hardware
F060185	30/01/19	02757	Quick Corporate Australia Pty Ltd	411.93	Stationery & Printing
F060189	30/01/19	03197	West Coast Turf	22,935.00	Gardening - Plants/Supplies
F060192	30/01/19	03552	Perth Safety Products Pty Ltd	825.00	Safety Clothing/Equipment
F060194	30/01/19	03901	Nutrarich Pty Ltd		Gardening - Plants/Supplies
F060195	30/01/19	04053	Totally Workwear		Safety Clothing/Equipment
F060205	30/01/19	04759	StrataGreen		Gardening - Plants/Supplies
F060207	30/01/19	04996	Signman	330.00	
F060219	31/01/19	00235	Business News		Publications/Newspapers
F060220	31/01/19	00282	Childrens Book Council of Australia (WA) Cloverdale Hardware & Western Supply		Books/CDs/DVDs
F060221 F060236	31/01/19 31/01/19	00311 01529	Safemaster Safety Products		Hardware Safety Clothing/Equipment
F060244	31/01/19	02640	Visual Inspirations Australia Pty Ltd		Craft/Display Materials
F060245	31/01/19	02862	James Bennett Pty Ltd		Books/CDs/DVDs
F060256	31/01/19	04491	Woolworths Limited (WA)		Groceries
F060261	31/01/19	04705	Quality Press Digital		Stationery & Printing
F060265	31/01/19	04860	Mesh Direct Media Pty Ltd	2,959.00	Signs
F060277	31/01/19	05404	LawGear	307.28	Safety Clothing/Equipment
F060278	31/01/19	05432	Bloomin Box Co	225.00	Flowers
	Materials Tota	ı		157,095.66	
ther					
F059807	02/01/19	02421	Illion (formerly Dun & Bradstreet)		Subscription Fee
88001	02/01/19	99999	Keith R Buckingham		Rate Refund
F059812	02/01/19	03960	Befriend Inc		Memorandum of Understanding Contribution
88002	04/01/19	00889	Petty Cash - Finance		Petty Cash Recoup
88003 =050830	04/01/19	00893	Petty Cash - Library		Petty Cash Recoup
F059830 F059831	04/01/19 04/01/19	165769 166076	DNA Kingston Training James W Prentice		Bond Payment/Refund Bond Payment/Refund
-059831 -059832	04/01/19	166392	Swan Transit		Bond Payment/Refund  Bond Payment/Refund
-059632 -059833	04/01/19	166579	Bernadette Tisado		Bond Payment/Refund
F059834	04/01/19	166604	Lainee Fenton		Bond Payment/Refund
-059835	04/01/19	166610	Stella Chinyama Shepande		Bond Payment/Refund
F059836	04/01/19	00242	Cabcharge Australia Pty Ltd		Taxi Fares
F059840	04/01/19	00878	Perth Airports Municipalities Group Inc		Membership Fee
F059859	04/01/19	04079	Belmont Men's Shed Inc	27,500.00	Accommodation Support Contribution
F059869	04/01/19	04887	Shonagh Hayes	121.29	Let's Celebrate Belmont expense reimbursement
F059881	04/01/19	05418	Emily Denham		Mobile phone expense reimbursement
-059882	04/01/19	99998	888 Building Co Pty Ltd	1,238.40	Council Crossover Subsidy
8006	11/01/19	00285	City of Armadale	2,200.00	South East Community Safety Group Projects
					Contribution
38010 =050006	11/01/19	99999	Sushim Roy Stillhight & Noonatal Dooth Support WA Inc.		Rate Refund
F059886 F050887	11/01/19	165562	Stillbirth & Neonatal Death Support WA Inc		Bond Payment/Refund
F059887 F059888	11/01/19 11/01/19	165888 166011	Joy Intervencion Fikreab Tekle		Bond Payment/Refund Bond Payment/Refund
F059888 F059889	11/01/19	166574	Eastern Metrpolitan Health Service		Bond Payment/Refund  Bond Payment/Refund
300003	11/01/19	166575	Yorganop Association Incorporated		Bond Payment/Refund
FN5989N	11/01/19	166581	Marcelline Kabongo		Bond Payment/Refund
	11/01/19	166588	Cody Fullbrook		Bond Payment/Refund
F059891			Cassandra Curtis		Bond Payment/Refund
F059891 F059892		166613			
F059891 F059892 F059893	11/01/19	166613 00242			
F059891 F059892 F059893 F059896	11/01/19 11/01/19	00242	Cabcharge Australia Pty Ltd Department of Transport	102.38	Taxi Fares
F059891 F059892 F059893 F059896 F059927	11/01/19		Cabcharge Australia Pty Ltd	102.38 207.40	
F059890 F059891 F059892 F059893 F059896 F059927 F059947 F059959	11/01/19 11/01/19 11/01/19	00242 03071	Cabcharge Australia Pty Ltd Department of Transport	102.38 207.40 61.65	Taxi Fares Vehicle Ownership Searches

					1010
Pmnt_Ref	Date	CR_Code		Pmnt_Amnt	Description A640
EF059961	11/01/19	99998	Judeen Nominees Pty Ltd		Rate Refund
EF059962 EF059900	11/01/19 11/01/19	99998 00429	LJ Hooker Belmont Economic Development Australia Ltd		Rate Refund Membership Fee
EF059903	11/01/19	00600	Institute of Public Works Engineering WA		Membership Fee - M Ralph
788011	18/01/19	00890	Petty Cash - Harman Park Adult Day Care	253.70	Petty Cash Recoup
788012	18/01/19	00892	Petty Cash - HACC		Petty Cash Recoup
EF059964 EF059965	18/01/19 18/01/19	166152 166282	Ryan Simeon Society for Creative Anachronism Ltd		Bond Payment/Refund Bond Payment/Refund
EF059966	18/01/19	166607	Jasbir Singh		Bond Payment/Refund
EF059967	18/01/19	166616	J Pearce and A Rochford		Bond Payment/Refund
EF059968	18/01/19	166617	Hadassah Morrissey		Bond Payment/Refund
EF059988 EF060037	18/01/19 18/01/19	01236 99998	Department of Fire & Emergency Services Tyler Nicol		Emergency Services Levy Sporting Trip Donation
EF060038	18/01/19	99998	Source Machinery		Application Fee Refund
EF060039	18/01/19	99998	Mitchell Peak		Sporting Trip Donation
EF059969 EF059979	18/01/19 18/01/19	00110 00600	Australian Institute of Management		Membership Fee
788017	25/01/19	00890	Institute of Public Works Engineering WA Petty Cash - Harman Park Adult Day Care		Membership Fee - J Christie Petty Cash Recoup
EF060044	25/01/19	04915	eBay Australia & New Zealand Pty Ltd		Goods & Services Tax
EF060048	25/01/19	166011	Fikreab Tekle		Bond Payment/Refund
EF060049	25/01/19 25/01/19	166620	Yvette Wynne		Bond Payment/Refund
EF060050 EF060051	25/01/19	166621 166624	Diane Lenida Damian Knowles		Bond Payment/Refund Bond Payment/Refund
EF060052	25/01/19	00116	Australasian Performing Right Association		Subscription
EF060060	25/01/19	01315	Zurich Australia Limited		Insurance Claim
EF060071	25/01/19	03071	Department of Transport		Vehicle Ownership Searches
EF060088 EF060098	25/01/19 25/01/19	05208 99998	Australian Outdoor Living L J Leed		Application Fee Refund Application Fee Refund
EF060099	25/01/19	99998	Rupinder Singh		Council Crossover Subsidy
EF060100	25/01/19	99998	Tahlia Hands		Sporting Trip Donation
EF060101	25/01/19	99998	David Raymond & Katherine Ann Hanson		Rate Refund
EF060102 EF060104	25/01/19 25/01/19	99998 99998	Duong Hoa Holdings Pty Ltd Shakeel Green		Rate Refund Sporting Trip Donation
					Executive Leadership Team Lunch Meeting with
EF060045	25/01/19	05121	City of Belmont Corporate Card CEO	162.50	new Director Infrastructure Services
EF060058	25/01/19	00600	Institute of Public Works Engineering WA		Membership Fee - Corporate membership
788023 788025	31/01/19 31/01/19	00890 01730	Petty Cash - Harman Park Adult Day Care Department of Finance Office of State Revenue		Petty Cash Recoup Rate Refund
EF060233	31/01/19	01730	Western Australian Treasury Corporation		Loan Repayment
EF060279	31/01/19	99998	Eastern Hills Outdoors		Application Fee Refund
EF060280	31/01/19	99998	Marco Pedeferri		Application Fee Refund
Property, Plan	Other Total	ent		378,433.54	•
EF059809	02/01/19	02849	Total Nissan & Kia - Total Autos (1990)	44,305.50	Plant Purchase
EF059797	02/01/19	00377	Dell Australia Pty Ltd	2,033.30	Computer Hardware
EF059876	04/01/19	05376	Austin Computers Osborne Park	· · · · · · · · · · · · · · · · · · ·	Computer Hardware
EF059899 EF059916	11/01/19 11/01/19	00377 02254	Dell Australia Pty Ltd PLE Computers	· · · · · · · · · · · · · · · · · · ·	Computer Hardware Computer Hardware
EF059977	18/01/19	00481	Forpark Australia		Playground Equipment - Peet Park & Pitman Park
EF059996	18/01/19	02254	PLE Computers		Computer Hardware
EF060034	18/01/19	05415	Sprocket Pty Ltd	· · · · · · · · · · · · · · · · · · ·	Office Equipment
EF060069 EF060066	25/01/19 25/01/19	02849 02254	Total Nissan & Kia - Total Autos (1990) PLE Computers		Plant Purchase Computer Hardware
EF060174	30/01/19	01831	Mow Master Turf Equipment		Plant Purchase
EF060231	31/01/19	01233	Stihl Shop Redcliffe		Plant Purchase
EF060260	31/01/19	04640	NetPlus Micro Computers		Computer Hardware
Salaries/Wage	Property, Pla	int & Equipir	ient rotai	233,891.69	=
EF059829	03/01/19	99971	Superchoice	127,918.36	Superannuation Contribution
WG020118	03/01/19	COB	City of Belmont Payroll		Salaries/Wages
TM070119 SL090119	07/01/19	COB	City of Belmont Payroll City of Belmont Payroll		Salaries/Wages
SE090119 EF059954	10/01/19 11/01/19	COB 99950	Australian Services Union		Salaries/Wages Salaries/Wages
EF059955	11/01/19	99952	Child Support Agency		Salaries/Wages
EF059956	11/01/19	99954	City of Belmont Social Club		Salaries/Wages
EF059957 EF059958	11/01/19 11/01/19	99960 99962	Health Insurance Fund of WA LGRCEU - WA Shire Councils Union		Salaries/Wages
EF059958 EF059963	15/01/19	99962 99971	Superchoice		Salaries/Wages Superannuation Contribution
WG170119	17/01/19	COB	City of Belmont Payroll		Salaries/Wages
SL230119	24/01/19	COB	City of Belmont Payroll		Salaries/Wages
WG300119	31/01/19	COB	City of Belmont Payroll		Salaries/Wages
WG310119	31/01/19 Salaries/Wag	COB nes Total	City of Belmont Payroll	2,018,677.34	_Salaries/Wages
Training and (	_	=			•
EF059853	04/01/19	02719	Aveling	55.00	Staff Safety Inductions
EF059906	11/01/19	00995	Rohitaswa Das	3,682.92	Gartner APAC Data and Analytics Summit - airfare and registration
EF059910	11/01/19	01240	WA Local Government Association	130.00	Future of Local Government Forum - J Christie and J Olynyk - registration
EF059994	18/01/19	02107	Mercer (Australia) Pty Ltd	3,025.00	CED Job Evaluation Workshop - C Bridges - registration
EF059991	18/01/19	01660	Local Government Planners Assoc	80.00	The Evidence of Strategic Planning seminar - G Healy - registration
EF060004	18/01/19	03453	Clare Bridges	733.00	Australian HR Institute National Convention - airfare; Staff Star Award gift vouchers
EF060018	18/01/19	04977	W A R P Training Australia Pty Ltd		Worksite Traffic Management course
EF060063	25/01/19	01605	ATM Australian Training Management	605.00	Confined Space Entry inc Gas Testing course WA Wetland Management Conference
EF060068	25/01/19	02433	Cockburn Wetlands Education Centre	134.58	WA Wetland Management Conference - Environmental staff

Pmnt Ref	Date	CR Code	Supplier	Pmnt Amnt	Description A641
EF060084	25/01/19	04884	PSN Events - Public Sector Network	104.50	Local Government Transformation Series - J Olynyk - registration
788022	31/01/19	00108	Australian Institute of Building Surveyors	300.00	NCC 2019 Changes and Bushfire Verification Method workshop - D Maher - registration
EF060232	31/01/19	01240	WA Local Government Association	4,345.00	Manage Recovery Activities for Local Government - Emergency Management course for 18 staff
	Training and	Conference	s Total	13,545.00	
MUNI Total				10,012,771.87	
Trust Funds					-
EF059883	08/01/19	150748	Building & Construction Industry Training Levy	77,235.55	Building & Construction Industry Training Levy
EF059884	08/01/19	154102	Building Commission Building Services Levy	55,085.02	Building Commission Building Services Levy
EF059885	08/01/19	164040	Department of Planning DAP fees	5,603.00	Department of Planning DAP fees
	Trust Funds	Total		137,923.57	
TRUST Total				137,923.57	
Grand Total				10,150,695.44	

Breakdown - Cheques : EFT : 152,022.78 9,998,672.66

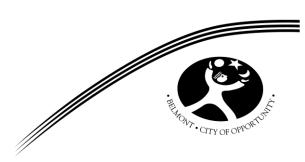
458,873.00 Total of all outstanding Creditor accounts as at 31 January 2019



Item 12.14 refers

**Attachment 30** 

Monthly Activity Statement as at 31 December 2018



## **City of Belmont**

#### Monthly Financial Activity Statement for the Period Ending December 2018

Note: Material variances have been identified in accordance with the Local Government (Financial Management) Regulations 34(1)(d) and Australian Accounting Standards (AASB 1031). A variance on the budgeted closing balance has been applied in the determination of material variances.

M=Material Variance

Budget: 19CLRBD1, Actual: 19CLACT Created:04-Jan-2019 14:47:41

	Budget	Budget YTD	Actual YTD	YTD Variance	YTD Var %
Expenditure					
pital					
Governance					
	90.750	90.750	72 642	16 107	17.95%
Finance Department	89,750	89,750	73,643	16,107	55.89% <b>N</b>
Computing	443,675	199,400	87,963		
Marketing & Communications	257,875	106,500	48,978	•	54.01% <b>N</b>
Transfer To Reserve	3,693,083	0	0	-	0.00%
Executive Services	89,750	44,875	40,463	•	9.83%
Chief Executive Officer	57,003	0	0	-	0.00%
Human Resources	134,625	44,875	0	44,875	100.00%
Governance Belmont Trust	69,003 43,697	6,000 0	38,848 0	-32,848 0	-547.46% 0.00%
			202.004		
Total Governance	4,878,461	491,400	289,894	201,506	41.01%
General purpose funding	44.075	0	0	0	0.000/
Property & Economic Development	44,875		-	0	0.00%
Financing Activities	675,561	334,028	334,100	-72	-0.02%
Total General purpose funding	720,436	334,028	334,100	-72	-0.02%
Law, order and public safety					
Belmont Community Watch	116,433	116,433	93,013	23,420	20.11%
Rangers	38,811	0	2,585	-2,585	0.00%
Crime Prevention & Comm Safety	98,500	49,250	9,420	39,830	80.87%
Total Law, order and public safety	253,744	165,683	105,018	60,665	36.61%
Health					
Health	77,622	38,811	33,172	5,639	14.53%
Total Health	77,622	38,811	33,172	5,639	14.53%
Education and welfare					
Community Services	161,308	83,686	52,582	31,104	37.17%
Belmont HACC Services	38,811	0	0_,00_	·	0.00%
Youth Services General	7,500	7,500	6,500		13.33%
Total Education and welfare	207,619	91,186	59,082	32,104	35.21%
Housing					
Orana Aged Housing	46,472	0	0	0	0.00%
Gabriel Gardens	36,112	13,056	0	13,056	100.00%
Faulkner Park Retirement Vill.	120,000	0	0	0	0.00%
Total Housing	202,584	13,056	0	13,056	100.00%
Community amenities					
Town Planning	395,814	95,814	42,801	53,013	55.33% <b>I</b>
Technical Services	94,000	4,500	46,526	-42,026	-933.90%
Total Community amenities	489,814	100,314	89,326	10,988	10.95%
Recreation and culture				,,,,,,	
Belmont Oasis	75,000	30,000	4,700	25,300	84.33%
Ruth Faulkner Library	86,600	23,895	32,694		-36.83%
Community Wellbeing	30,000	23,695	9,810	-9,810	0.00%
Grounds Operations	2,610,628	1,323,628	710,510	•	46.32% <b>I</b>
Total Recreation and culture	2,802,228	1,377,523	757,714	619,809	44.99%
Transport	2,002,220	1,377,323	131,114	013,009	44.3370

	Budget	Budget YTD	Actual YTD	YTD Variance	YTD Var %
Road Works	10,690,276	4,611,978	2,778,459	1,833,520	39.76% N
Streetscapes	591,993	28,496	33,131	-4,635	-16.27%
Footpath Works	732,580	290,594	12,309	278,285	95.76% N
Drainage Works	847,272	436,169	313,187	122,982	28.20% N
Operations Centre	1,307,288	782,658	306,414	476,244	60.85% <b>N</b>
Total Transport	14,169,409	6,149,895	3,443,499	2,706,396	44.01%
<b>Economic services</b>					
Ascot Close Housing	45,000	0	37,800	-37,800	0.00%
Wahroonga Housing	45,000	0	3,500	-3,500	0.00%
Building Control	122,497	44,875	0	44,875	100.00%
Building Operations	32,091,361	9,402,892	7,390,944	2,011,948	21.40% <b>N</b>
Building Overheads	0	0	40,695	-40,695	0.00%
Total Economic services	32,303,858	9,447,767	7,472,939	1,974,828	20.90%
Other property and services					
Technical Services	224,375	134,625	72,429	62,197	46.20% N
Total Other property and services	224,375	134,625	72,429	62,197	46.20%
tal Capital	56,330,150	18,344,288	12,657,173	5,687,115	31.00%

	Budget	Budget YTD	Actual YTD	YTD Variance	YTD Var %
perating					
Governance					
Finance Department	2,025,811	1,025,961	1,020,453	5,507	0.54%
Computing	2,457,221	1,311,790	1,242,424	69,367	5.29%
Marketing & Communications	1,210,885	629,291	586,680	42,611	6.77%
Reimbursements	267,336	133,668	134,994	-1,326	-0.99%
Insurance	754,813	691,962	683,798	8,165	1.18%
Executive Services	1,820,441	917,598	882,363	35,235	3.84%
Chief Executive Officer	827,360	413,501	381,949	31,552	7.63%
Records Management	797,536	398,198	380,066	18,132	4.55%
Human Resources	1,381,336	692,181	727,537	-35,356	-5.11%
Governance	3,806,988	1,932,870	1,757,601	175,268	9.07%
Belmont Trust	170,000	85,000	6,461	78,539	92.40%
Accommodation Costs	599,583	312,444	265,470	46,974	15.03%
Total Governance	16,119,310	8,544,465	8,069,796	474,668	5.56%
General purpose funding	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0.400.01=	0.445.05=	400	6 460:
Rates	2,619,939	2,139,045	2,149,385	-10,340	-0.48%
General Purpose Income	1,040	520	5,003	-4,483	-862.15%
Property & Economic Development	1,023,614	552,751	458,306	94,445	17.09%
Financing Activities	705,976	361,600	318,179	43,421	12.01%
Total General purpose funding	4,350,570	3,053,916	2,930,873	123,043	4.03%
Law, order and public safety					
Belmont Community Watch	1,304,380	550,546	581,673	-31,127	-5.65%
BelmontNeighbourhood Watch	2,758	1,479	491	988	66.83%
Criminal Damage	149,481	74,683	81,339	-6,655	-8.91%
Rangers	962,872	482,283	397,994	84,289	17.48%
Crime Prevention & Comm Safety	712,723	363,394	279,869	83,525	22.98%
Volunteer Emergency Services	109,428	61,369	47,091	14,278	23.27%
Total Law, order and public safety	3,241,642	1,533,755	1,388,456	145,299	9.47%
Health					
Health	1,431,977	740,384	646,823	93,561	12.64%
Immunisation	14,936	7,468	6,305	1,163	15.57%
Total Health	1,446,913	747,852	653,129	94,723	12.67%
Education and welfare					
Aboriginal Strategies	313,414	156,468	97,579	58,889	37.64%
Senior Citizens Centre	9,359	5,084	8,254	-3,170	-62.35%
Meals On Wheels	0	0	0		0.00%
Podiatry	0	0	490	-490	0.00%
Community Services	869,049	456,438	405,314	51,124	11.20%
Community Lifestyle & Learning	651,517	344,413	268,520	75,893	22.04%
Volunteers Programs	93,450	46,706	39,195		16.08%
Belmont HACC Services	2,450,216	1,235,282	1,192,258	43,024	3.48%
Youth Services General Pre-Schools & Kindys	713,566 6,578	299,783 4,907	296,243 5,843	3,541 -937	1.18% -19.09%
Total Education and welfare	5,107,149	2,549,080	2,313,696	235,384	9.23%
Housing	-,,	,,	,: ·- <b>,</b> -30	,	
Ascot Close Housing	61,996	37,800	25,658	12,142	32.12%
Wahroonga Housing	109,211	91,312	97,790		-7.09%
Orana Aged Housing	58,528	32,240	32,564	-324	-1.00%
Gabriel Gardens	57,888	32,022	33,091	-1,069	-3.34%
Faulkner Park Retirement Vill.	80,000	40,000	44,575	-4,575	-11.44%
Total Housing	367,623	233,374	233,678	-304	-0.13%
Community amenities					
Regional Development	23,986	23,916	23,826		0.38%
Town Planning	2,922,330	1,468,380	1,332,780	135,601	9.23%
	0.470.000	2 600 250	2 745 704	25 446	4 220/
Sanitation Charges Technical Services	6,470,333 192,927	2,680,258 104,729	2,715,704 88,279	-35,446 16,450	-1.32% 15.71%

	Budget	Budget YTD	Actual YTD	YTD Variance	YTD Var %
Total Community amenities	9,609,575	4,277,284	4,160,588	116,695	2.73%
Recreation and culture					
Marketing & Communications	931,700	517,726	517,369	357	0.07%
Donations and Grants	389,859	227,699	117,884	109,816	48.23% <b>I</b>
Belmont Trust	4,960	2,492	3,668	-1,175	-47.16%
Public Facilities Operations	91,605	73,553	83,154	-9,601	-13.05%
Belmont Oasis	623,685	330,903	341,483	-10,580	-3.20%
Youth & Family Services Centre	145,585	74,592	55,679	18,913	25.36%
Ruth Faulkner Library	2,378,405	1,223,441	1,145,699	77,742	6.35% I
Community Services	55,089	46,794	44,262	2,532	5.41%
Community Wellbeing	812,981	353,971	351,880	2,091	0.59%
Building - Active Reserves	897,033	477,069	453,417	23,653	4.96%
Building Operations	840	0	0	0	0.00%
Streetscapes	22,000	11,000	4,454	6,546	59.51%
Grounds Operations	5.543.734	2,819,074	2,615,745	203,330	7.21% <b>I</b>
Grounds - Active Reserves	1,133,705	484,125	565,190	-81,065	-16.74% <b>I</b>
Grounds Overheads	1,484,057	814,766	690,989	123,776	15.19% I
Total Recreation and culture	14,515,239	7,457,206	6,990,872	466,334	6.25%
Transport					
Road Works	1,005,814	521,020	483,608	37,412	7.18%
Streetscapes	1,787,463	598,033	604,405	-6,373	-1.07%
Footpath Works	209,300	104,650	87,514	17,137	16.38%
Drainage Works	377,830	188,915	105,271	83,644	44.28%
Operations Centre	757,711	406,374	396,369	10,006	2.46%
Grounds Operations	66,358	33,110	28,201	4,909	14.83%
Total Transport	4,204,477	1,852,103	1,705,368	146,735	7.92%
<b>Economic services</b>					
Building Control	1,291,818	643,485	641,595	1,890	0.29%
Building Control Customer Service	457,023	228,162	243,350	-15,188	-6.66%
Building Operations	987,746	531,143	526,544	4,599	0.87%
Building Overheads	94,300	47,485	40,397	7,088	14.93%
Streetscapes	9,749	4,898	3,872	1,026	20.94%
Total Economic services	2,840,636	1,455,173	1,455,758	-585	-0.04%
Other property and services					
Building Operations	3,930	1,510	1,480	30	1.96%
Public Works Overheads	1,544,053	836,444	872,095	-35,651	-4.26%
Plant Operating Costs	1,084,267	570,642	536,628	34,013	5.96%
Technical Services	2,704,355	1,308,602	1,226,800	81,802	6.25% I
Other Public Works	890,241	387,631	395,994	-8,363	-2.16%
Total Other property and services	6,226,846	3,104,829	3,032,998	71,831	2.31%
al Operating	68,029,981	34,809,035	32,935,212	1,873,823	5.38%
al 1. Expenditure	124,360,131	53,153,323	45,592,385	7,560,938	14.22%

	Budget	Budget YTD	Actual YTD	YTD Variance	YTD Var %
Revenue					
pital					
Governance					
	70.000	00.004	C4 400	4.050	0.000/
Finance Department	-76,080	-62,834	-61,182	•	2.63% 0.00%
Computing	-375,367	0	-25,409	25,409	100.00%
Marketing & Communications Executive Services	-75,872	-8,710	0	-, -	0.75%
Chief Executive Officer	-62,834	-31,417 0	-31,182 0		0.75%
Human Resources	-39,908		0	-	100.00%
Governance	-94,251	-31,417	-	-31,417	0.00%
Belmont Trust	-39,908 -174,960	0	-16,909 0	16,909 0	0.00%
Total Governance	-939,180	-134,378	-134,682		-0.23%
Total Governance	303,100	104,010	104,002	004	0.2070
General purpose funding	4 404 447	0	0	2	0.000/
Property & Economic Development	-1,131,417	0	0		0.00%
Financing Activities	-6,545	-2,182	0	-2,182	100.00%
Total General purpose funding	-1,137,962	-2,182	0	-2,182	100.00%
Law, order and public safety					
Belmont Community Watch	-81,516	-81,516	-62,864	-18,652	22.88%
Rangers	-27,172	0	-18,182	18,182	0.00%
Total Law, order and public safety	-108,688	-81,516	-81,045	-471	0.58%
Health					
Health	-100,553	-27,172	-27,364	192	-0.71%
Total Health	-100,553	-27,172	-27,364	192	-0.71%
Education and welfare					
Community Services	-112,933	-58,589	-31,273	-27,316	46.62%
Community Lifestyle & Learning	-5,899	0	0	0	0.00%
Belmont HACC Services	-38,661	0	0	0	0.00%
Total Education and welfare	-157,493	-58,589	-31,273	-27,316	46.62%
Housing					
Ascot Close Housing	-61,996	0	0	0	0.00%
Wahroonga Housing	-109,211	0	0		0.00%
Total Housing	-171,207	0	0	0	0.00%
Community amenities		_	_		_
Town Planning	-380,699	-67,079	-62,000	-5,079	7.57%
Technical Services	-18,842	0	-18,842	18,842	0.00%
Total Community amenities	-399,541	-67,079	-80,842	13,763	-20.52%
Recreation and culture					
Ruth Faulkner Library	-71,348	0	0	0	0.00%
Community Wellbeing	-7,077	0	0	0	0.00%
Grounds Operations	-663,345	-90,835	-66,615	-24,220	26.66%
Grounds Overheads	-35,969	0	0	0	0.00%
Total Recreation and culture	-777,739	-90,835	-66,615	-24,220	26.66%
Transport					
Transport Road Works	-5,654,680	-3,628,623	-1,574,626	-2,053,997	56.61%

	Budget	Budget YTD	Actual YTD	YTD Variance	YTD Var %
Operations Centre	-1,249,520	-313,216	-109,909	-203,307	64.91% <b>I</b>
Total Transport	-6,954,200	-3,941,839	-1,684,535	-2,257,304	57.27%
Economic services					
Ascot Close Housing	-45,000	0	0	0	0.00%
Wahroonga Housing	-45,000	0	0	0	0.00%
Building Control	-85,761	-31,417	-59,000	27,583	-87.80%
Building Operations	-15,731,980	-1,643,140	-1,481,692	-161,448	9.83% <b>I</b>
Total Economic services	-15,907,741	-1,674,557	-1,540,692	-133,865	7.99%
Other property and services					
Public Works Overheads	-23,756	0	-16,818	16,818	0.00%
Technical Services	-232,593	-67,079	-72,815	5,736	-8.55%
Total Other property and services	-256,349	-67,079	-89,633	22,554	-33.62%
tal Capital	-26,910,653	-6,145,226	-3,736,680	-2,408,545	39.19%

	Budget	Budget YTD	Actual YTD	YTD Variance	YTD Var %
erating					
Governance					
Finance Department	-2,036,940	-1,018,470	-1,020,453	1,983	-0.19%
Computing	-2,422,601	-1,211,301	-1,242,424	•	-2.57%
Marketing & Communications	-155,000	-75,000	-33,000	·	56.00%
Reimbursements	-267,336	-133,668	-87,338	•	34.66%
Insurance	-776,478	-769,175	-726,133	-43,042	5.60%
Executive Services	0	0	-10	10	0.00%
Chief Executive Officer	0	0	-1,265	1,265	0.00%
Records Management	-797,536	-398,768	-380,066	-18,702	4.69%
Human Resources	-1,365,836	-682,918	-727,537	44,619	-6.53%
Governance	0	0	-2,293	·	0.00%
Belmont Trust	-43,697	0	0	-	0.00%
Accommodation Costs	-599,583	-299,792	-265,470	-34,322	11.45%
Total Governance	-8,465,007	-4,589,091	-4,485,990	-103,101	2.25%
General purpose funding					
Rates	-50,246,446	-49,768,116	-48,748,637	, ,	2.05% N
General Purpose Income	-435,639	-215,794	-215,720		0.03%
Property & Economic Development	-405,520	-245,294	-222,947		9.11%
Financing Activities	-2,011,214	-1,005,607	-686,614	-318,993	31.72%
Total General purpose funding	-53,098,819	-51,234,811	-49,873,918	-1,360,893	2.66%
Law, order and public safety					
Belmont Community Watch	0	0	-4,500	4,500	0.00%
Rangers	-251,575	-125,787	-104,630	-21,157	16.82%
Crime Prevention & Comm Safety	-10,000	-5,000	-3,317	-1,683	33.67%
Volunteer Emergency Services	-75,532	-40,000	-21,435	-18,565	46.41%
Total Law, order and public safety	-337,107	-170,787	-133,882	-36,905	21.61%
Health					
Health	-449,869	-273,434	-245,128	-28,307	10.35%
Immunisation	-2,000	-1,000	-708	-292	29.20%
Total Health	-451,869	-274,434	-245,836	-28,599	10.42%
Education and welfare					
Aboriginal Strategies	0	0	-214	214	0.00%
Senior Citizens Centre	-5,496	-2,748	-2,537	-211	7.67%
Community Services	0	0	-1,209	1,209	0.00%
Belmont HACC Services	-2,450,366	-1,230,951	-1,264,021	33,069	-2.69%
Youth Services General	-70,500	-35,250	-33,075	-2,175	6.17%
Total Education and welfare	-2,526,362	-1,268,949	-1,301,056	32,107	-2.53%
Housing					
Orana Aged Housing	-105,000	0	0	0	0.00%
Gabriel Gardens	-94,000	0	0		0.00%
Faulkner Park Retirement Vill.	-200,000	-150,000	-94,211	-55,789	37.19% N
Total Housing	-399,000	-150,000	-94,211	-55,789	37.19%
Community amenities					
Town Planning	-1,133,249	-566,625	-715,405	148,780	-26.26% <b>N</b>
Sanitation Charges	-6,460,661	-6,413,199	-6,337,042	-76,157	1.19% N
	-4,050	-3,300	-3,300	0	0.00%
Technical Services	1,000	-,			

	Budget	Budget YTD	Actual YTD	YTD Variance	YTD Var %
Beauties and address					
Recreation and culture					/
Marketing & Communications	-33,333	-14,286	-23,718	9,432	-66.02%
Donations and Grants	0	0	-496	496	0.00%
Public Facilities Operations	-303,312	-151,570	-147,969	-3,601	2.38%
Belmont Oasis	-1,000	0	0	0	0.00%
Youth & Family Services Centre	-13,360	-6,680	-2,299	-4,381	65.58%
Ruth Faulkner Library	-80,589	-56,689	-48,387	-8,302	14.64%
Community Wellbeing	-6,300	-4,313	-5,400	1,087	-25.21%
Grounds Operations	-13,552	-1,425	-1,762	337	-23.67%
Grounds Overheads	-1,448,088	-720,045	-732,760	12,715	-1.77%
Total Recreation and culture	-1,899,534	-955,007	-962,791	7,784	-0.82%
Transport					
Road Works	-266,571	-139,716	-140,786	1,070	-0.77%
Streetscapes	-104,000	-52,000	0	-52,000	100.00% <b>N</b>
<u> </u>			440.700		20 570/
Total Transport	-370,571	-191,716	-140,786	-50,930	26.57%
Economic services					
Building Control	-394,352	-206,622	-205,317	-1,305	0.63%
Building Control Customer Service	-457,023	-228,511	-243,350	14,838	-6.49%
Building Operations	0	0	-17,595	17,595	0.00%
Building Overheads	-114,041	-59,110	-58,947	-163	0.28%
Total Economic services	-965,416	-494,243	-525,208	30,965	-6.27%
Other property and services					
Public Works Overheads	-1,520,297	-668,485	-528,644	-139,841	20.92% <b>N</b>
Plant Operating Costs	-1,710,437	-807,688	-708,344	-99,345	12.30% <b>N</b>
Technical Services	-486,714	-243,357	-199,668	-43,690	17.95%
Other Public Works	-69,000	-24,500	-79,833	55,332	-225.85% <b>N</b>
Total Other property and services	-3,786,448	-1,744,031	-1,516,488	-227,543	13.05%
otal Operating	-79,898,093	-68,056,193	-66,335,912	-1,720,281	2.53%
otal 2. Revenue	-106,808,746	-74,201,419	-70,072,593	-4,128,826	5.56%
Opening/Closing Funds					
perating					
P&L Clearing					
Opening Balance - Budget Only	-18051385	0	0	0	0.00%
Closing Balance - Budget Only	500,000	0	0	0	0.00%
Total P&L Clearing	-17,551,385	0	0	0	0.00%
otal 3. Opening/Closing Funds	-17,551,385	0	0	0	0.00%
				3,432,112	19.79% <b>N</b>
	0	-21,048,096	-24,480,208	3,432,112	19./9% <b>N</b>

Nett Current Assets:

-42,531,593



Item 12.15 refers

**Attachment 31** 

Monthly Activity Statement as at 31 January 2019



## **City of Belmont**

#### Monthly Financial Activity Statement for the Period Ending January 2019

Note: Material variances have been identified in accordance with the Local Government (Financial Management) Regulations 34(1)(d) and Australian Accounting Standards (AASB 1031). A variance on the budgeted closing balance has been applied in the determination of material variances.

M=Material Variance

Budget: 19CLRBD1, Actual: 19CLACT Created:01-Feb-2019 13:03:25

	Budget	Budget YTD	Actual YTD	YTD Variance	YTD Var %
Expenditure					
pital					
Governance					
Finance Department	89,750	89,750	73,643	16,107	17.95%
Computing	443,675	200,150	87,963	·	56.05% <b>M</b>
Marketing & Communications	257,875	106,500	64,978		38.99%
Transfer To Reserve	3,693,083	0	0 .,0.0		0.00%
Executive Services	89.750	44,875	40,463	4,412	9.83%
Chief Executive Officer	57,003	0	0	0	0.00%
Human Resources	134,625	44,875	0	44,875	100.00%
Governance	69,003	64,003	38,848	25,155	39.30%
Belmont Trust	43,697	0	0	0	0.00%
Total Governance	4,878,461	550,153	305,894	244,259	44.40%
General purpose funding					
Property & Economic Development	44,875	0	0	0	0.00%
Financing Activities	675,561	334,028	334,100	-72	-0.02%
Total General purpose funding	720,436	334,028	334,100	-72	-0.02%
Law, order and public safety					
Belmont Community Watch	116,433	116,433	93,013	23,420	20.11%
Rangers	38,811	0	2,585	-2,585	0.00%
Crime Prevention & Comm Safety	98,500	57,458	9,420	48,038	83.61%
Total Law, order and public safety	253,744	173,891	105,018	68,873	39.61%
Health					
Health	77,622	38,811	33,172	5,639	14.53%
Total Health	77,622	38,811	33,172	5,639	14.53%
Education and welfare					
Community Services	161,308	83,686	52,582	31,104	37.17%
Belmont HACC Services	38,811	00,000	02,002	01,104	0.00%
Youth Services General	7,500	7,500	6,500	1,000	13.33%
Total Education and welfare	207,619	91,186	59,082	32,104	35.21%
Housing					
Orana Aged Housing	46,472	0	0	0	0.00%
Gabriel Gardens	36,112	15,232	0	15,232	100.00%
Faulkner Park Retirement Vill.	120,000	0	0	0	0.00%
Total Housing	202,584	15,232	0	15,232	100.00%
Community amenities					
Town Planning	395,814	95,814	42,801	53,013	55.33% <b>N</b>
Technical Services	94,000	9,000	46,886	-37,886	-420.95%
Total Community amenities	489,814	104,814	89,686	15,128	14.43%
Recreation and culture					
Belmont Oasis	75,000	30,000	4,700	25,300	84.33%
Ruth Faulkner Library	86,600	23,895	32,694	·	-36.83%
Community Wellbeing	30,000	0	9,810		0.00%
Grounds Operations	2,610,628	1,377,628	910,838	466,791	33.88% N
Total Recreation and culture	2,802,228	1,431,523	958,042	473,481	33.08%
Transport					

	Budget	Budget YTD	Actual YTD	YTD Variance	YTD Var %
Road Works	10,690,276	5,048,265	6,743,361	-1,695,096	-33.58% <b>N</b>
Streetscapes	591,993	148,496	33,131	115,365	77.69% <b>N</b>
Footpath Works	732,580	388,239	38,764	349,475	90.02% <b>N</b>
Drainage Works	847,272	509,054	399,132	109,922	21.59% N
Operations Centre	1,307,288	785,992	308,438	477,554	60.76% N
Total Transport	14,169,409	6,880,047	7,522,826	-642,779	-9.34%
<b>Economic services</b>					
Ascot Close Housing	45,000	0	37,800	-37,800	0.00%
Wahroonga Housing	45,000	0	3,500	-3,500	0.00%
Building Control	122,497	83,686	0	83,686	100.00% N
Building Operations	32,091,361	11,174,135	8,646,952	2,527,184	22.62% N
Building Overheads	0	0	40,695	-40,695	0.00%
Total Economic services	32,303,858	11,257,821	8,728,947	2,528,875	22.46%
Other property and services					
Technical Services	224,375	179,500	109,464	70,036	39.02% <b>N</b>
Total Other property and services	224,375	179,500	109,464	70,036	39.02%
tal Capital	56,330,150	21,057,007	18,246,231	2,810,775	13.35%

	Budget	Budget YTD	Actual YTD	YTD Variance	YTD Var %
perating					
Governance					
Finance Department	2,025,811	1,186,522	1,189,836	-3,315	-0.28%
Computing	2,457,221	1,487,671	1,360,676	126,995	8.54% <b>I</b>
Marketing & Communications	1,210,885	719,746	651,213	68,533	9.52% <b>I</b>
Reimbursements	267,336	146,334	171,304	-24,969	-17.06%
Insurance	754,813	695,375	684,298	11,077	1.59%
Executive Services	1,820,441	1,046,280	1,021,465	24,815	2.37%
Chief Executive Officer	827,360	476,492	462,347	14,146	2.97%
Records Management	797,536	457,472	435,276	22,196	4.85%
Human Resources	1,381,336	796,423	840,951	-44,529	-5.59%
Governance	3,806,988	2,208,264	2,021,791	186,472	8.44% <b>I</b>
Belmont Trust	170,000	85,000	6,461	78,539	92.40% <b>I</b>
Accommodation Costs	599,583	335,824	291,713	44,111	13.14%
Total Governance	16,119,310	9,641,403	9,137,332	504,071	5.23%
General purpose funding	0.040.000	0.040.400	0.045.050	040	0.0007
Rates	2,619,939	2,216,463	2,215,850	612	0.03%
General Purpose Income	1,040	607	5,003	-4,396	-724.75%
Property & Economic Development Financing Activities	1,023,614 705,976	618,966 361,600	524,294 372,155	94,672 -10,555	15.30% <b>I</b> -2.92%
Total General purpose funding	4,350,570	3,197,636	3,117,302	80,334	2.51%
Law, order and public safety					
Belmont Community Watch	1,304,380	658,682	691,076	-32,394	-4.92%
BelmontNeighbourhood Watch	2,758	1,692	536	1,156	68.32%
Criminal Damage	149,481	86,611	91,016	-4,404	-5.09%
Rangers	962,872	556,727	461,063	95,665	17.18% <b>I</b>
Crime Prevention & Comm Safety	712,723	418,849	316,158	102,691	24.52% I
Volunteer Emergency Services	109,428	66,634	51,078	15,557	23.35%
Total Law, order and public safety	3,241,642	1,789,197	1,610,927	178,270	9.96%
Health					
Health	1,431,977	847,057	751,223	95,834	11.31% <b>I</b>
Immunisation	14,936	8,713	7,406	1,307	15.00%
Total Health	1,446,913	855,770	758,629	97,141	11.35%
Education and welfare					
Aboriginal Strategies	313,414	175,331	118,694	56,637	32.30% I
Senior Citizens Centre	9,359	5,431	9,182	-3,750	-69.05%
Meals On Wheels	0	0	0	0	0.00%
Podiatry	0	0	490	-490	0.00%
Community Services	869,049	514,609	474,480	40,129	7.80%
Community Lifestyle & Learning	651,517	392,591	306,967	85,624	21.81% <b>I</b>
Volunteers Programs	93,450	52,033	42,426	9,607	18.46%
Belmont HACC Services	2,450,216	1,425,038	1,383,574	41,465	2.91%
Youth Services General Pre-Schools & Kindys	713,566 6,578	357,084 4,911	353,944 5,992	3,140 -1,081	0.88% -22.01%
Total Education and welfare	5,107,149	2,927,028	2,695,748	231,280	7.90%
Housing	0,101,149	2,021,020	2,000,140	201,200	1.50/0
Ascot Close Housing	61,996	41,050	29,053	11,997	29.22%
Wahroonga Housing	109,211	93,791	100,718	-6,927	-7.39%
Orana Aged Housing	58,528	35,682	35,814	-132	-0.37%
Gabriel Gardens	57,888	35,247	38,499	-3,252	-9.23%
Faulkner Park Retirement Vill.	80,000	40,000	44,575	-4,575	-11.44%
Total Housing	367,623	245,770	248,660	-2,890	-1.18%
Community amenities					
Regional Development	23,986	23,927	23,837	90	0.38%
Town Planning	2,922,330	1,692,560	1,530,451	162,109	9.58% <b>I</b>
Sanitation Charges	6,470,333	3,074,529	3,038,835	35,694	1.16%
Technical Services	192,927	113,630	96,704	16,926	14.90%

	Budget	Budget YTD	Actual YTD	YTD Variance	YTD Var %
Total Community amenities	9,609,575	4,904,645	4,689,826	214,819	4.38%
Recreation and culture					
Marketing & Communications	931,700	566,128	565,574	554	0.10%
Donations and Grants	389,859	239,476	145,384	94,092	39.29% <b>!</b>
Belmont Trust	4,960	2,908	3,668	-760	-26.14%
Public Facilities Operations	91,605	75,087	84,737	-9,650	-12.85%
Belmont Oasis	623,685	366,329	387,442	-21,113	-5.76%
Youth & Family Services Centre	145,585	83,582	72,470	11,113	13.30%
Ruth Faulkner Library	2,378,405	1,396,364	1,300,956	95,408	6.83% I
Community Services	55,089	47,469	44,276	3,193	6.73%
Community Wellbeing	812,981	419,082	397,967	21,115	5.04%
Building - Active Reserves	897,033	553,327	493,108	60,218	10.88% <b>I</b>
Building Operations	840	0	0	00,210	0.00%
Streetscapes	22,000	11,000	6,909	4,091	37.20%
Grounds Operations	5.543.734	3,254,899	3,108,785	146,114	4.49% I
Grounds - Active Reserves	1,133,705	609,455	762,412	-152,956	-25.10% <b>I</b>
Grounds Overheads	1,484,057	924,216	840,403	83,813	9.07% I
Total Recreation and culture	14,515,239	8,549,323	8,214,089	335,233	3.92%
Transport					
Road Works	1,005,814	593,888	626,089	-32,201	-5.42%
Streetscapes	1,787,463	693,362	718,379	-25,017	-3.61%
Footpath Works	209,300	122,092	92,975	29,117	23.85%
Drainage Works	377,830	220,401	136,235	84,165	38.19% <b>I</b>
Operations Centre	757,711	455,319	466,816	-11,497	-2.52%
Grounds Operations	66,358	39,544	33,614	5,930	15.00%
Total Transport	4,204,477	2,124,606	2,074,108	50,498	2.38%
Economic services					
Building Control	1,291,818	740,696	736,128	4,568	0.62%
Building Control Customer Service	457,023	263,458	282,904	-19,445	-7.38%
Building Operations	987,746	596,708	598,372	-1,664	-0.28%
Building Overheads	94,300	54,904	53,673	1,231	2.24%
Streetscapes	9,749	5,713	3,940	1,773	31.04%
Total Economic services	2,840,636	1,661,479	1,675,016	-13,537	-0.81%
Other property and services					
Building Operations	3,930	1,510	1,480	30	1.96%
Public Works Overheads	1,544,053	951,071	1,029,175	-78,104	-8.21% <b>I</b>
Plant Operating Costs	1,084,267	654,256	631,273	22,983	3.51%
Technical Services	2,704,355	1,504,445	1,411,053	93,393	6.21% <b>I</b>
Other Public Works	890,241	458,181	476,305	-18,124	-3.96%
Total Other property and services	6,226,846	3,569,463	3,549,286	20,177	0.57%
tal Operating	68,029,981	39,466,320	37,770,923	1,695,397	4.30%
tal 1. Expenditure	124,360,131	60,523,326	56,017,154	4,506,173	7.45%

	Budget	Budget YTD	Actual YTD	YTD Variance	YTD Var %
Revenue					
pital					
Governance					
Finance Department	-76,080	-62,834	-61,182		2.63%
Computing	-375,367	0	-25,409		0.00%
Marketing & Communications	-75,872	-8,710	0	•	100.00%
Executive Services Chief Executive Officer	-62,834 -39,908	-31,417 0	-31,182 0		0.75% 0.00%
Human Resources	-94,251	-31,417	0		100.00%
Governance	-39,908	-31,417	-16,909		0.00%
Belmont Trust	-174,960	0	0		0.00%
Total Governance	-939,180	-134,378	-134,682	304	-0.23%
General purpose funding					
Property & Economic Development	-1,131,417	0	0	0	0.00%
Financing Activities	-6,545	-2,909	0		100.00%
<del>-</del>					
Total General purpose funding	-1,137,962	-2,909	0	-2,909	100.00%
Law, order and public safety					
Belmont Community Watch	-81,516	-81,516	-62,864	-18,652	22.88%
Rangers	-27,172	0	-18,182		0.00%
Total Law, order and public safety	-108,688	-81,516	-81,045	-471	0.58%
Health					
Health	-100,553	-27,172	-27,364	192	-0.71%
Total Health	-100,553	-27,172	-27,364	192	-0.71%
Education and welfare					
Community Services	-112,933	-58,589	-31,273	-27,316	46.62%
Community Lifestyle & Learning	-5,899	0	0	•	0.00%
Belmont HACC Services	-38,661	0	0		0.00%
Total Education and welfare	-157,493	-58,589	-31,273	-27,316	46.62%
Housing					
Ascot Close Housing	-61,996	0	0	0	0.00%
Wahroonga Housing	-109,211	0	0		0.00%
Total Housing	-171,207	0	0	0	0.00%
Total Housing	-171,207	v	U	U	0.00 %
Community amenities	202.022	67.070	204 775	064.000	204.0007
Town Planning Technical Services	-380,699 -18,842	-67,079 0	-331,775 -18,842		-394.60% <b>I</b> 0.00%
Total Community amenities	-399,541	-67,079	-350,617	283,538	-422.69%
Recreation and culture					
Ruth Faulkner Library	-71,348	0	0	0	0.00%
Community Wellbeing	-7,077	0	0		0.00%
Grounds Operations	-663,345	-91,091	-66,615	-	26.87%
Grounds Overheads	-35,969	0	0		0.00%
Total Recreation and culture	-777,739	-91,091	-66,615	-24,476	26.87%
Transport					
-	-5,654,680	-3,814,231	-1,574,626	-2,239,605	58.72% <b>I</b>
Road Works	-3.034.000	-3,014.231	-1,574.020	-2,239.003	JU.7 Z /n I

	Budget	Budget YTD	Actual YTD	YTD Variance	YTD Var %
Operations Centre	-1,249,520	-340,613	-109,909	-230,704	67.73% I
Total Transport	-6,954,200	-4,154,844	-1,684,535	-2,470,309	59.46%
Economic services					
Ascot Close Housing	-45,000	0	0	0	0.00%
Wahroonga Housing	-45,000	0	0	0	0.00%
Building Control	-85,761	-31,417	-59,000	27,583	-87.80%
Building Operations	-15,731,980	-1,835,018	-1,481,692	-353,326	19.25% <b>I</b>
Total Economic services	-15,907,741	-1,866,435	-1,540,692	-325,743	17.45%
Other property and services					
Public Works Overheads	-23,756	0	-16,818	16,818	0.00%
Technical Services	-232,593	-94,251	-72,815	-21,436	22.74%
Total Other property and services	-256,349	-94,251	-89,633	-4,618	4.90%
tal Capital	-26,910,653	-6,578,264	-4,006,456	-2,571,808	39.10%

	Budget	Budget YTD	Actual YTD	YTD Variance	YTD Var %
erating					
Governance					
Finance Department	-2,036,940	-1,188,215	-1,189,836	1,621	-0.14%
Computing	-2,422,601	-1,413,184	-1,360,676	-52,508	3.72%
Marketing & Communications	-155,000	-75,000	-110,500	35,500	-47.33%
Reimbursements	-267,336	-155,946	-114,092	-41,853	26.84%
Insurance	-776,478	-769,545	-726,133	-43,411	5.64%
Executive Services	0	0	-10	10	0.00%
Chief Executive Officer	0	0	-1,265	1,265	0.00%
Records Management	-797,536	-465,229	-435,277	-29,953	6.44%
Human Resources	-1,365,836	-796,738	-840,951	44,213	-5.55%
Governance	0	0	-2,386	•	0.00%
Belmont Trust	-43,697	0	0	0	0.00%
Accommodation Costs	-599,583	-349,757	-291,713	-58,044	16.60% <b>I</b>
Total Governance	-8,465,007	-5,213,613	-5,072,840	-140,774	2.70%
General purpose funding					
Rates	-50,246,446	-49,853,297	-48,767,149	-1,086,148	2.18%
General Purpose Income	-435,639	-215,794	-215,720	-74	0.03%
Property & Economic Development	-405,520	-271,998	-263,232	-8,766	3.22%
Financing Activities	-2,011,214	-1,173,208	-889,538	-283,671	24.18% I
Total General purpose funding	-53,098,819	-51,514,297	-50,135,638	-1,378,659	2.68%
Law, order and public safety					
Belmont Community Watch	0	0	-4,500	4,500	0.00%
Rangers	-251,575	-146,752	-123,276	-23,476	16.00%
Crime Prevention & Comm Safety	-10,000	-5,833	-3,606	-2,227	38.18%
Volunteer Emergency Services	-75,532	-75,532	-42,870	-32,662	43.24%
Total Law, order and public safety	-337,107	-228,117	-174,252	-53,865	23.61%
Health					
Health	-449,869	-303,090	-277,065	-26,025	8.59%
Immunisation	-2,000	-1,167	-858	-309	26.46%
Total Health	-451,869	-304,257	-277,923	-26,334	8.66%
Education and welfare					
Aboriginal Strategies	0	0	-214	214	0.00%
Senior Citizens Centre	-5,496	-3,206	-2,913	-293	9.14%
Community Services	0	0	-1,287	1,287	0.00%
Belmont HACC Services	-2,450,366	-1,718,702	-1,766,223	47,521	-2.76%
Youth Services General	-70,500	-50,625	-48,956	-1,669	3.30%
Total Education and welfare	-2,526,362	-1,772,533	-1,819,592	47,060	-2.65%
Housing					
Orana Aged Housing	-105,000	0	0	0	0.00%
Gabriel Gardens	-94,000	0	0	0	0.00%
Faulkner Park Retirement Vill.	-200,000	-150,000	-94,211	-55,789	37.19% <b>N</b>
Total Housing	-399,000	-150,000	-94,211	-55,789	37.19%
Community amenities				404.700	40.070/
Community amenities Town Planning	-1 133 240	-661 062	-/85 830	174 /h×	-18 87% I
Town Planning	-1,133,249 -6.460.661	-661,062 -6.441.352	-785,830 -6.337.759	124,768 -103.593	-18.87% <b>I</b> 1.61% <b>I</b>
	-1,133,249 -6,460,661 -4,050	-661,062 -6,441,352 -3,300	-785,830 -6,337,759 -3,300	-103,593 0	-18.87% I 1.61% I 0.00%

	Budget	Budget YTD	Actual YTD	YTD Variance	YTD Var %
Recreation and culture					
Marketing & Communications	-33,333	-17,583	-24,109	6,525	-37.11%
Donations and Grants	0	0	-496	496	0.00%
Public Facilities Operations	-303,312	-175,411	-178,980	3,568	-2.03%
Belmont Oasis	-1,000	0	0	0	0.00%
Youth & Family Services Centre	-13,360	-7,793	-2,515	-5,278	67.73%
Ruth Faulkner Library	-80,589	-60,672	-52,288	-8,385	13.82%
Community Wellbeing	-6,300	-4,313	-5,418	1,105	-25.63%
Grounds Operations	-13,552	-6,904	-16,928	10,024	-145.19%
Grounds Overheads	-1,448,088	-840,030	-861,614	21,583	-2.57%
Total Recreation and culture	-1,899,534	-1,112,708	-1,142,347	29,640	-2.66%
Transport					
Road Works	-266,571	-139,716	-140,786	1,070	-0.77%
Streetscapes	-104,000	-52,000	-4,135	-47,865	92.05%
Operations Centre	0	0	-1,335	1,335	0.00%
·					
Total Transport	-370,571	-191,716	-146,256	-45,460	23.71%
Economic services					
Building Control	-394,352	-237,910	-268,917	31,007	-13.03%
Building Control Customer Service	-457,023	-266,597	-282,904	16,307	-6.12%
Building Operations	0	0	-17,595	17,595	0.00%
Building Overheads	-114,041	-65,507	-69,390	3,883	-5.93%
Total Economic services	-965,416	-570,014	-638,806	68,792	-12.07%
Other preparty and consists					
Other property and services	4 500 007	044.070	004.000	000.070	05.000/ 1
Public Works Overheads	-1,520,297	-811,872	-601,900	-209,972	25.86% N
Plant Operating Costs	-1,710,437	-958,360	-810,635	-147,724	15.41% <b>N</b>
Technical Services Other Public Works	-486,714 -69,000	-283,917 -32,583	-228,377 -109,256	-55,540 76,672	19.56% <b>N</b> -235.31% <b>N</b>
Total Other property and services	-3,786,448	-2,086,731	-1,750,168	-336,564	16.13%
tal Operating	-79,898,093	-70,249,699	-68,378,922	-1,870,777	2.66%
tal 2. Revenue	-106,808,746	-76,827,963	-72,385,378	-4,442,585	5.78%
Opening/Closing Funds					
erating					
P&I Clearing					_
P&L Clearing			0	0	0.00%
Opening Balance - Budget Only	-18051385	0	O .		
	-18051385 500,000	0	0	0	0.00%
Opening Balance - Budget Only				0 <b>0</b>	0.00%
Opening Balance - Budget Only Closing Balance - Budget Only  Total P&L Clearing	-17,551,385	<b>0</b>	0		0.00%
Opening Balance - Budget Only Closing Balance - Budget Only	500,000	0	0	0	

Add Opening Balance:

Nett Current Assets:

-18,051,385

-34,419,609

Budget Budget YTD Actual YTD YTD Variance YTD Var %